



Shire of
Perenjori
Embrace Opportunity

Shire of Perenjori – Ordinary Council Meeting

AGENDA

Thursday 20 March 2025



NOTICE OF AN ORDINARY COUNCIL MEETING

Dear Council Member,

The next Ordinary Meeting of the Shire of Perenjori will be held on Thursday 20 March 2025 in the Latham Community Centre, Latham WA 6616, commencing at 3.00 pm.

A handwritten signature in black ink, appearing to read 'Paul Anderson', is positioned above the printed name.

Paul Anderson

CHIEF EXECUTIVE OFFICER

Date: 14 March 2025

Table of Contents

1.	Declaration of Opening/Announcement of Visitors:.....	5
2.	Audio Recording of Proceedings	5
3.	Opening Prayer:	5
4.	Disclaimer Reading:	5
5.	Record of Attendance/Apologies/Leave of Absence:	5
	5.1 ATTENDANCE:.....	5
6.	Public Question Time:.....	5
	6.1 RESPONSE TO QUESTIONS TAKEN ON NOTICE:	5
	6.2 QUESTIONS WITHOUT NOTICE:	5
7.	Applications for Leave of Absence:.....	6
	7.1 APPLICATION/S FOR LEAVE OF ABSENCE:.....	6
8.	Confirmation of Minutes of Previous Meetings:.....	6
	8.1 ORDINARY COUNCIL MEETING HELD ON 20 FEBRUARY 2025.....	6
9.	Announcements by Presiding Member Without Discussion:.....	6
10.	Petitions/Deputations/Presentations:	6
11.	Announcements of Matters for Which Meeting May Be Closed:.....	6
12.	Declaration of Interest:	6
13.	Finance:	7
	13.1 MONTHLY FINANCIAL REPORT – 28 FEBRUARY 2025	7
	13.2 SCHEDULE OF FINANCIAL ACTIVITY FOR THE PERIOD ENDED 28 FEBRUARY 2025	9
14.	Community Development and Services:.....	11
	14.1 FUTURE OF LATHAM HALL	11
	14.2 PERENJORI AGRICULTURAL SHOW 2025	15
15.	Governance:.....	17
	15.1 ADOPTION OF COMPLIANCE AUDIT RETURN 2024	17
	15.2 PROPOSED RAIL SIDING & ASSOCIATED WORKS – LOT 101 MULLEWA – WUBIN ROAD, PERENJORI	19
	15.3 AUSTRALIAN LOCAL GOVERNMENT ASSOCIATION: NATIONAL GENERAL ASSEMBLY REGISTRATION	25
16.	Confidential Reports:.....	28
17.	Ordering the Common Seal:	28
18.	Reports of Committees and Members:	28
19.	Motions of Which Previous Notice Has Been Given:	28
20.	Notice of Motions:.....	28
21.	New Business of an Urgent Nature Admitted by Council:	28
22.	Closure of Meeting:.....	28
23.	Next Meeting:	28

Council Roles

Advocacy:

When Council advocates on its own behalf or on behalf of its community to another level of government / body / agency.

Executive/Strategic:

The substantial direction setting and oversight role of the Council e.g. Adopting plans and reports, accepting tenders, directing operations, setting and amending budgets.

Legislative:

Includes adopting local laws, town planning schemes and policies.

Review:

When Council reviews decisions made by Officers.

Quasi-Judicial:

When Council determines an application/matter that directly affects a person's rights and interests. The Judicial character arises from the obligations to abide by the principles of natural justice.

Examples of Quasi-Judicial authority include town planning applications, building licences, applications for other permits/licences (e.g.: under Health Act, Dog Act or Local Laws) and other decisions that may be appealable to the State Administrative Tribunal.

Disclaimer

"Any statement, comment or decision made at a Council or Committee meeting regarding any application for an approval, consent or licence, including a resolution of approval, is not effective as an approval of any application and must not be relied upon as such.

Any person or entity that has an application before the Shire must obtain, and should only rely on, written notice of the Shire's decision and any conditions attaching to the decision and cannot treat as an approval anything said or done at a Council or Committee meeting.

Any advice provided by an employee of the Shire on the operation of a written law, or the performance of a function by the Shire, is provided in the capacity of an employee, and to the best of that person's knowledge and ability. It does not constitute, and should not be relied upon, as a legal advice or representation by the Shire. Any advice on a matter of law, or anything sought to be relied upon as a representation by the Shire should be sought in writing and should make clear the purpose of the request."

Shire of Perenjori

Agenda for the Shire of Perenjori Ordinary Meeting of Council to be held on Thursday, 20 February 2025, at the Shire of Perenjori Council Chambers, Perenjori WA 6620, commencing at 3.00 pm.

1. Declaration of Opening/Announcement of Visitors:

The Shire President to declare the meeting open and welcome those in attendance.

Acknowledgement of Traditional Custodians: -

As per the Shire of Perenjori Policy (N° 1021) we wish to acknowledge the traditional owners of the land upon which the Shire of Perenjori is situated and to demonstrate respect for the original custodians.

I respectfully acknowledge the past and present traditional owners of the land on which we are meeting, the Badimia people. It is a privilege to be standing on Badimia country.

Acknowledgment of Pioneers;

I acknowledge the pioneers who settled this country, developed the land and turned it into the productive country we know today.

2. Audio Recording of Proceedings

In accordance with r.14I of the *Local Government (Administration) Regulations 1996*, this meeting will be recorded and made publicly available on the Shire's website, with publication of meeting minutes.

3. Opening Prayer:

The Shire President to read the opening prayer.

4. Disclaimer Reading:

As printed.

5. Record of Attendance/Apologies/Leave of Absence:

5.1 ATTENDANCE:

Members:

Staff:

Distinguished Visitors:

Members of The Public:

Leave of Absence:

Apologies:

6. Public Question Time:

6.1 RESPONSE TO QUESTIONS TAKEN ON NOTICE:

Insert Response

6.2 QUESTIONS WITHOUT NOTICE:

Insert Question

7. Applications for Leave of Absence:

7.1 APPLICATION/S FOR LEAVE OF ABSENCE:

OFFICER RECOMMENDATION

Council Resolution Number:

Moved:

Seconded:

That _____ be granted leave of absence for the meeting of _____

Motion put and carried / lost

For:

Against:

8. Confirmation of Minutes of Previous Meetings:

8.1 ORDINARY COUNCIL MEETING HELD ON 20 FEBRUARY 2025

OFFICER RECOMMENDATION

Council Resolution Number:

Moved:

Seconded:

That the Minutes of the Ordinary Meeting of Council held on 20 February 2025, be confirmed as true and correct subject to no / the following corrections.

Motion put and carried / lost

For:

Against:

[Next Item](#)

9. Announcements by Presiding Member Without Discussion:

10. Petitions/Deputations/Presentations:

11. Announcements of Matters for Which Meeting May Be Closed:

12. Declaration of Interest:

“Members should fill in Disclosure of Interest forms for items in which they have a financial, proximity or impartiality interest and forward these to the Presiding Member before the meeting commences.”



Shire of
Perenjori
Embrace Opportunity

Shire of Perenjori – Ordinary Council Meeting

MINUTES

Thursday 20 February 2025



NOTICE OF AN ORDINARY COUNCIL MEETING

Dear Council Member,

The next Ordinary Meeting of the Shire of Perenjori will be held on Thursday 20 February 2025 in the Shire of Perenjori Council Chambers, Perenjori WA 6620, commencing at 3.00 pm.

A handwritten signature in black ink, appearing to read 'Paul Anderson', is positioned above the printed name.

Paul Anderson

CHIEF EXECUTIVE OFFICER

Date: 14 February 2025

Table of Contents

1.	Declaration of Opening/Announcement of Visitors:.....	5
2.	Audio Recording of Proceedings	5
3.	Opening Prayer:	5
4.	Disclaimer Reading:	5
5.	Record of Attendance/Apologies/Leave of Absence:	5
	5.1 ATTENDANCE:.....	5
6.	Public Question Time:.....	6
	6.1 RESPONSE TO QUESTIONS TAKEN ON NOTICE:	6
	6.2 QUESTIONS WITHOUT NOTICE:	6
7.	Applications for Leave of Absence:.....	6
	7.1 APPLICATION/S FOR LEAVE OF ABSENCE:.....	6
8.	Confirmation of Minutes of Previous Meetings:.....	6
	8.1 ORDINARY COUNCIL MEETING HELD ON 19 DECEMBER 2024	6
9.	Announcements by Presiding Member Without Discussion:.....	7
10.	Petitions/Deputations/Presentations:	7
11.	Announcements of Matters for Which Meeting May Be Closed:.....	7
	LATE CONFIDENTIAL ITEM – CHIEF EXECUTIVE OFFICER RECRUITMENT	7
12.	Declaration of Interest:	7
	IMPARTIALITY INTEREST – CR CAMPBELL (ITEM 14.1)	7
	IMPARTIALITY INTEREST – PAUL ANDERSON (ITEM 21.1)	7
13.	Finance:	8
	13.1 MONTHLY FINANCIAL REPORT – 31 DECEMBER 2024	8
	13.2 SCHEDULE OF FINANCIAL ACTIVITY FOR THE PERIOD ENDED 31 DECEMBER 2024.....	10
	13.3 MONTHLY FINANCIAL REPORT – 31 JANUARY 2025.....	12
	13.4 SCHEDULE OF FINANCIAL ACTIVITY FOR THE PERIOD ENDED 31 JANUARY 2025	14
	13.5 2024-25 ANNUAL BUDGET REVIEW	16
	13.6 REMOVAL OF PLANT TRAILER FROM 2024-25 CAPITAL PURCHASES PROGRAM.....	18
	13.7 ELECTED MEMBER SUPERANNUATION PAYMENTS.....	20
14.	Community Development and Services:.....	23
	14.1 LATHAM FIRE AND AMBULANCE SHEDS	23
	14.2 RECOGNITION OF PREVIOUS RESIDENTS – K & I DELANE	27
15.	Governance:.....	32
	15.1 ANNUAL REPORT AMENDMENTS	32
	15.2 ANNUAL ELECTORS MEETING – 13 FEBRUARY 2025.....	34
	15.3 LOCAL GOVERNMENT ORDINARY ELECTION 2025.....	36
	15.4 PROPOSED TREE FARM – LOT 4754 SOLOMON RD & LOT 1 PERENJORI-ROTHSAY RD, BOWGADA	38
	15.5 APPOINTMENT OF PRESIDING AND DEPUTY PRESIDING MEMBER – AUDIT COMMITTEE	48
	15.6 LEASE RENEWAL (LEASE L6808) – LICENSE TO OCCUPY RAIL CORRIDOR, BRITT STREET, LATHAM	50
	15.7 TERRA MINING ROAD USER, CONTRIBUTION AND UPGRADE AGREEMENT	54
16.	Confidential Reports:.....	59
17.	Ordering the Common Seal:	59
18.	Reports of Committees and Members:	59
19.	Motions of Which Previous Notice Has Been Given:	59
20.	Notice of Motions:.....	59
21.	New Business of an Urgent Nature Admitted by Council:	59
	21.1 LATE CONFIDENTIAL ITEM – CHIEF EXECUTIVE OFFICER RECRUITMENT	60
	21.2 LATE ITEM – VARIATIONS TO THE MID WEST SECONDARY FREIGHT NETWORK PROGRAM	62
22.	Closure of Meeting:.....	65
23.	Next Meeting:	65

Council Roles

Advocacy:

When Council advocates on its own behalf or on behalf of its community to another level of government /body /agency.

Executive/Strategic:

The substantial direction setting and oversight role of the Council e.g. Adopting plans and reports, accepting tenders, directing operations, setting and amending budgets.

Legislative:

Includes adopting local laws, town planning schemes and policies.

Review:

When Council reviews decisions made by Officers.

Quasi-Judicial:

When Council determines an application/matter that directly affects a person's rights and interests. The Judicial character arises from the obligations to abide by the principles of natural justice.

Examples of Quasi-Judicial authority include town planning applications, building licences, applications for other permits/licences (e.g.: under Health Act, Dog Act or Local Laws) and other decisions that may be appealable to the State Administrative Tribunal.

Disclaimer

"Any statement, comment or decision made at a Council or Committee meeting regarding any application for an approval, consent or licence, including a resolution of approval, is not effective as an approval of any application and must not be relied upon as such.

Any person or entity that has an application before the Shire must obtain, and should only rely on, written notice of the Shire's decision and any conditions attaching to the decision and cannot treat as an approval anything said or done at a Council or Committee meeting.

Any advice provided by an employee of the Shire on the operation of a written law, or the performance of a function by the Shire, is provided in the capacity of an employee, and to the best of that person's knowledge and ability. It does not constitute, and should not be relied upon, as a legal advice or representation by the Shire. Any advice on a matter of law, or anything sought to be relied upon as a representation by the Shire should be sought in writing and should make clear the purpose of the request."

Shire of Perenjori

Minutes for the Shire of Perenjori Ordinary Meeting of Council held on Thursday, 20 February 2025, at the Shire of Perenjori Council Chambers, Perenjori WA 6620.

1. Declaration of Opening/Announcement of Visitors:

The Shire President declared the meeting open and welcomed those in attendance at 3.03 pm.

Acknowledgement of Traditional Custodians: -

As per the Shire of Perenjori Policy (Nº 1021) we wish to acknowledge the traditional owners of the land upon which the Shire of Perenjori is situated and to demonstrate respect for the original custodians.

I respectfully acknowledge the past and present traditional owners of the land on which we are meeting, the Badimia people. It is a privilege to be standing on Badimia country.

2. Audio Recording of Proceedings

In accordance with r.14I of the *Local Government (Administration) Regulations 1996*, this meeting will be recorded and made publicly available on the Shire's website, with publication of meeting minutes.

3. Opening Prayer:

The Shire President read the opening prayer.

Acknowledgment of Pioneers;

I acknowledge the pioneers who settled this country, developed the land and turned it into the productive country we know today.

4. Disclaimer Reading:

As printed.

5. Record of Attendance/Apologies/Leave of Absence:

5.1 ATTENDANCE:

Members:	Cr Jude Sutherland (President) Cr Les Hepworth (Deputy President) Cr Daniel Bradford Cr Andrew Fraser Cr Colin Bryant Cr Brian Campbell Cr Dael Sparkman
Staff:	Paul Anderson (Chief Executive Officer) Nola Comerford (Manager Corporate Community Services) Marty Noordhof (Manager Infrastructure Services) Bianca Plug (Governance Officer)
Distinguished Visitors:	Nil
Members of The Public:	Rob Cooling
Leave of Absence:	Nil
Apologies:	Nil

6. Public Question Time:

6.1 RESPONSE TO QUESTIONS TAKEN ON NOTICE:

Nil

6.2 QUESTIONS WITHOUT NOTICE:

Nil

7. Applications for Leave of Absence:

7.1 APPLICATION/S FOR LEAVE OF ABSENCE:

Nil

8. Confirmation of Minutes of Previous Meetings:

8.1 ORDINARY COUNCIL MEETING HELD ON 19 DECEMBER 2024

COUNCIL DECISION

Council Resolution Number: 200225.1

Moved: Cr Sparkman Seconded: Cr Bryant

That the Minutes of the Ordinary Meeting of Council held on 19 December 2024, be confirmed as true and correct subject to no corrections.

Motion put and carried 7/0

For: Cr Sutherland, Cr Hepworth, Cr Bryant, Cr Sparkman, Cr Bradford, Cr Fraser, Cr Campbell

Against: Nil

[Next Item](#)

9. Announcements by Presiding Member Without Discussion:

Nil

10. Petitions/Deputations/Presentations:

Nil

11. Announcements of Matters for Which Meeting May Be Closed:

LATE CONFIDENTIAL ITEM – CHIEF EXECUTIVE OFFICER RECRUITMENT

In accordance with Section 5.23(2) of the *Local Government Act 1995*, part of the meeting was closed to members of the public as the following sub-sections applied to the late confidential item:

- (a) a matter affecting an employee or employees; and
- (c) a contract entered into, or which may be entered into, by the local government and which relates to a matter to be discussed at the meeting.

12. Declaration of Interest:

IMPARTIALITY INTEREST – CR CAMPBELL (ITEM 14.1)

Cr Campbell declared an impartiality interest in Item 14.1 Latham Fire and Ambulance Sheds due to being a Latham Fire Brigade Captain and member of the Latham Progress Association. Cr Campbell remained in the room.

IMPARTIALITY INTEREST – PAUL ANDERSON (ITEM 21.1)

Paul Anderson declared an impartiality interest in Item 21.1 Late Confidential Item - Chief Executive Officer Recruitment due to currently being employed in the position of Chief Executive Officer. Paul Anderson remained in the room.

13. Finance:

13.1 MONTHLY FINANCIAL REPORT – 31 DECEMBER 2024

Applicant:	Shire of Perenjori
File:	ADM 0082
Report Date:	20 February 2025
Disclosure of Interest:	Nil
Voting Requirements:	Simple Majority
Author:	Ally Bryant – Finance Manager
Responsible Officer:	Paul Anderson – Chief Executive Officer
Attachments:	13.1.1 - Monthly Statement of Financial Activity for 31 December 2024

Summary

This item recommends that Council receives the Financial Activity Statements for the periods ending 31 December 2024.

Background

Regulation 34 of the Local Government (Financial Management) Regulations 1996 requires local governments to prepare a Monthly Statement of Financial Activity.

Recent changes to the Regulations require a focus on reporting and variance analysis by nature and type rather than by program. The Shire's reports have historically been prepared by both nature and type and program with the variance analysis done by program. All reports for 2024-25 to date have been prepared by both nature and type and program, but with the variance analysis being done by nature and type in compliance with the revised Regulations.

Statutory Environment

Regulation 34 of the Local Government (Financial Management) Regulations 1996 requires local governments to prepare a Monthly Statement of Financial Activity.

Policy Implications

Nil

Consultation

Nil

Financial Implications

Shown in the attached data.

Strategic Community Plan

Goal 4: A strong and diverse Council working closely with the proactive and involved community.

Strategic Objectives

4.6. The organisation, assets and finances of the Shire are managed responsibly

Officer Comment

The audited opening surplus for the 2023-24 financial year is \$3,170,411. As previously advised, this surplus is notionally deceiving as the State Government transferred 85% of its 2024-25 financial assistance grants (totalling \$2.496m) to the Shire in June 2024.

Operating revenue YTD is \$6,081,828 over budget by \$502,234.

Operating expenditure YTD is \$7,345,626, over budget by \$331,351.

Key variances between the YTD budget and actuals are explained in the attached report on pages 2-3.

COUNCIL DECISION

Council Resolution Number: 200225.2

Moved: Cr Hepworth

Seconded: Cr Bradford

That the Monthly Financial Report to 31 December 2024 as attached be received.

Motion put and carried 7/0

For: Cr Sutherland, Cr Hepworth, Cr Bryant, Cr Sparkman, Cr Bradford, Cr Fraser, Cr Campbell

Against: Nil

[Next Item](#)

13.2 SCHEDULE OF FINANCIAL ACTIVITY FOR THE PERIOD ENDED 31 DECEMBER 2024

Applicant:	Shire of Perenjori
File:	ADM 0082
Report Date:	20 February 2025
Disclosure of Interest:	Nil
Voting Requirements:	Simple Majority
Author:	Gypsie Douglas – Finance Officer
Responsible Officer:	Ally Bryant – Finance Manager
Attachments:	13.2.1 - Accounts for Payment 31 December 2024 13.2.2 - Corporate Credit Card Statement & Breakdown (NAB)

Summary

Council is presented the list of payments made from the Municipal, Trust and Reserve Accounts under delegation since the last Ordinary Council Meeting.

Background

Council delegates authority to the Chief Executive Officer annually:

To make payments from Trust, Reserve and Municipal Fund;

To purchase goods and services to a value of not more than \$250,000;

Legal Compliance

Local Government Act 1995

S6.5. Accounts and records

Local Government (Financial Management) Regulations 1996

R11. Payments, procedures for making etc.

R12. Payments from municipal fund or trust fund, restrictions on making

(1) A payment may only be made from the municipal fund or the trust fund —

(a) if the local government has delegated to the CEO the exercise of its power to make payments from those funds — by the CEO; or

(b) otherwise, if the payment is authorised in advance by a resolution of the council.

(2) The council must not authorise a payment from those funds until a list prepared under regulation 13(2) containing details of the accounts to be paid has been presented to the council.

S13. Payments from municipal fund or trust fund by CEO, CEO's duties as to etc.

(1) If the local government has delegated to the CEO the exercise of its power to make payments from the municipal fund or the trust fund, a list of accounts paid by the CEO is to be prepared each month showing for each account paid since the last such list was prepared —

(a) the payee's name; and

(b) the amount of the payment; and

(c) the date of the payment; and

(d) sufficient information to identify the transaction.

(2) A list of accounts for approval to be paid is to be prepared each month showing —

(a) for each account which requires council authorisation in that month —

(i) the payee's name; and

(ii) the amount of the payment; and

(iii) sufficient information to identify the transaction; and

(b) the date of the meeting of the council to which the list is to be presented.

(3) A list prepared under sub-regulation (1) or (2) is to be —

- (a) presented to the council at the next ordinary meeting of the council after the list is prepared; and
- (b) recorded in the minutes of that meeting.

Policy Implications

Nil

Council Policy Compliance

Payments are checked to ensure compliance with Council's Purchasing Policy Number 4007 – Procurement Policy.

Financial Implications

All payments are made in accordance with the adopted annual budget.

Strategic Community Plan

Goal 4: A strong and diverse Council working closely with the proactive and involved community.

4.6. The organisation, assets and finances of the Shire are managed responsibly.

Consultation

Paul Anderson – Chief Executive Officer

Ally Bryant – Finance Manager

Officer Comment

Accounts paid for the month ending 31 December 2024.

Municipal Account	
EFT 18160 - 18267	\$2,178,019.53
Direct Debits	\$95,917.22
Cheques	\$0.00
Corporate MasterCard	\$7,106.26
Bank Fees	\$ 0.00
Total	\$2,281,313.78

Trust Account – Mt Gibson Public Benefit Funds	
EFT – Transfer to another account (Close Term Deposit)	\$0.00
Cheques	\$0.00
Bank Fees	\$0.00
Total	\$0.00

Totalling **\$2,281,313.78** from *Municipal* and *Trust Accounts* for the month ending **31 December 2024**.

COUNCIL DECISION

Council Resolution Number: 200225.3

Moved: Cr Fraser

Seconded: Cr Hepworth

That the cheques and electronic payments as per the attached schedules of accounts for payment totaling \$2,281,313.78 (Two million, two hundred and eighty-one thousand, three hundred and thirteen dollars and seventy eight cents) be accepted.

Motion put and carried 7/0

For: Cr Sutherland, Cr Hepworth, Cr Bryant, Cr Sparkman, Cr Bradford, Cr Fraser, Cr Campbell

Against: Nil

[Next Item](#)

13.3 MONTHLY FINANCIAL REPORT – 31 JANUARY 2025

Applicant:	Shire of Perenjori
File:	ADM 0082
Report Date:	20 February 2025
Disclosure of Interest:	Nil
Voting Requirements:	Simple Majority
Author:	Ally Bryant – Finance Manager
Responsible Officer:	Paul Anderson – Chief Executive Officer
Attachments:	13.3.1 - Monthly Statement of Financial Activity for 31 January 2025

Summary

This item recommends that Council receives the Financial Activity Statements for the periods ending 31 January 2025.

Background

Regulation 34 of the Local Government (Financial Management) Regulations 1996 requires local governments to prepare a Monthly Statement of Financial Activity.

Recent changes to the Regulations require a focus on reporting and variance analysis by nature and type rather than by program. The Shire's reports have historically been prepared by both nature and type and program with the variance analysis done by program. All reports for 2024-25 to date have been prepared by both nature and type and program, but with the variance analysis being done by nature and type in compliance with the revised Regulations.

Statutory Environment

Regulation 34 of the Local Government (Financial Management) Regulations 1996 requires local governments to prepare a Monthly Statement of Financial Activity.

Policy Implications

Nil

Consultation

Nil

Financial Implications

Shown in the attached data.

Strategic Community Plan

Goal 4: A strong and diverse Council working closely with the proactive and involved community.

Strategic Objectives

4.6. The organisation, assets and finances of the Shire are managed responsibly

Officer Comment

The audited opening surplus for the 2023-24 financial year is \$3,170,411. As previously advised, this surplus is notionally deceiving as the State Government transferred 85% of its 2024-25 financial assistance grants (totalling \$2.496m) to the Shire in June 2024.

Operating revenue YTD is \$6,286,697 over budget by \$399,987.

Operating expenditure YTD is \$8,224,826 over budget by \$26,341.

Key variances between the YTD budget and actuals are explained in the attached report on pages 2-3.

COUNCIL DECISION

Council Resolution Number: 200225.4

Moved: Cr Bryant

Seconded: Cr Bradford

That the Monthly Financial Report to 31 January 2025 as attached be received.

Motion put and carried 7/0

For: Cr Sutherland, Cr Hepworth, Cr Bryant, Cr Sparkman, Cr Bradford, Cr Fraser, Cr Campbell

Against: Nil

[Next Item](#)

13.4 SCHEDULE OF FINANCIAL ACTIVITY FOR THE PERIOD ENDED 31 JANUARY 2025

Applicant:	Shire of Perenjori
File:	ADM 0082
Report Date:	20 February 2025
Disclosure of Interest:	Nil
Voting Requirements:	Simple Majority
Author:	Gypsie Douglas – Finance Officer
Responsible Officer:	Ally Bryant – Finance Manager
Attachments:	13.4.1 - Accounts for Payment 31 January 2025 13.4.2 - Corporate Credit Card Statement & Breakdown (NAB)

Summary

Council is presented the list of payments made from the Municipal, Trust and Reserve Accounts under delegation since the last Ordinary Council Meeting.

Background

Council delegates authority to the Chief Executive Officer annually:

To make payments from Trust, Reserve and Municipal Fund;

To purchase goods and services to a value of not more than \$250,000;

Legal Compliance

Local Government Act 1995

S6.5. Accounts and records

Local Government (Financial Management) Regulations 1996

R11. Payments, procedures for making etc.

R12. Payments from municipal fund or trust fund, restrictions on making

(1) A payment may only be made from the municipal fund or the trust fund —

(a) if the local government has delegated to the CEO the exercise of its power to make payments from those funds — by the CEO; or

(b) otherwise, if the payment is authorised in advance by a resolution of the council.

(2) The council must not authorise a payment from those funds until a list prepared under regulation 13(2) containing details of the accounts to be paid has been presented to the council.

S13. Payments from municipal fund or trust fund by CEO, CEO's duties as to etc.

(1) If the local government has delegated to the CEO the exercise of its power to make payments from the municipal fund or the trust fund, a list of accounts paid by the CEO is to be prepared each month showing for each account paid since the last such list was prepared —

(a) the payee's name; and

(b) the amount of the payment; and

(c) the date of the payment; and

(d) sufficient information to identify the transaction.

(2) A list of accounts for approval to be paid is to be prepared each month showing —

(a) for each account which requires council authorisation in that month —

(i) the payee's name; and

(ii) the amount of the payment; and

(iii) sufficient information to identify the transaction; and

(b) the date of the meeting of the council to which the list is to be presented.

(3) A list prepared under sub-regulation (1) or (2) is to be —

- (a) presented to the council at the next ordinary meeting of the council after the list is prepared; and
- (b) recorded in the minutes of that meeting.

Policy Implications

Nil

Council Policy Compliance

Payments are checked to ensure compliance with Council's Purchasing Policy Number 4007 – Procurement Policy.

Financial Implications

All payments are made in accordance with the adopted annual budget.

Strategic Community Plan

Goal 4: A strong and diverse Council working closely with the proactive and involved community.

4.6. The organisation, assets and finances of the Shire are managed responsibly.

Consultation

Paul Anderson – Chief Executive Officer

Ally Bryant – Finance Manager

Officer Comment

Accounts paid for the month ending 31 January 2025.

Municipal Account	
EFT 18557 - 18642	\$1,455,635.79
Direct Debits	\$143,734.55
Cheques	\$0.00
Corporate MasterCard	\$5,351.44
Bank Fees	\$ 312.16
Total	\$1,605,033.94

Trust Account – Mt Gibson Public Benefit Funds	
EFT – Transfer to another account (Close Term Deposit)	\$0.00
Cheques	\$0.00
Bank Fees	\$0.00
Total	\$0.00

Totalling **\$1,605,033.94** from *Municipal* and *Trust Accounts* for the month ending **31 January 2025**.

COUNCIL DECISION

Council Resolution Number: 200225.5

Moved: Cr Bradford

Seconded: Cr Fraser

That the cheques and electronic payments as per the attached schedules of accounts for payment totaling \$1,605,033.94 (one million, six hundred and five thousand, thirty three dollars and ninety four cents) be accepted.

Motion put and carried 7/0

For: Cr Sutherland, Cr Hepworth, Cr Bryant, Cr Sparkman, Cr Bradford, Cr Fraser, Cr Campbell

Against: Nil

[Next Item](#)

13.5 2024-25 ANNUAL BUDGET REVIEW

Applicant:	Shire of Perenjori
File:	ADM 0339
Report Date:	20 February 2025
Disclosure of Interest:	Nil
Voting Requirements:	Absolute Majority
Author:	Ally Bryant – Finance Manager
Responsible Officer:	Paul Anderson – Chief Executive Officer
Attachments:	13.5.1 – 2024-25 Annual Budget Review

Summary

Section 33A of the *Local Government (Financial Management) Regulations 1996* requires that a review of its annual budget is carried out between 1 January and 28 February of every year.

Background

The review has been carried out by senior staff responsible for budgets. Key areas where there have been changes in financial situation have been identified and changes to the budget recommended.

The Shire is required to provide a copy of the budget review to the Department of Local Government within 30 days after Council has made its determinations.

The more significant changes to income or expenditures are addressed in this report.

Statutory Environment

Local Government (Financial Management) Regulations 1996 Part 33A requires the Council to consider the review for the period from 1 July and ending no earlier than 31 December of that financial year. The regulation also requires the Council to consider the local governments financial position, consider the options presented and decide whether to adopt the review as presented, any part of the review or any recommendations made in the review, and consider the outcomes that are forecast in the budget.

Policy Implications

Nil

Consultation

Paul Anderson – Chief Executive Officer

Nola Comerford – Manager of Corporate and Community Services

Marty Noordhof – Manager of Infrastructure Services

Financial Implications

Shown in the attached report, with no changes to the budgeted closing net surplus/deficit.

Strategic Community Plan

Goal 4: A strong and diverse Council working closely with the proactive and involved community.

4.6. The organisation, assets and finances of the Shire are managed responsibly.

Officer Comment

The 2024-25 Budget was approved by Council in July 2024.

As a budget review compares to the original budget adopted it is to be noted that the budget review also addresses all amendments adopted by Council since July 2024.

This budget review addresses the key changes in circumstances since the budget was approved. The key movements are:

- Increase in net operating of \$7,567. A review of operating accounts was conducted and accounts altered to reflect their true budget requirements.
- Capital Grant income increased by \$1,191,470. This is the increase of MWSGFN funding \$1,137,635, decrease in revenue for CSRFF Grant of \$61,623 and increase Lottery West Grant \$115,458.
- Capital projects increasing by \$1,069,286. This is made up by a reduction in Plant & Equipment of \$99,989, increase in Roads \$1,106,556, decrease in Other Infrastructure \$2,500, increase in Buildings \$135,219 and Furniture & Fittings decrease \$70,000

Capital Job	Original Budget	Amend Budget	Comment
Community Hub (Supermarket)	\$3,503,070	\$4,107,684	Resolution 241024.6 \$507,930
MWSGFN Morawa South Rd	\$0	\$1,054,395	Resolution 211124.2 \$919,128
MWSGFN Hil Rd	\$168,233	\$155,425	Reflecting max grant available
MWSGFN Boundary Rd	\$2,433,491	\$2,529,539	Reflecting max grant available
Perenjori Rothsay Rd	\$0	\$259,155	Resolution 211124.2
Warriedar Coppermine Rd	\$446,500	\$189,191	Resolution 211124.2
Carnamah Perenjori Rd	\$219,817	\$230,468	
Crossing Rd	\$417,220	\$382,758	
Iona Rd	\$18,810	\$9,696	
Altas Payroll	\$70,000	\$0	
Admin Vehicle	\$40,000	\$44,531	
Additional Housing C/F 23/24	\$36,000	\$58,647	
GROH Housing	\$1,200,000	\$600,000	
Latham Bowls Club	\$184,428	\$181,928	
Perenjori Pavillion Upgrades	\$0	\$115,458	Lottery West Grant Funded
Utility – Works Manager	\$65,000	\$61,505	
Utility – Works Crew	\$65,000	\$46,025	
Bob Cat Trailer	\$20,000	\$0	
Multi Tyre Roller Attachment	\$70,000	\$0	
Multi Tyre Roller Attachment	\$70,000	\$71,950	
New Prime Mover	\$360,000	\$366,000	
Upgrade Chalets 3&4	\$30,000	\$22,500	
Total	\$9,417,569	\$10,486,855	
Net Result	\$1,069,286		

- Transfer from Reserves decreased by \$262,070 being \$70,000 less from IT Communication Reserve and \$192,070 less from Mt Gibson Infrastructure Reserve.
- Transfer to Reserves decreasing by \$5,000 due to reducing transfer to IT Communication Reserve from \$105,330 to \$100,330.

Attached is the statutory budget review document which details the recommended budget adjustments. The net impact of the adjustment is to maintain the closing surplus/deficit at \$0.

COUNCIL DECISION

Council Resolution Number: 200225.6

Moved: Cr Sparkman

Seconded: Cr Hepworth

That Council, in accordance with Regulation 33A of the *Local Government (Financial Management) Regulations 1996*, adopts the 2024-25 Budget Review, and submits a copy of the adopted 2024-25 Budget Review to the Department of Local Government within 14 days of Council adoption.

Motion put and carried 7/0 by Absolute Majority

For: Cr Sutherland, Cr Hepworth, Cr Bryant, Cr Sparkman, Cr Bradford, Cr Fraser, Cr Campbell

Against: Nil

[Next Item](#)

13.6 REMOVAL OF PLANT TRAILER FROM 2024-25 CAPITAL PURCHASES PROGRAM

Applicant:	Shire of Perenjori
File:	ADM 0339
Report Date:	20 February 2025
Disclosure of Interest:	Nil
Voting Requirements:	Simple Majority
Author:	Marty Noordhof – Manager Infrastructure Services
Responsible Officer:	Paul Anderson – Chief Executive Officer
Attachments:	Nil

Summary

The purpose of this report is to seek councils' endorsement to remove the purchase of a plant trailer from the 2024-2025 capital purchases program.

Background

The Shire initially intended to purchase a plant trailer for use by staff to facilitate the safe and convenient transportation of small plant equipment around the Shire. However, upon further consideration, it was determined that a trailer of sufficient size to accommodate a payload exceeding 4 tonnes (such as the weight of a bobcat) would likely have an Aggregate Trailer Mass (ATM) in the range of 8-9 tonnes. This exceeds the Gross Combination Mass (GCM) of our largest flat-top rigid truck, which is only capable of towing a trailer with a maximum weight of 5.6 tonnes.

Statutory Environment

The Local Government Act 1995 (WA) is the primary legislation governing local government operations in Western Australia. The Act includes provisions regarding financial management, budgeting, and decision-making processes that local governments must follow, including the capital budget.

- Section 6.2 - Adoption of Budget: Local governments are required to adopt an annual budget for the local government. Any changes to the budget, including reductions in the capital budget, must be done in accordance with proper procedures outlined in the Act.
- Section 6.8 - Expenditure from Budget: If a local government wants to alter its budget during the year (such as reducing the capital budget), it must have a resolution of the Council to approve such a change. Expenditure not included in the adopted budget, or any changes to existing budget allocations, requires formal approval.

Policy Implications

Nil

Consultation

Paul Anderson – Chief Executive Officer

Financial Implications

Reduction in capital budget of \$20,000 due to the cancellation of the purchase of a plant trailer.

Strategic Community Plan

Goal 4: A strong and diverse Council working closely with the proactive and involved community.
4.6. The organisation, assets and finances of the Shire are managed responsibly.

Officer Comment

Given these constraints, it is recommended that the purchase of a plant trailer be reconsidered, as such a trailer would either remain underutilised or place Shire staff at risk of non-compliance with legal towing limits.

COUNCIL DECISION

Council Resolution Number: 200225.7

Moved: Cr Campbell

Seconded: Cr Bryant

That Council:

- 1. Notes that a trailer of sufficient size to accommodate a payload exceeding 4 tonnes would exceed the Gross Combination Mass (GCM) of the Shire's largest flat-top rigid truck, which is limited to towing a trailer of up to 5.6 tonnes.**

AND

- 2. Agrees to the recommendation to not proceed with the purchase of a plant trailer, as it would either remain underutilised or create a situation where Shire staff could be at risk of non-compliance with legal towing limits.**

Motion put and carried 7/0

For: Cr Sutherland, Cr Hepworth, Cr Bryant, Cr Sparkman, Cr Bradford, Cr Fraser, Cr Campbell

Against: Nil

[Next Item](#)

13.7 ELECTED MEMBER SUPERANNUATION PAYMENTS

Applicant:	Shire of Perenjori
File:	ADM 0073
Report Date:	20 February 2025
Disclosure of Interest:	Nil
Voting Requirements:	Absolute Majority
Author:	Nola Comerford – Manager Corporate Community Services
Responsible Officer:	Paul Anderson – Chief Executive Officer
Attachments:	Nil

Summary

This report requires Council to consider the inclusion of superannuation contribution payments in addition to remuneration received for performing the role of elected member.

Background

The superannuation guarantee was introduced by the Australian Government in 1992/1993, with compulsory contributions rising at regular intervals from 3% of wages in that year. The superannuation guarantee is scheduled to rise incrementally from 11.5% of wages in 2024/25 to 12% by 1 July 2025.

New provisions commenced on 1 February 2025 to streamline the process for local governments to pay superannuation to council members.

The changes are part of the work to implement the *Local Government Amendment Act 2024* and will allow local governments to resolve to pay superannuation to council members, by an absolute majority decision.

While council members are not considered employees under the Superannuation Guarantee Act, they are now able to be treated in the same manner as employees of the local government for the purposes of calculating superannuation contribution payments.

From 19 October 2025, it will be compulsory for all class 1 and 2 local governments to pay superannuation to council members. This will remain optional for class 3 and 4 local governments. The Shire of Perenjori is a class 4 local government.

Statutory Environment

New sections 5.99B to 5.99E of the *Local Government Act 1995* allow government to pay superannuation to council members, through a decision-making process which requires an absolute majority vote.

Policy Implications

Whilst Council does not have any policies in reference to superannuation, the Shire of Perenjori's Enterprise Bargaining Agreement 2019 (EBA), Section 14.1.1 states *'Payments made by the Employer in excess of the amount prescribed in the Superannuation Guarantee Administration Act 1992 and the Superannuation Guarantee Charge Act 1992 (as varied from time to time) will be contingent upon the Employee making their own contributions of up to 6%. These additional contributions will be matched up to a maximum additional contribution of 6%.'*

The EBA is specific to 'Employees employed by the Shire of Perenjori, excluding Senior Employees as defined by clause 5.37 of the Local Government Act 1995'.

Consultation

Paul Anderson – Chief Executive Officer
Ally Bryant – Finance Manager

Financial Implications

The cost of paying superannuation for council members would need to be included in Council's Annual Budget.

The minimum superannuation guarantee paid from 1 July 2024 is 11.5%, which is scheduled to progressively increase to 12% on 1 July 2025. Based on current annual Elected Member, President and Deputy President payments of \$78,712, an allowance of \$9,445 would be required in the 2025/26 Annual Budget. A pro-rata allowance of \$9,052 would be required in the 2024/25 Annual Budget review if the superannuation was commenced in this financial year.

Strategic Community Plan

Goal 4: A strong and diverse Council working closely with the proactive and involved community.

4.6. The organisation, assets and finances of the Shire are managed responsibly.

Officer Comment

From 1 February 2025, elected members are eligible to receive superannuation contribution payments following amendments to the *Local Government Act 1995*. The conditions of payment include:

- Council resolves to make available superannuation contribution payments to elected members.
- The superannuation contribution payments are limited to the superannuation guarantee (currently \$11.5%, incrementally rising to 12% from 1 July 2025) and are payable at the same intervals as a Councillor's annual fee.
- Individual Councillors must nominate a registered superannuation fund for receipt of the superannuation guarantee payment.
- A Councillor may forgo the superannuation contribution payment or request a reduced payment less than the superannuation guarantee charge, in writing.

If Council resolve to instigate payment of superannuation for elected members, it is recommended that the payments commence from 1 July 2025 to align with Council Budget.

Council may alternatively consider an increase in council member annual fees with which elected members may make their own contributions to their superannuation fund.

PROCEDURAL MOTION

Council Resolution Number: 200225.8

Moved: Cr Sparkman

Seconded: Cr Hepworth

That Council suspend Standing Orders, *Clause 9.5 Limitation on number of speeches*, at 3.11 pm to allow for open discussion of Item 13.7.

Motion put and carried 7/0

For: Cr Sutherland, Cr Hepworth, Cr Bryant, Cr Sparkman, Cr Bradford, Cr Fraser, Cr Campbell

Against: Nil

PROCEDURAL MOTION

Council Decision Number: 200225.9

Moved: Cr Hepworth

Seconded: Cr Fraser

That Council reinstate Standing Orders at 3.12 pm.

Motion put and carried /0

For: Cr Sutherland, Cr Hepworth, Cr Bryant, Cr Sparkman, Cr Bradford, Cr Fraser, Cr Campbell

Against: Nil

OFFICER RECOMMENDATION

- 1. Approve payment of the superannuation contribution payments in addition to the annual fee paid to elected members, commencing on 1 July 2025.**
- 2. Note that a superannuation contribution payment allocation will be included in the 2025/26 Annual Budget.**
- 3. Councillors who wish to receive the superannuation contribution payment to complete the Australia Taxation Office's online 'Choice of Superannuation Fund' form and return to the Chief Executive Officer.**

OR

- 4. Review annual elected member fees for the 2025/26 Annual Budget with a view to compensating the amount that would have been paid in superannuation guaranteed payments.**

OR

- 5. Resolve to continue to pay elected members annual fees and allowances without a superannuation guarantee payment.**

COUNCIL DECISION

Council Resolution Number: 200225.10

Moved: Cr Bryant

Seconded: Cr Fraser

That Council:

- 5. Resolve to continue to pay elected members annual fees and allowances without a superannuation guarantee payment.**

Motion put and carried 7/0 by Absolute Majority

For: Cr Sutherland, Cr Hepworth, Cr Bryant, Cr Sparkman, Cr Bradford, Cr Fraser, Cr Campbell

Against: Nil

[Next Item](#)

14. Community Development and Services:

14.1 LATHAM FIRE AND AMBULANCE SHEDS

Applicant:	Shire of Perenjori
File:	ADM 0186
Date:	20 February 2025
Disclosure of Interest:	Cr Campbell - Impartiality Interest
Voting Requirements:	Simple Majority
Author:	Nola Comerford – Manager Corporate and Community Services
Responsible Officer:	Paul Anderson – Chief Executive Officer
Attachments:	Nil

Summary

For Council to consider the future of two sheds located in the Latham townsite, previously utilised for St John Ambulance and Bushfire Brigade vehicles.

Background

Two galvanised iron / Colorbond sheds are situated at 27 Britt Street in the Latham townsite. Originally utilised for parking of the St John Ambulance van and Latham Bushfire Brigade truck, the sheds were decommissioned after the construction of new sheds for the emergency service vehicles.

At the Ordinary Council Meeting held 30 May 2024, a recommendation to sell the structures by public tender was put to Council but it was resolved to lay the matter on the table in order to seek community expressions of interest on the future of the sheds.

That Council:

1. Subject to Section 3.58 (3) of the Local Government Act 1995, resolve to sell by public tender two x sheds situated at 27 Britt Street, Latham
2. Delegate authority to the Chief Executive Officer to accept the highest bids offered.

Council Recommendation Number: 300524.11

Moved: Cr Bryant

Seconded: Cr Fraser

That the item be laid on the table until the September Ordinary Council Meeting to enable further consultation with the community.

Motion put and carried: 6/0

For: Cr Hepworth, Cr Bradford, Cr Bryant, Cr Campbell, Cr Fraser, Cr Sparkman

Against: Nil

Following publication of an Expression of Interest notice, only one response was received being from the Latham Progress Association who requested the use of both sheds, remaining on the current site, for the purpose of storing gardening equipment.

At the Ordinary Council Meeting held 24 October 2024, Council resolved to sell the sheds by public tender. However, following advertising, no tenders were received.

Council Resolution Number: 241024.10

Moved: Cr Fraser

Seconded: Cr Bryant

That Council:

1. Subject to Section 3.58 (3) of the Local Government Act 1995, resolve to sell by public tender two x sheds situated at 27 Britt Street, Latham; and
2. Request the Chief Executive Officer to present offers received to Council to determine the most advantageous tenders.

Motion put and carried 4/1

For: Cr Bryant, Cr Bradford, Cr Sparkman, Cr Fraser

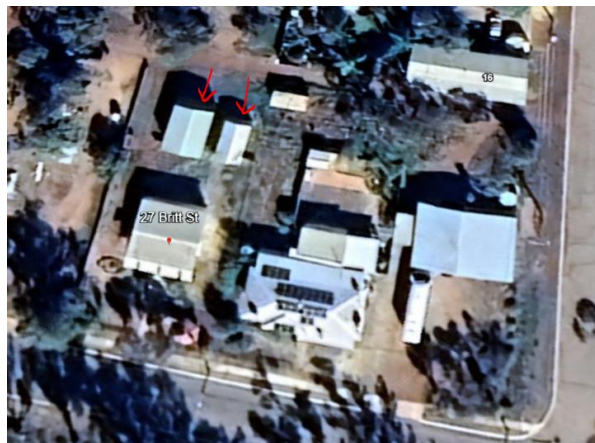
Against: Cr Hepworth



Above: Ex-St John Ambulance shed



Above: Ex-Bushfire Brigade shed



Above – Location of sheds

Statutory Environment

Local Government Act 1995

Policy Implications

Nil

Consultation

Elected Members

Paul Anderson – Chief Executive Officer

Marty Noordhof – Manager Infrastructure Services

Ally Bryant – Finance Manager

General Public

Financial Implications

Dependent on Council decision:

- Ongoing maintenance and insurance costs; or
- Potential expenditure of approximately \$6,500 for demolition of sheds.

Strategic Community Plan

Goal 2: Eco-friendly, attractive and well-maintained towns, surrounded by outstanding natural beauty, landscapes, flora and fauna to be protected and enjoyed.

2.3. The Shire's buildings and leases are administered to an appropriate standard for the benefit of the community according to their need and use.

Officer Comment

The sheds were decommissioned in 2016 after construction of a new shed which houses both emergency service vehicles, funded by contribution from Council, DFES, and the Mt Gibson Public Benefit Trust.

Following a call for public tenders for purchase of the sheds, there were no offers received.

Following publication of an Expression of Interest notice for community groups to utilise the sheds, only one response was received being from the Latham Progress Association who requested the use of both sheds, remaining on the current site, for the purpose of storing gardening equipment.

If Council resolve to permit the Latham Progress Association use of the sheds on the existing Council-owned site, Council will remain responsible for maintenance and insurance.

Since the previous reports to Council, one of the sheds has been utilised by a Shire Works Crew member for parking a Council-owned utility to protect it from the elements.

The particulars of the sheds are as follows:

1. Galvanised iron shed with concrete floor – 3.7m x 7.4m (2.7m high).
2. Colorbond shed with concrete floor – 6m x 9m (3.5m high).

The Manager Corporate Community Services, Nola Comerford, tabled a late memo dated 19 February 2025 regarding a verbal offer to purchase the sheds which had been received after the publication of the agenda. The Council noted the memo and considered the offer to purchase in their deliberations.

PROCEDURAL MOTION

Council Resolution Number: 200225.11

Moved: Cr Campbell

Seconded: Cr Bradford

That Council suspend Standing Orders, *Clause 9.5 Limitation on number of speeches*, at 3.13 pm to allow for open discussion of Item 14.1.

Motion put and carried 7/0

For: Cr Sutherland, Cr Hepworth, Cr Bryant, Cr Sparkman, Cr Bradford, Cr Fraser, Cr Campbell

Against: Nil

PROCEDURAL MOTION

Council Decision Number: 200225.12

Moved: Cr Fraser

Seconded: Cr Bradford

That Council reinstate Standing Orders at 3.25 pm.

Motion put and carried /0

For: Cr Sutherland, Cr Hepworth, Cr Bryant, Cr Sparkman, Cr Bradford, Cr Fraser, Cr Campbell

Against: Nil

OFFICER RECOMMENDATION

1. Resolve to demolish one/both of the sheds located at 27 Britt Street, Latham.

OR

2. Resolve to grant the Latham Progress Association permission to utilise one shed located at 27 Britt Street, Latham for the purpose of garden equipment storage and retain the other for staff parking of Council utility.

COUNCIL DECISION

Council Resolution Number: 200225.13

Moved: Cr Hepworth

Seconded: Cr Bradford

That Council resolve option:

2. Resolve to grant the Latham Progress Association permission to utilise one shed located at 27 Britt Street, Latham for the purpose of garden equipment storage and retain the other for staff parking of Council utility.

Motion put and carried 6/0

For: Cr Sutherland, Cr Hepworth, Cr Bryant, Cr Bradford, Cr Fraser, Cr Campbell

Against: Cr Sparkman

[Next Item](#)

14.2 RECOGNITION OF PREVIOUS RESIDENTS – K & I DELANE

Applicant:	Anne Marie Maccora
File:	ADM 0414
Date:	20 February 2025
Disclosure of Interest:	Nil
Voting Requirements:	Simple Majority
Author:	Nola Comerford – Manager Corporate and Community Services
Responsible Officer:	Paul Anderson – Chief Executive Officer
Attachments:	14.2.1 – Letter from Anne Marie Maccora

Summary

For Council to consider a request for naming of a space in the Bunjil townsite in recognition of past residents, Kevin and Irene Delane, who farmed in the Bunjil area.

Background

The attached correspondence was received from Anne Marie Maccora, the daughter of Kevin and Irene Delane. Mrs Maccora has requested that Council acknowledge recognition of her parent's history in the Bunjil region in the form of a shaded table and seating within the Bunjil townsite.

Statutory Environment

Local Government Act 1995

Policy and Standards for Geographical Naming in Western Australia

Policy Implications

Nil

Consultation

Paul Anderson – Chief Executive Officer

Financial Implications

Expenditure of approximately \$8,500 (eight thousand, five hundred dollars) to supply and install shelter and seating combination (an additional expense of approximately \$4,000 [four thousand dollars] for construction of a concrete pad, if required) would be allocated in the 2025/26 Annual Budget.

A road name change would include a service charge of \$295 (two hundred and ninety-five dollars) to Landgate if a name change was approved, plus supply and installation of new road signage.

Strategic Community Plan

Goal 4: A strong and diverse Council working closely with the proactive and involved community.

4.6. The organisation, assets and finances of the Shire are managed responsibly.

Officer Comment

The Delane family have requested the installation of sheltered seating to provide a rest area in acknowledgement of Kevin and Irene Delane who farmed in the Bunjil area.

If Council resolves to support this request, it sets a precedent that could open applications from previous residents for varied methods in honouring their family history in a public manner.

The request for installation of sheltered seating to provide a rest area dedicated to Kevin and Irene Delane would presumably be at Council's expense, there being no indication in Mrs Maccora's letter that the Delane family would contribute to the cost. The cost of supply and installation of sheltered seating would be approximately \$8,500 and an additional \$4,000 if a concrete pad was required. A commemorative plaque would cost approximately \$400.



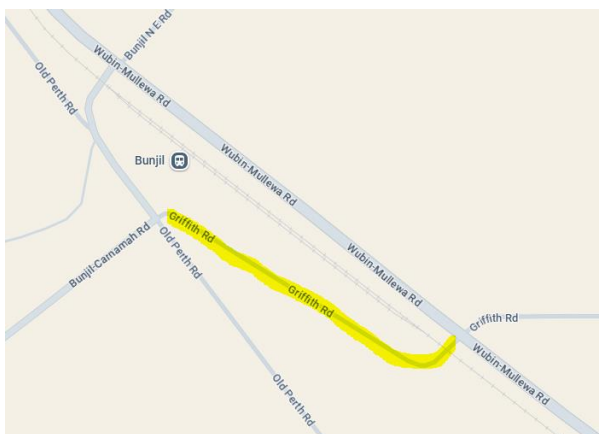
Above: Potential option for shaded seating valued at \$5,553

The Past and Present Residents' Wall in the Perenjori townsite has been utilised to honour and acknowledge residents who have made a contribution to the district. There are currently two plaques on the wall recording the Delane family, which is a cost-effective manner in which Kevin and Irene Delane could be acknowledged at a cost of \$350, plus installation.



Above: Two plaques erected at the Residents' Wall in acknowledgement of the Delane families

Mrs Maccora has not suggested a road name change, but Council may consider this alternative which has previously been utilised as a way of recognising individual and family contribution to the district. One option is to rename a small section (0.7km) of Griffith Road, which links the Wubin-Mullewa Road and Bunjil-Carnamah Road behind the Bunjil CBH Receival Point. Griffith Road extends 15.5 kilometres in an eastern direction from the Bunjil townsite and would be impacted by a 0.7km decrease in length.



Above: Portion of Griffith Road – approximately 0.7 kilometre



Above: Google Earth image of potential road name change section

Any change in road names is subject to approval from the WA Geographical Names Board.

The Delane family would be required to seek community support for a road name change application, including landowners bordering the section of road who may be impacted by a change of road name.

An excerpt from the Policies and Standards for Geographical Naming in Western Australia:

1.13 Unnecessary name changes

Any change to an existing approved name is discouraged. As all official names are meant to be enduring, any proposals to change the name of a feature, administrative boundary or road must include evidence of a compelling reason for a change, for example the proposal is deemed to be in the public interest for safety reasons.

The changing of a name merely to correct or re-establish historical usage shall not be considered in itself a reason to change a name.

Evidence of community consultation and feedback from all relevant agencies and jurisdictions must be included with the submission. All such requests will be required to be presented to Landgate for consideration.

If Landgate determines that the name change is unnecessary or non-essential, yet there is enough community support for the change and it is compliant with the relevant naming policies and standards, it may be approved but it shall incur a service charge.

2.4 Naming amendments

Road names are intended to be enduring. The renaming of any road is discouraged unless there are good reasons for a change of name.

Reasons that may be considered in support of a name change are:

- *Redesign of a road layout*
- *Changed traffic flow*
- *Mail delivery problems*
- *The misspelling of a name in the original application*
- *Name duplication issues*
- *Property street addressing issues*

Renaming shall be necessary when a road is made into a cul-de-sac, resulting in two or more separated sections of road. Such separations can cause difficulties for emergency service responders and the delivery of other services to the area. The renaming of a portion of separated road may also be used to solve address numbering problems.

Where a change to the name of a road is proposed, the new name selected shall conform to all the necessary naming policies and standards.

For regional roads the change of name must have broad community support, and for local roads, there must be majority support from the affected land owners and residents.

The requirements of emergency service responders for clear, unambiguous road naming shall also be a consideration. Proposals normally require the support of local government, but the Minister is the final authority in all such matters. Submissions for road name changes deemed to be non-essential or unnecessary shall incur a service charge.

PROCEDURAL MOTION

Council Resolution Number: 200225.14

Moved: Cr Sparkman

Seconded: Cr Bradford

That Council suspend Standing Orders, *Clause 9.5 Limitation on number of speeches*, at 3.26 pm to allow for open discussion of Item 14.2

Motion put and carried 7/0

For: Cr Sutherland, Cr Hepworth, Cr Bryant, Cr Sparkman, Cr Bradford, Cr Fraser, Cr Campbell

Against: Nil

PROCEDURAL MOTION

Council Decision Number: 200225.15

Moved: Cr Campbell

Seconded: Cr Bryant

That Council reinstate Standing Orders at 3.32 pm.

Motion put and carried 7/0

For: Cr Sutherland, Cr Hepworth, Cr Bryant, Cr Sparkman, Cr Bradford, Cr Fraser, Cr Campbell

Against: Nil

OFFICER RECOMMENDATION

1. **Allocate funds in the 2025/26 Annual Budget for the supply and installation of sheltered seating within the Bunjil townsite, complete with a plaque commemorating Kevin and Irene Delane's contribution to the community to a maximum total of \$_____.**

OR

2. **Advise the Delane family that Council are supportive of the Delane family's request to install a commemorative rest-stop in the Bunjil townsite which would need to be at the Delane family's expense and include consultation with the Chief Executive Officer on a suitable location.**

OR

3. **Decline the request and recommend to the Delane family that they seek community support in changing the name of a road which is relevant to the Delane family history.**

OR

4. **Decline the request and recommend that Kevin and Irene Delane be acknowledged by installation of a plaque on the Past and Present Residents' Wall in the Perenjori townsite.**

COUNCIL DECISION

Council Resolution Number: 200225.16

Moved: Cr Sparkman

Seconded: Cr Hepworth

That the item be laid on the table to allow further consultation with the community and enable administration time to investigate further options and costings.

Motion put and carried 7/0

For: Cr Sutherland, Cr Hepworth, Cr Bryant, Cr Sparkman, Cr Bradford, Cr Fraser, Cr Campbell

Against: Nil

[Next Item](#)

15. Governance:

15.1 ANNUAL REPORT AMENDMENTS

Applicant:	Shire of Perenjori
File:	ADM 0339
Date:	20 February 2025
Disclosure of Interest:	Nil
Voting Requirements:	Simple Majority
Author:	Nola Comerford – Manager Corporate and Community Services
Responsible Officer:	Paul Anderson – Chief Executive Officer
Attachments:	15.1.1 - Annual Report 2023/24

Summary

For Council to note Auditor amendments to the 2023/24 Annual Report.

Background

Following adoption of the 2023/24 Annual Report at the December Ordinary Council Meeting, the Office of the Auditor General (OAG) advised that a mandatory review had been undertaken on the Annual Report to check if there were any inconsistencies with the audited report. The review noted that some figures did not agree with the audited financial statements and requested these be updated accordingly to report the correct figures.

Council Resolution Number: 191224.11

Moved: Cr Fraser

Seconded: Cr Bryant

The Audit Committee recommends that Council:

1. Accepts the 2023/24 Annual Report containing the Annual Financial Report and Auditor's Report for the financial year ending 30 June 2024.
2. Adopts the 2023/24 Annual Report, containing the Annual Financial Report and the Auditor's Report for the financial year ending 30 June 2024.
3. Confirms the date for the Annual Electors Meeting to be held at 5.00 pm on Thursday 13 February 2025 at the Latham Community Centre and authorise the statutory advertising.

Motion put and carried 7/0 by Absolute Majority

For: Cr Sutherland, Cr Hepworth, Cr Bryant, Cr Sparkman, Cr Bradford, Cr Fraser, Cr Campbell

Against: Nil

Statutory Environment

Local Government Act, 1995.

Section 5.27(1) states: A general meeting of electors of a district is to be once every financial year.

Section 5.27(2) states: A general meeting is to be held on a day selected by the Local Government but not more than 56 days after the Local Government accepts the Annual Report for the previous financial year.

Section 5.29(1) states: The CEO is to convene an elector's meeting by giving –

(a) at least 14 days local public notice; and

(b) each council member at least 14 days' notice of the date, time, place and purpose of the meeting.

Acceptance of annual reports 5.54.

(1) Subject to subsection (2), the annual report for a financial year is to be accepted by the local government no later than 31 December after the financial year.

(2) If the auditor's report is not available in time for the annual report to be accepted by 31 December after that financial year, the annual report is to be accepted by the local government no later than two months after the auditor's report becomes available.

Policy Implications

Nil

Consultation

Paul Anderson – Chief Executive Officer

Ally Bryant – Finance Manager

Bianca Plug – Governance Officer

Financial Implications

Nil

Strategic Community Plan

Goal 4: A strong and diverse Council working closely with the proactive and involved community

4.1. The community is well-informed and engaged

4.6. The organisation, assets and finances of the Shire are managed responsibly

Officer Comment

Amendments to page 8 of the 2023/24 Annual Report have been implemented as per the OAG's advice. A summary of these changes are as follows:

- Changes to wording in first paragraph to reflect a surplus 'after imposition of general rates'.
- 'Net current assets' figure amended to \$5.994 million.
- 'Non-operating grants' received in 2022-23 amended to \$2.287 million.
- 'Other revenue items', consisting of fees and charges, interest and other revenue amended to \$1.674 million.
- Amendment of wording for Total Debt to reflect \$1.000 million for building of Shire infrastructure.
- Operating Expenditure reference repayment of caravans supplied by DFES following Cyclone Seroja.

COUNCIL DECISION

Council Resolution Number: 200225.17

Moved: Cr Fraser

Seconded: Cr Sparkman

That Council note the amendments to the 2023/24 Annual Report.

Motion put and carried 7/0

For: Cr Sutherland, Cr Hepworth, Cr Bryant, Cr Sparkman, Cr Bradford, Cr Fraser, Cr Campbell

Against: Nil

[Next Item](#)

15.2 ANNUAL ELECTORS MEETING – 13 FEBRUARY 2025

Applicant:	Shire of Perenjori
File:	ADM 0381
Date:	20 February 2025
Disclosure of Interest:	Nil
Voting Requirements:	Simple Majority
Author:	Bianca Plug – Governance & Executive Support Officer
Responsible Officer:	Paul Anderson – Chief Executive Officer
Attachments:	15.2.1 – Annual Electors Meeting Minutes held 13 February 2025

Summary

The Annual Electors Meeting was held on 13 February 2025 at the Latham Community Centre in accordance with the *Local Government Act 1995*.

Background

A local government is required to prepare an Annual Report each year which provides an overview of the operations, activities and major projects undertaken by the Shire for the period. It also includes major initiatives that are proposed to commence or continue in the next financial year.

Once an Annual Report has been accepted by Council, an Annual Electors Meeting (AEM) must be held within 56 days. As prescribed by *Regulation 15 of the Local Government (Administration) Regulations 1996*, the purpose of the AEM is to discuss the annual report for the previous financial year and any other general business. The AEM presents an opportunity for electors to ask questions of Council and propose motions (recommendations). Electors present are asked to vote on proposed motions. Motions that are carried are considered by Councillors at the next appropriate Ordinary Meeting of Council.

Statutory Environment

Local Government Act 1995

5.33. Decisions made at electors' meetings

(1) All decisions made at an electors' meeting are to be considered at the next ordinary council meeting or, if that is not practicable —

- (a) at the first ordinary council meeting after that meeting; or
- (b) at a special meeting called for that purpose, whichever happens first.

(2) If at a meeting of the council a local government makes a decision in response to a decision made at an electors' meeting, the reasons for the decision are to be recorded in the minutes of the council meeting.

Policy Implications

Nil

Consultation

Paul Anderson – Chief Executive Officer
Elected Members

Financial Implications

Nil

Strategic Community Plan

Goal 4: A strong and diverse Council working closely with the proactive and involved community.

4.2 The Shire listens to and works closely with the community and its decision-making is transparent and accountable.

Officer Comment

In accordance with the *Local Government Act 1995* the Council is required to consider the motions that are carried at the Annual Electors Meetings. At the AEM held on 13 February 2025, there was only one resolution which was the receiving of the Annual Report of the Shire of Perenjori, for the year ending 30 June 2024.

There were no motions from the meeting that were put forward for Council consideration.

COUNCIL DECISION

Council Resolution Number: 200225.18

Moved: Cr Bradford

Seconded: Cr Bryant

That Council:

- 1. Note the Minutes of the Annual Electors Meeting held on 13 February 2025 and;**
- 2. Note that there are no decisions to be considered from the Minutes of the Annual Electors Meeting.**

Motion put and carried 7/0

For: Cr Sutherland, Cr Hepworth, Cr Bryant, Cr Sparkman, Cr Bradford, Cr Fraser, Cr Campbell

Against: Nil

[Next Item](#)

15.3 LOCAL GOVERNMENT ORDINARY ELECTION 2025

Applicant:	Shire of Perenjori
File:	ADM 0886
Date:	20 February 2025
Disclosure of Interest:	Nil
Voting Requirements:	Absolute Majority
Author:	Bianca Plug – Governance Officer
Responsible Officer:	Paul Anderson – Chief Executive Officer
Attachments:	15.3.1 – WA Electoral Commission Cost Estimate Letter 2025

Summary

For Council to consider the appointment of the Western Australian Electoral Commissioner to conduct the 2025 ordinary local government elections in the Shire of Perenjori, and the method by which the election is to be conducted.

Background

The next ordinary local government elections are scheduled for 18 October 2025, with three positions up for re-election. Under the *Local Government Act 1995* (the Act), Council may determine the method by which the election is conducted. That is by postal election, or as a voting in-person election. The Act provides the following definitions:

- postal election an election at which the method of casting votes is by posting or delivering them to an electoral officer on or before election day; or
- voting in person election an election at which the principal method of casting votes is by voting in person on election day but at which votes can also be cast in person before election day, or posted or delivered, in accordance with regulations.

Council must also determine who is to be appointed returning officer, responsible for conducting the election. Section 4.20 of the Act provides that in the event that a postal election is decided upon as the method of election, Council must appoint the Western Australian Electoral Commissioner to take responsibility for conducting the election. Should Council determine to hold an in-person vote, the election may be conducted by the Electoral Commissioner, the Chief Executive Officer or some other approved person.

An absolute majority decision of Council is required to appoint the WA Electoral Commission to conduct the poll by postal voting.

Statutory Environment

Local Government Act 1995

Local Government (Elections) Regulations 1997

Policy Implications

Nil

Consultation

Western Australian Electoral Commission

Financial Implications

The Commission has estimated the cost to conduct the Shire's election in 2025 as a postal election at approximately \$10,766 (ex GST). This cost has been based on the following assumptions:

- The method of the election will be postal;
- 3 Councillor(s) vacancies;
- 400 electors;
- response rate of approximately 60%
- appointment of a local Returning Officer; and
- count to be conducted at the Shire of Perenjori administration office using CountWA.

This is an estimate provided by the Commission and may vary depending on a range of factors. An allocation will be included in the 2025/26 annual budget to meet all election costs inclusive of advertising, WA Electoral Commission charges and incidentals.

Strategic Community Plan

Goal 4: A strong and diverse Council working closely with the proactive and involved community

4.1. The community is well-informed and engaged

4.2. The Shire listens to and works closely with the community and its decision-making is transparent and accountable

4.3. The Shire advocates and partners effectively on behalf of the community

Officer Comment

Each local government can choose to conduct an election as a postal election in preference to a voting 'in person' election. All postal elections must be conducted by the Western Australian Electoral Commission (WAEC). Postal voting is more expensive but removes a lot of the administrative requirements usually performed by the local government. Council has conducted postal elections since 2020, under the control of the Electoral Commission.

A postal voting election package is sent to all electors on the local government's roll automatically. The package can be sent as soon as practicable after the close of nominations.

The postal voting package contains:

- voting instructions;
- candidate profiles (reproduced in such form as the returning officer determines);
- a ballot paper;
- a ballot paper envelope;
- an elector's certificate and;
- a pre-paid envelope for return of the papers to the returning officer.

Completed postal voting papers can be checked by the returning officer at any time prior to the close of polling, and candidates may be present.

The officer recommendation presented is based on past Council decisions to conduct elections by postal vote. There are several advantages to appointing the WA Electoral Commission to conduct the election using postal voting:

- The Electoral Commissioner appoints the returning officer. Shire employees are still involved in helping electors with replacement ballot papers, with the independent Returning Officer providing the training and guidance as required;
- removes any perception of bias if the election is run by the Chief Executive Officer;
- every elector receives the ballot papers with clear instructions including a profile of each candidate;
- there is no inconvenience on election day if events occur that distract electors from attending the polling place/s
- absentee owners that are registered on the Shire's electoral roll receive the ballot papers;
- electors have the convenience of time to deliver the completed ballot paper to the post office or the Shire office and;
- candidates' profiles are delivered to every elector with their voting package.

COUNCIL DECISION

Council Resolution Number: 200225.19

Moved: Cr Bradford

Seconded: Cr Hepworth

That Council:

1. Pursuant to section 4.20(4) of the *Local Government Act 1995*, declares the Electoral Commissioner to be responsible for the conduct of the 2025 ordinary elections together with any other elections or polls which may be required for the Shire of Perenjori and;
2. Pursuant to section 4.61(2) of the *Local Government Act 1995*, determines that the method for conducting the 2025 ordinary elections for the Shire of Perenjori is by way of a postal election.

Motion put and carried 7/0 by Absolute Majority

For: Cr Sutherland, Cr Hepworth, Cr Bryant, Cr Sparkman, Cr Bradford, Cr Fraser, Cr Campbell

Against: Nil

[Next Item](#)

15.4 PROPOSED TREE FARM – LOT 4754 SOLOMON RD & LOT 1 PERENJORI-ROTHSAY RD, BOWGADA

Applicant:	Carbon Neutral Pty Ltd for D&D Farming Pty Ltd
File:	A64 / A694
Date:	20 February 2025
Disclosure of Interest:	Nil
Voting Requirements:	Simple Majority
Author:	Simon Lancaster - Planning Advisor
Responsible Officer:	Paul Anderson – Chief Executive Officer
Attachments:	15.4.1 – Tree Farm Application

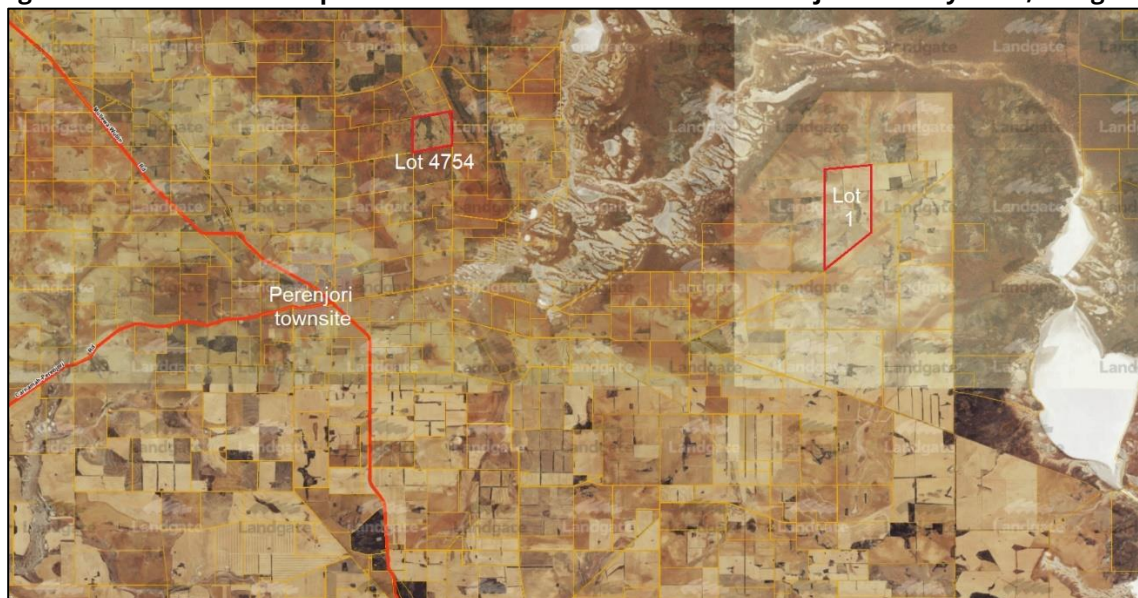
Summary

Council is in receipt of an application from Carbon Neutral to revegetate Lot 4754 Solomon Road and Lot 1 Perenjori-Rothsay Road, Bowgada. This report recommends conditional approval of the application.

Background

Lot 4754 Solomon Road, Bowgada is located 8km north-east of the Perenjori townsite and Lot 1 Perenjori-Rothsay Road is located 25km east of the Perenjori townsite.

Figure 15.4.2 – Location map of Lot 4754 Solomon Road & Lot 1 Perenjori-Rothsay Road, Bowgada



Lot 4754 is 404.0932ha in area and contains approximately 130ha (32%) of remnant vegetation on outcrops and along tributary lines and contour banks generally comprising York and Salmon gums with the remaining area cleared and used for farming purposes. The applicant proposed to revegetate the remaining cleared 275ha (68%) of Lot 4754 with mallee plantings.

Lot 1 is 1,215.6311ha in area and predominantly cleared and used for farming purposes with pockets of remnant vegetation on outcrops generally comprising wattle, casuarina and teatree for approximately 156ha (13%) of the property land area. The applicant proposed to replant 865ha (71%) of Lot 1 with a variety of Eucalyptus, Acacia and Allocasuarina species. An approximate 193ha (16%) red loam area in the south-eastern corner of Lot 1 would be retained for farming purposes.

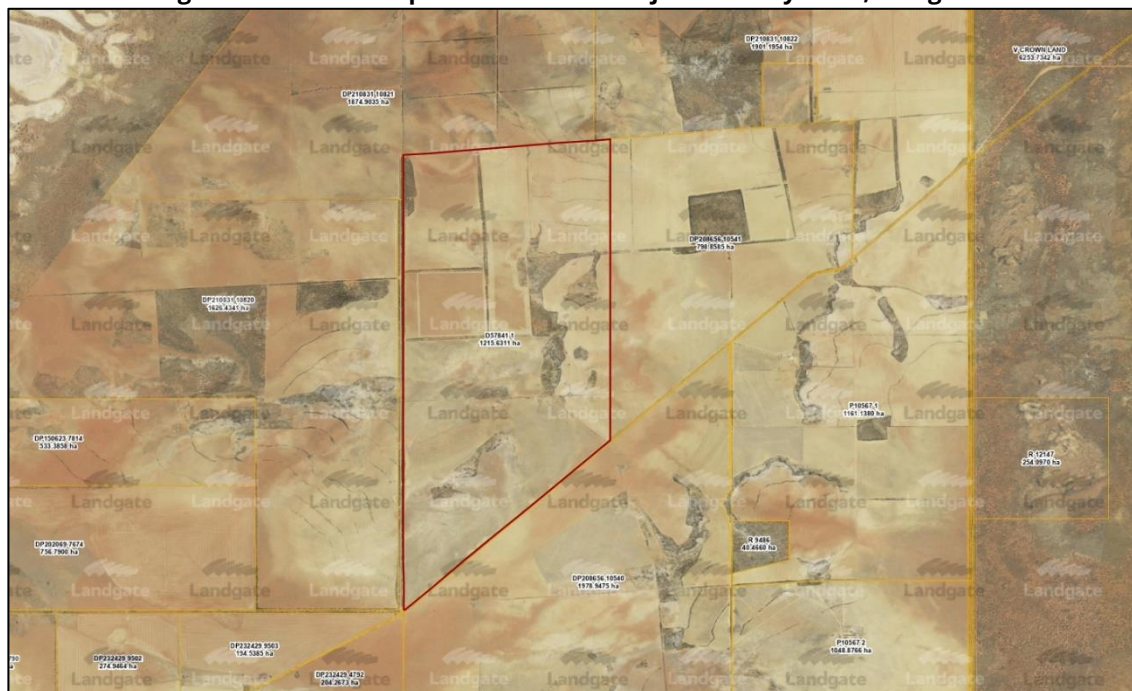
Planting would be undertaken using a combination of direct seeding and seedling establishment, with it anticipated by the 3rd year of revegetation that managed grazing could commence on the replanted portions of both properties.

A copy of the application is provided as **separate Attachment 15.4.1**.

Figure 15.4.3 – Aerial photo of Lot 4754 Solomon Road, Bowgada



Figure 15.4.4 – Aerial photo of Lot 1 Perenjori-Rothsay Road, Bowgada



In support of their proposal the applicant has noted that:

"Carbon Neutral understands the general concern Shires have over declining population bases and loss of agricultural production from a range of causes. It is Carbon Neutrals philosophy to contribute positively to addressing these issues by facilitating ongoing agricultural production wherever appropriate and sensible on its tree farms, and offering alternative income streams to contribute to local communities.

The proposal for Farmstrong incorporates ongoing agricultural production but is essentially a 'whole of Lot' application for a tree farm. Carbon Neutral is seeking approval of this proposal under Clause 8.1.1 ("Variations") of the Policy and has engaged an independent consultant to provide a land capability assessment for the Shire's consideration.

This independent assessment is attached."

Figure 15.4.5 – Proposed Planting and Fire Management Plan for Lot 4754 Solomon Road & Lot 1 Perenjori-Rothsay Road, Bowgada

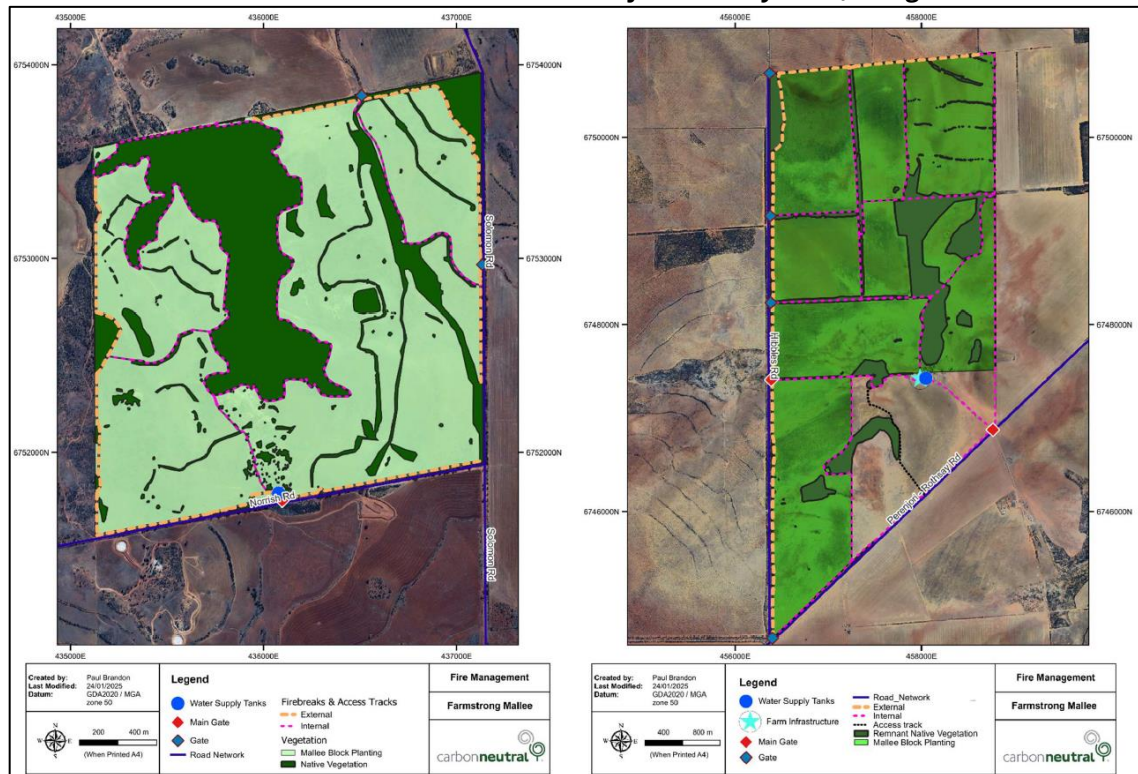
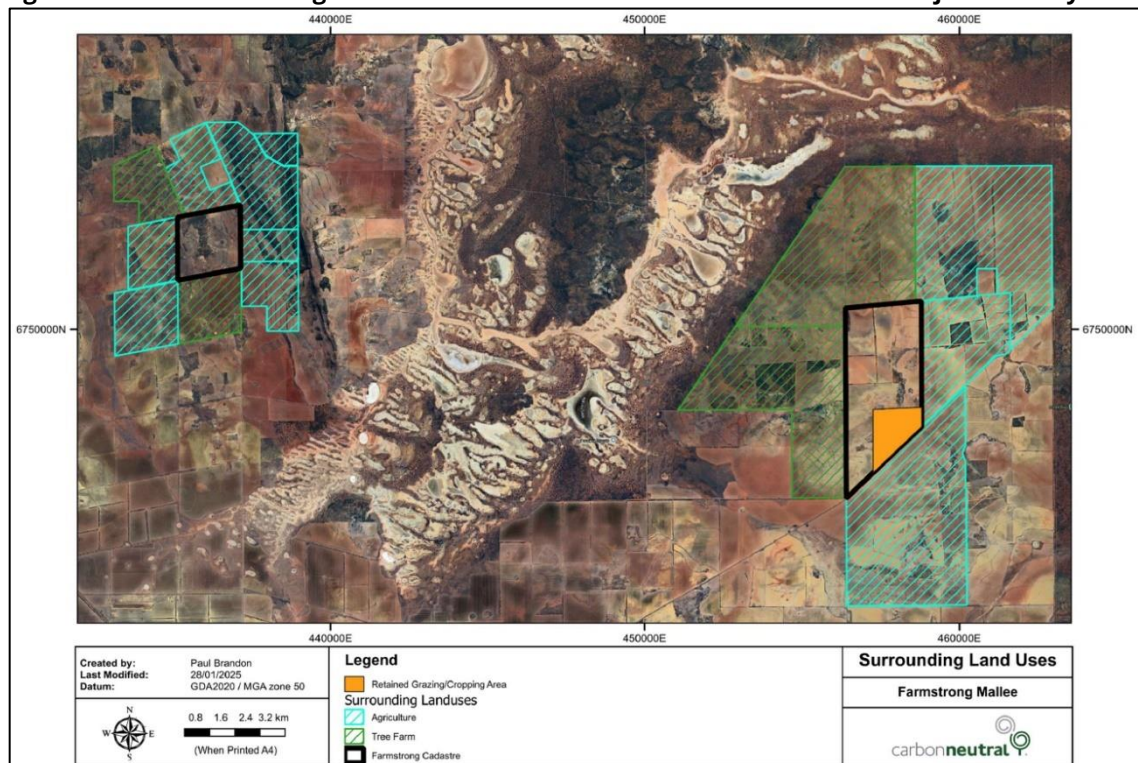


Figure 15.4.6 – Surrounding Land Uses to Lot 4754 Solomon Road & Lot 1 Perenjori-Rothsay Road



Statutory Environment

Lot 4754 Solomon Road & Lot 1 Perenjori-Rothsay Road, Bowgada are zoned 'Rural' under the Shire of Perenjori Local Planning Scheme No.3 ('the Scheme') which lists the objectives for this zone as being:

- To provide for the maintenance or enhancement of specific local rural character.
- To protect broad acre agricultural activities such as cropping and grazing and intensive uses such as horticulture as primary uses, with other rural pursuits and rural industries as secondary uses in circumstances where they demonstrate compatibility with the primary use.

- *To maintain and enhance the environmental qualities of the landscape, vegetation, soils and water bodies, to protect sensitive areas especially the natural valley and watercourse systems from damage.*
- *To provide for the operation and development of existing, future and potential rural land uses by limiting the introduction of sensitive land uses in the Rural zone.*
- *To provide for a range of non-rural land uses where they have demonstrated benefit and are compatible with surrounding rural uses."*

This proposal would meet the definition of a 'Tree Farm' which is defined in the *Planning and Development (Local Planning Schemes) Regulations 2015* as being: *"means land used commercially for tree production where trees are planted in blocks of more than one hectare, including land in respect of which a carbon right is registered under the Carbon Rights Act 2003 section 5"*

'Tree Farm' is listed as a 'D' use in the Scheme Zoning Table for the 'Rural' zone which *"means that the use is not permitted unless the local government has exercised its discretion by granting development approval"*.

Schedule 2 Part 9 Clause 67 of the *Planning and Development (Local Planning Schemes) Regulations 2015* lists the following relevant matters to be considered by local government in considering a development application:

- "(a) the aims and provisions of this Scheme and any other local planning scheme operating within the Scheme area;...*
- ...(e) any approved State Planning Policy;*
- (f) any policy of the Commission;*
- (fa) any local planning strategy for this Scheme endorsed by the Commission;*
- (g) any local planning policy for the Scheme area;...*
- ...(m) the compatibility of the development with its setting, including –*
 - (i) the compatibility of the development with the desired future character of its setting; and*
 - (ii) the relationship of the development to development on adjoining land or on other land in the locality including, but not limited to, the likely effect of the height, bulk, scale, orientation and appearance of the development;*
- (n) the amenity of the locality including the following —*
 - (i) environmental impacts of the development;*
 - (ii) the character of the locality;*
 - (iii) social impacts of the development;*
- (o) the likely effect of the development on the natural environment or water resources and any means that are proposed to protect or to mitigate impacts on the natural environment or the water resource;...*
- ...(q) the suitability of the land for the development taking into account the possible risk of flooding, tidal inundation, subsidence, landslip, bush fire, soil erosion, land degradation or any other risk;*
- (r) the suitability of the land for the development taking into account the possible risk to human health or safety;...*
- ... (w) the history of the site where the development is to be located;*
- (x) the impact of the development on the community as a whole notwithstanding the impact of the development on particular individuals;...*
- ...(zb) any other planning consideration the local government considers appropriate."*

Policy Implications

The Shire adopted its Tree Farm Local Planning Policy at its 21 March 2024 meeting to assist applicants by providing general guidelines and a clear outline of the Shire's position and to assist Council in determining applications for tree farms by setting out matters to have regard for when assessing applications.

A copy of the Tree Farm Local Planning Policy can be viewed at the following link:

[shire-of-perenjori-local-planning-policy-no-1-tree-farms](#)

The policy has the following objectives:

- *To actively encourage the integration of tree farms with existing agricultural uses as a complementary but integrated use. Traditional agricultural activities such as cropping, grazing, and food production should generally remain the predominant use.*
- *To actively oppose the use of an entire lot area for tree farms (particularly where it contains a dwelling) unless the applicant has clearly demonstrated extenuating circumstances or provided significant justification warranting support for a variation to any aspect of this Policy.*
- *To support applications that actively integrate tree farms with existing agricultural activities on lots in recognition of the economic, environmental and social benefits.*

- *To encourage planting areas with linkages to existing remnant vegetation on the same lot or adjacent lots. Where appropriate encourage linkages with vegetation on adjacent reserves however consult with the relevant reserve authority or manager.*
- *To protect and enhance native vegetation, wetlands and water courses and assist in the reduction of salinity, waterlogging and erosion.*
- *To support continuing broad acre agriculture and production as the primary and priority land use in the Rural zone, consistent with the planning framework's underlying theme to protect productive agricultural land.*
- *To minimise the potential for any loss of population or agricultural land through the use of the entire lot area for tree farms, and encourage tree farms that provide a supplementary income to farmers.*
- *To achieve tree farm layouts which do not compromise the fire safety of the local community or the biodiversity conservation and management of reserves.*
- *To achieve high quality bushfire management plans which are independent and self-sufficient.*
- *To encourage the selection of tree species that are complementary to native remnant vegetation and will assist in maintaining landscape function."*

The policy outlines the need for planning controls with regards to tree farms noting that:

"With new emerging tree farms for carbon sequestration, Council is concerned over the potential for these to be developed on whole lots. Council is particular concerned over the accumulative impact of tree farms on entire lots over time, and the potential for population loss and a reduction of food production.

Some applicants may propose long term harvesting whilst other may not nominate to harvest at all (means the land may never revert back to traditional agricultural uses.)

Whilst some of the issues are difficult to quantify, this policy aims to encourage an integrated approach of plantings with more traditional farming or agricultural uses.

The Policy recognises that the development of whole lots for tree farms discourages the continuation of broad hectare farming in a way the development of a tree farm at a smaller scale would not."

Approximately 32% of Lot 4754 is remnant vegetation and the applicant proposes to revegetate the previously cleared/currently farmed 68% remaining portion of Lot 4754 in its entirety.

Approximately 13% of Lot 1 is remnant vegetation and the applicant proposes to revegetate 71% of Lot 1 that is previously cleared/currently farmed and retain 16% for farming.

This application exceeds Council's Policy criteria that establishes a general maximum planting area of 30-35% of the property and has therefor been presented to Council for determination rather than be determined by Shire staff under delegated authority.

"8.0 Policy Requirements

In assessing any application for a tree farm Council will have regard for the following matters;

8.1 Continuing Agricultural Activities

Traditional agricultural activities such as cropping, grazing and food production should generally remain the predominant land use with tree farms as an ancillary integrated and complementary use.

This Policy does not attempt to introduce maximum planting areas by means of a percentage of the lot area, simply because some flexibility needs to be afforded.

As a general rule the Shire will assess if a tree farm area is 'ancillary' having regard for;

- 1 The area of each lot;*
- 2 The proportion or percentage of the tree farm area on a lot by lot basis. Generally a 30-35% maximum planting area is construed as ancillary although each application will be assessed on its merits on a 'case by case' basis;*
- 3 The extent of existing remnant vegetation areas; and*
- 4 Whether a significant portion of each lot can continue to be used for traditional extensive agriculture or for food production.*

Council will be able to clearly identify whether agriculture remains as the primary use, simply because detailed site plans are lodged for all applications and show the extent of planting areas. This Policy aims to actively encourage the integration of tree farms with traditional agricultural farming activities. Whilst there are other factors contributing towards reductions of rural population, Council wishes to ensure that new land uses do not exacerbate an existing ongoing problem.

Council will not generally support the planting of whole lots for tree farms due to the potential for the displacement of agricultural pursuits and loss of agricultural land.

There is a general presumption against 'whole of lot' tree farms in the Rural zone, unless a proponent can demonstrate to the Council's satisfaction that there are exceptional circumstances that warrant a variation in accordance with Clause 8.1.1.

Council considers that the development of whole lots with tree farms is inconsistent with the Scheme's objective to protect broad acre agricultural activities in the Rural zone as:

- (a) Tree farms may not be harvested except in the long term, or in the case of carbon sequestration plantings, not at all;*
- (b) The concept of broad hectare farming primarily contemplates the continuation of traditional productive agricultural activities;*
- (c) The use of an entire lot for a tree farm to the exclusion of traditional productive agricultural activities does not involve an appropriate integration of rural land uses, but instead runs the risk that the entire lot will never be returned to productive use, and at the very least involves a diminution of the productive capacity of the land;*
- (d) Any proposed whole lot tree farm discourages the continuation of broad hectare farming in a way the development of a tree farm at a smaller scale would not. Integrated ancillary plantings consistent with this Policy serve to encourage the retention of productive agricultural activities whereas whole lot plantings have the opposite effect;*
- (e) Tree farms that are not well managed or have no on site manager (with absentee ownership) or are abandoned, can cause significant long term land use conflict with surrounding broad acre farming or intensive agricultural activities due to increased bushfire risk, lack of fire break maintenance, lack of general property maintenance, uncontrolled weeds, and uncontrolled vermin or feral animals, including wild pigs.*

8.1.1 Variations

In dealing with this matter, Council will have regard for any specific circumstances where a variation to this criteria 8.1.1 may be warranted, however in those cases the onus will be on the applicant to demonstrate the particular merits of a proposal or justification for an assessment on a 'whole lot' basis.

If an applicant seeks any variation or support for a tree farm on a 'whole lot' basis then the application should be accompanied by justification such as;

- Comprehensive Land Capability Studies specific to the lot by a suitably qualified environmental consultant;*
- Farm Management Plan;*
- Site specific soil or water test results demonstrating areas not suitable for agricultural food production;*
- Photographs clearly showing shallow soils or rocky areas which visually demonstrate that areas on a lot are not suitable for traditional agriculture. A site plan should be provided that numbers each photograph, and shows the direction that the photograph was taken.*
- Demonstration that planting areas will have specific local environmental benefits for the lot(s) subject of the application (refer 8.3). For example, larger planting areas may be warranted as native vegetation block planting adjacent to a creek line, or for salinity prone areas.*

Information and justification for a variation to this Policy needs to rely on site specific studies and information. General broad land capability mapping is not by itself considered to be sufficient as justification for a variation of this Policy."

The received application, provided as **separate Attachment 15.4.1**, includes property assessments from Farmanco Management Consultants.

The assessment of Lot 4754 Solomon Road, Bowgada includes the following recommendation:

"Soils were checked for soil type and drilled (with a long Tungsten drill bit) to check as well.

The land was once farmed in a conventional sense for cropping and most likely livestock. There is one equipped water supply and a small tank. No infrastructure. There is no evidence of recent farming cultivation or planting of a crop on the farm. It may have been cropped a few times in the last twenty years, (but without substantive satellite imagery that is an estimate)...

...The majority of this farm is not suited to profitable cropping or grazing currently. With trees planted it will be suited to low density grazing after trees are established. (If it is fenced and water supplies are upgraded).

My recommendation is that this farm be planted with trees. My view is that it should be ripped (as has significant areas of shallow soil types) with a dozer or grader to a depth of 500 to 750mm to improve

the long-term success of the tree planting and improve growth rates of trees planted. Once the trees are established it will be suitable to have sheep livestock run on it at low stocking rates or rotationally grazed with adjoining properties. This would keep weeds and grass levels down to low levels. This would also help maintain the boundary fence and water supply as well as ensure the land is productive with multiple land use for the longer term."

The assessment of Lot 1 Perenjori-Rothsay Road, Bowgada includes the following recommendation:

"This property has been actively farmed recently. The fences are in very good condition and there is water piped to all paddocks. The country near the house was cropped in 2024.

This farm has predominantly lighter soil types which are most likely acidic. There are significant amounts of poverty bush on this farm.

The majority of this farm is not suited to profitable cropping or grazing currently. It would need very high uneconomic rates of lime sand and soil amelioration to enable plant roots to access soil moisture. My recommendation is that this farm be planted with trees. My view is that lighter soils should be ripped with a tree planter. Shallower soils would ideally be ripped with a dozer or grader to a depth of 500 to 750mm to improve the long-term success of the tree planting and improve growth rates of trees planted. Once the trees are established it will be suitable to have sheep livestock run on it at low to medium stocking rates or rotationally grazed with adjoining properties. This would keep weeds and grass levels down to low levels. This would also help maintain the boundary and internal fences and water supply as well as ensure the land is productive with multiple land use for the longer term."

Strategic Implications

The Western Australian Planning Commission's State Planning Policy 2.5 - Rural Planning notes the following in relation to applications of this nature:

"Tree farming is an umbrella term used to describe the planting of trees to generate economic return and/or environmental benefits. It has been a rapidly emerging industry in a number of rural locations across the State. Usually this has involved the planting of trees for harvest. However, more recently the planting of trees for carbon sequestration has emerged as a new rural land use. Tree farming which involves harvesting is a primary production activity that also sequesters carbon. The different types of tree farms i.e. integrated, chip logs or saw logs, require varied planning approaches.

WAPC policy in regard to tree farming is:

- (a) tree farming is supported and encouraged on rural land as a means of diversifying rural economies and providing economic and environmental benefit;*
- (b) tree farming should generally not occur on priority agricultural land;*
- (c) tree farming should generally be a permitted use on rural land, except where development of a treefarm would create an extreme or unacceptable bushfire risk or when responding to specific local circumstances as identified in a strategy or scheme;*
- (d) local govts should manage the location, extent and application requirements for tree farming in their communities through local planning strategies, schemes and/or local planning policies;*
- (e) in planning for tree farming, local government considerations should include but are not limited to, potential bushfire risk, environmental and economic factors, water availability and recharge, visual landscape impacts, transport impacts of tree farming (where harvesting is proposed), planting thresholds, appropriate buffers, and location relative to conservation estates and sensitive land uses;*
- (f) where tree farm proposals are integrated with farm management for the purpose of natural resource management and occupy no more than 10 per cent of the farm, the proposal should not require local government development approval; and*
- (g) the establishment of tree farms does not warrant the creation of new or smaller rural lots."*

A copy of the WAPC's SPP2.5 can be viewed at the following link:

[State Planning Policy 2.5 - Rural Planning - December 2016 \(www.wa.gov.au\)](https://www.wa.gov.au/government/publications/state-planning-policy-2.5-rural-planning-december-2016)

The 'Code of Practice for Timber Plantations in Western Australia' was first prepared by the Forest Industries Federation (WA) in partnership with the State Government in 2006 and updated in 2014 and a copy of the document can be viewed at the following link:

[Code of Practice for Timber Plantations in Western Australia](https://www.wa.gov.au/government/publications/code-of-practice-for-timber-plantations-in-western-australia)

The Code of Practice notes that:

"The purpose of this Code is to provide goals and guidelines to plantation managers so that operations in plantations in Western Australia are economically competitive and sustainable and are consistent with other resource management objectives.

The Code also facilitates assessment of State timber plantation practices by the Australian Government. This was a prerequisite to ensure that controls on the export of unprocessed wood from public and private plantations are removed.

The Western Australian Planning Commission and local town planning schemes can rely upon the adoption of, and adherence to, this Code by plantation managers as an integral part of the planning and land-use process, whether or not formal planning approval is required.

Local governments can rely on adherence to this Code where planning approval for the establishment of a plantation is not required under a town planning scheme (i.e. is a permitted land use).

Compliance with this Code will provide:

- the plantation industry with confidence to secure export opportunities for plantation products without additional licences or approvals;*
- plantation managers with a single reference document encompassing all relevant rules and regulations relating to plantations in Western Australia; however, plantation managers should be aware that this Code might not reflect the current status of regulations as they may change over time;*
- increased confidence for investors dealing with plantation managers;*
- a professional, credible and sustainable industry by establishing the framework within which growers can apply best practice; and*
- an environmentally conscious market with sustainably produced wood or wood derived products."*

The Department of Fire & Emergency Services also have the 'Guidelines for Plantation Fire Protection' that can be viewed at the following link:

[Publications \(dfes.wa.gov.au\)](https://publications.dfes.wa.gov.au)

Consultation

Council is not obliged to undertake consultation, however, it can refer the application to surrounding landowners and the Department of Primary Industries & Regional Development, the Department of Fire & Emergency Services, the Department of Water & Environmental Regulation, and the Department of Biodiversity, Conservation & Attractions inviting comment should it wish to do so.

Financial Implications

Nil

Strategic Community Plan

Goal 2: Eco-friendly, attractive and well-maintained towns, surrounded by outstanding natural beauty, landscapes, flora and fauna to be protected and enjoyed

2.2. Land use and building regulations are designed and administered to meet the current and future needs of the community

Officer Comment

The introduction of carbon planting to Mid West Shires in the last decade has become increasingly common and an issue that Councils have generally accepted but with some cautionary principles to ensure that they are located and managed appropriately.

Councils have occasionally raised concern that traditional agricultural activities such as cropping, grazing and food production should generally remain the predominant land use in its productive rural areas and that agroforestry or carbon plantations should remain as ancillary and complementary uses rather than displacing ones. Whilst there are other factors contributing towards reductions in rural populations, Councils have typically wished to ensure that new land uses do not exacerbate an existing ongoing issue.

Council have often sought to encourage tree farms in areas subject to land degradation, including areas of high salinity, water logging, and high levels of chemical contamination where there are clear natural resource management benefits, and discourage them upon land that is considered to be of high agricultural value or contain strategic surface gravel deposits required for the maintenance and construction of road networks.

Concerns have also been raised in the past where tree farms are managed remotely with only occasional visitation from employees who are attending to a series of plantations across a wide region, and how this can differ from the ability to control invasive species and manage fire risk more typically available when owners/operators are residing on-site.

PROCEDURAL MOTION

Council Resolution Number: 200225.20

Moved: Cr Sparkman

Seconded: Cr Bradford

That Council suspend Standing Orders, *Clause 9.5 Limitation on number of speeches*, at 3.35 pm to allow for open discussion of Item 15.4.

Motion put and carried 7/0

For: Cr Sutherland, Cr Hepworth, Cr Bryant, Cr Sparkman, Cr Bradford, Cr Fraser, Cr Campbell

Against: Nil

PROCEDURAL MOTION

Council Decision Number: 200225.21

Moved: Cr Fraser

Seconded: Cr Bradford

That Council reinstate Standing Orders at 3.38 pm.

Motion put and carried 7/0

For: Cr Sutherland, Cr Hepworth, Cr Bryant, Cr Sparkman, Cr Bradford, Cr Fraser, Cr Campbell

Against: Nil

COUNCIL DECISION

Council Resolution Number: 200225.22

Moved: Cr Bradford

Seconded: Cr Fraser

That Council grant formal planning approval for a Tree Farm upon Lot 4754 Solomon Road and Lot 1 Perenjori-Rothsay Road, Bowgada subject to the following:

Conditions:

- 1. Development shall be in accordance with the management plan included as Attachment 15.4.1 (with the February 2025 Council Agenda) and subject to any modifications required as a consequence of any condition(s) of this approval. The endorsed plans shall not be modified or altered without the prior written approval of the local government.**
- 2. Any additions to or change of use of any part of the land (not the subject of this consent/approval) considered by the Shire Chief Executive Officer to represent significant variation from the approved development plan requires further application and planning approval for that use/addition.**
- 3. The applicant is to implement and maintain reporting mechanisms for complaints concerning the operation of the development. In the event of a substantiated complaint being received the applicant is required to demonstrate mitigation response(s) to the approval of the local government. Such response(s) will be treated as conditions of approval/required modifications to the management plan.**
- 4. The water infrastructure on the tree farm (both existing and any additional infrastructure that may be required for fire management purposes) must at all times be maintained to a standard satisfactory to the Shire of Perenjori.**
- 5. Water adequate for both the managed grazing of the tree farm and fire management must be provided at all times.**
- 6. Vehicle access, water tanks and vegetation about the residence, outbuilding(s) and infrastructure must be maintained in accordance with the Asset Protection Zone requirements as contained in the Department for Planning, Lands and Heritage's *Guidelines for Planning in Bushfire Prone Areas* (document and appendices).**

Advice Note:

- (a) The management plan must display regard for the 'Code of Practice for Timber Plantations in Western Australia' (Forest Industries Federation WA, 2014), the 'Code of Practice for the use of agricultural and veterinary chemicals in WA' (Department of Primary Industries & Regional Development, 2005), the 'Guidelines for Plantation Fire Protection' (Department of Fire & Emergency Services, 2011) and Shire of Perenjori Tree Farms Local Planning Policy.
- (b) If an applicant is aggrieved by this determination there is a right (pursuant to the *Planning and Development Act 2005*) to have the decision reviewed by the State Administrative Tribunal. Such application must be lodged within 28 days from the date of determination.

Motion put and carried 7/0

For: Cr Sutherland, Cr Hepworth, Cr Bryant, Cr Sparkman, Cr Bradford, Cr Fraser, Cr Campbell

Against: Nil

[Next Item](#)

15.5 APPOINTMENT OF PRESIDING AND DEPUTY PRESIDING MEMBER – AUDIT COMMITTEE

Applicant:	Shire of Perenjori
File:	ADM 0382
Date:	20 February 2025
Disclosure of Interest:	Nil
Voting Requirements:	Absolute Majority
Author:	Bianca Plug – Governance Officer
Responsible Officer:	Paul Anderson – Chief Executive Officer
Attachments:	Nil

Summary

This item recommends that Council appoints a presiding member and deputy presiding member to the Audit Committee as required under the *Local Government Act 1995*.

Background

The Local Government Amendment Bill 2024 (the Bill) which proposes to change the *Local Government Act 1995* (the Act), passed WA Parliament in November 2024. The Bill delivers the second tranche of local government reforms and includes changes to allow for greater transparency and clarity on how Council committees operate.

The appointment process for the presiding member and deputy presiding member has now been simplified.

Until December 2024, it was up to the members of a committee to elect these roles by secret ballot. The Act now requires any committee established under section 5.8 of the Act to be appointed by Council before 1 July 2025. The Shire's Audit Committee falls under this requirement.

Statutory Environment

Local Government Act 1995

5.8. Establishment of committees

A local government may establish* committees of 3 or more persons to assist the council.

5.12. Presiding members and deputies

- (1) The local government must appoint* a member of a committee to be the presiding member of the committee.
- (2) The local government may appoint* a member of a committee to be the deputy presiding member of the committee.

* Absolute majority required.

Policy Implications

Nil

Consultation

WALGA – Governance Team

Paul Anderson – Chief Executive Officer

Financial Implications

Nil

Strategic Community Plan

Goal 4: A strong and diverse Council working closely with the proactive and involved community

4.6. The organisation, assets and finances of the Shire are managed responsibly

Officer Comment

Regulations under the second tranche are being developed that will require Audit Committees to be revised as 'Audit, Risk and Improvement Committees'. Audit, Risk and Improvement Committees will be required to have an independent chair to ensure a level of independence and impartial oversight. The commencement of these regulations at yet to be confirmed but are expected to be introduced during 2025.

OFFICER RECOMMENDATION

1. In accordance with s5.12(1) of the *Local Government Act 1995* appoint Cr _____ as the Presiding Member for the Audit Committee and;
2. In accordance with s5.12(2) of the *Local Government Act 1995* appoint Cr _____ as the Deputy Presiding Member for the Audit Committee.

COUNCIL DECISION

Council Resolution Number: 200225.23

Moved: Cr Fraser

Seconded: Cr Bradford

That Council:

1. In accordance with s5.12(1) of the *Local Government Act 1995* appoint Cr Jude Sutherland as the Presiding Member for the Audit Committee and;
2. In accordance with s5.12(2) of the *Local Government Act 1995* appoint Cr Leslie Hepworth as the Deputy Presiding Member for the Audit Committee.

Motion put and carried 7/0 by Absolute Majority

For: Cr Sutherland, Cr Hepworth, Cr Bryant, Cr Sparkman, Cr Bradford, Cr Fraser, Cr Campbell

Against: Nil

[Next Item](#)

15.6 LEASE RENEWAL (LEASE L6808) – LICENSE TO OCCUPY RAIL CORRIDOR, BRITT STREET, LATHAM

Applicant:	Burgess Rawson for the Public Transport Authority
File:	ADM 0273
Date:	20 February 2025
Disclosure of Interest:	Nil
Voting Requirements:	Simple Majority
Author:	Bianca Plug – Governance Officer
Responsible Officer:	Paul Anderson – Chief Executive Officer
Attachments:	15.6.1 – Expired Lease L6808

Summary

For Council to consider the renewal of an expired license to occupy a portion of rail corridor in Latham, for the purpose of beautification.

Background

The Shire of Perenjori entered into a 10 year lease with the Western Australian Government Railways Commission of Westrail on 1 October 1999 which provided the Shire with a license to occupy a portion of rail corridor along Britt Street, Latham for the purpose of beautification.

In April 2010, the Public Transport Authority (PTA) advised the Shire that under provisions of the *Rail Freight Act 2000*, any alterations, variations or additions to Lease L6808, including any new long-term agreement, must be negotiated directly with WestNet Rail (previously Westrail). At the time, both the PTA and the Shire were satisfied with the existing lease conditions, and in June 2010, the Shire agreed to extend Lease L6808 on a month-to-month basis. Until a new lease is formalised, the Shire of Perenjori will continue on an overholding tenancy as outlined in the licence.

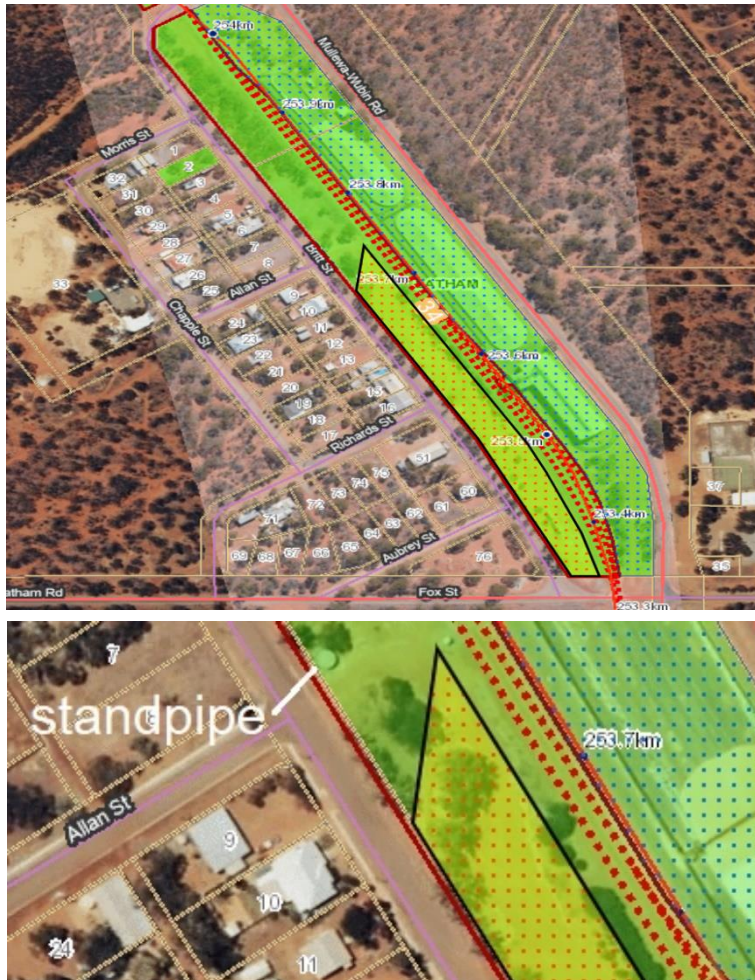
Since 1999 when the lease was first established, Westrail has undergone significant changes in ownership and management.

In December 2000, the Government announced that the freight business of the Western Australian Government Railways (WAGR), trading as Westrail, had been sold to the Australian Railroad Group Pty Ltd (ARG). In 2006 ARG was sold to a consortium comprising of Babcock and Brown. In late 2009, Babcock and Brown Infrastructure became known as Prime Infrastructure and in mid 2011, Prime Infrastructure was delisted as a public company. Prime Infrastructure then became owned by Brookfield Infrastructure Partners. To reflect these ownership changes, WestNet Rail was renamed Brookfield Rail in August 2011 and in July 2017, Brookfield Rail changed its name to ARC Infrastructure.

Arc Infrastructure is now the lessee of the rail network under the 2000 lease agreement and, for the purposes of access arrangements under the *Railways (Access) Act 1998*, has management and control of the non-urban rail network.

Arc Infrastructure recently approached the Public Transport Authority regarding the expired lease and advised that they are interested in renewing Lease L6808 with the Shire of Perenjori. Should Council resolve to renew the lease, a renewal request will be submitted to the Public Transport Authority who will obtain approval directly from Arc Infrastructure prior to establishing a new lease.

The land permitted for use is approximately 10,085 sqm as identified below by the thick black border filled with red dots.



The existing lease does not currently cover the portion of corridor where the Latham standpipe is located, as shown above. Advice provided by Burgess Rawson for the Public Transport Authority, is to include the area of reserve that the standpipe is located, should Council resolve to renew the lease.

Statutory Environment

Public Transport Authority Act 2003

Policy Implications

Nil

Consultation

Burgess Rawson for the Public Transport Authority
Paul Anderson – Chief Executive Officer
Marty Noordhof – Manager Infrastructure Services

Financial Implications

Financial implications are minimal. The Shire currently maintains the parks and gardens within the Latham townsite once per month which includes maintenance to the entire length of Britt Street rail corridor when needed. Employee wages for this maintenance are already covered under the Annual Budget.

Strategic Community Plan

Goal 2: Eco-friendly, attractive and well-maintained towns, surrounded by outstanding natural beauty, landscapes, flora and fauna to be protected and enjoyed

2.3. The Shire's buildings and leases are administered to an appropriate standard for the benefit of the community according to their need and use

2.7. Parks, gardens, street trees and reserves are appropriately managed according to their need and use

Officer Comment

The Shire has always maintained the entire rail corridor reserve along Britt Street, not just the portion included within the lease. In addition to the standpipe being outside the current licensed area, it is recommended that if renewed, the new lease should encompass the entire Britt Street rail corridor.

There is an option to terminate the existing license and relinquish the Shires responsibility to maintain the rail corridor. While firebreaks are a legal requirement under Section 33 of the *Bush Fires Act 1954* and requires property owners / managers to clear firebreaks on or before 16 November and maintain them up to and including 31 March the following year, the rail corridor may become untidy outside of these requirements if a lease is not in place.

The PTA have advised that a lease agreement can be renewed for a term of up to 10 years.

PROCEDURAL MOTION

Council Resolution Number: 200225.24

Moved: Cr Campbell

Seconded: Cr Sparkman

That Council suspend Standing Orders, *Clause 9.5 Limitation on number of speeches*, at 3.41 pm to allow for open discussion of Item 15.6.

Motion put and carried 7/0

For: Cr Sutherland, Cr Hepworth, Cr Bryant, Cr Sparkman, Cr Bradford, Cr Fraser, Cr Campbell

Against: Nil

PROCEDURAL MOTION

Council Decision Number: 200225.25

Moved: Cr Bryant

Seconded: Cr Sparkman

That Council reinstate Standing Orders at 3.43 pm.

Motion put and carried 7/0

For: Cr Sutherland, Cr Hepworth, Cr Bryant, Cr Sparkman, Cr Bradford, Cr Fraser, Cr Campbell

Against: Nil

OFFICER RECOMMENDATION

That Council resolve to:

- 1. Authorise the Chief Executive Officer to submit a renewal request to the Public Transport Authority to renew Lease (L6808) with Arc Infrastructure for a term of ____ years for the purpose of beautification and Latham Standpipe usage and, request the licensed area to include the rail corridor parallel to Britt Street, Latham.**

OR

- 2. Authorise the Chief Executive Officer to advise the Public Transport Authority of Councils intent to terminate the existing Lease (L6808) relinquishing the Shires responsibility to maintain the rail corridor.**

COUNCIL DECISION

Council Resolution Number: 200225.26

Moved: Cr Campbell

Seconded: Cr Bryant

That Council resolve option:

- 1. Authorise the Chief Executive Officer to submit a renewal request to the Public Transport Authority to renew Lease (L6808) with Arc Infrastructure for a term of 10 years for the purpose of beautification and Latham Standpipe usage and, request the licensed area to include the rail corridor parallel to Britt Street, Latham.**

Motion put and carried 7/0

For: Cr Sutherland, Cr Hepworth, Cr Bryant, Cr Sparkman, Cr Bradford, Cr Fraser, Cr Campbell

Against: Nil

[Next Item](#)

15.7 TERRA MINING ROAD USER, CONTRIBUTION AND UPGRADE AGREEMENT

Applicant:	Terra Mining
File:	ADM 0901
Date:	20 February 2025
Disclosure of Interest:	Nil
Voting Requirements:	Simple Majority
Author:	Bianca Plug – Governance Officer
Responsible Officer:	Paul Anderson – Chief Executive Officer
Attachments:	15.7.1 – Terra Mining Proposal for an Extension to Road Agreement 15.7.2 – Current Road User, Contribution and Upgrade Agreement

Summary

For Council to consider a proposal by the proponent of the Extension Hill Mining Project, Terra Mining to extend the current Road User, Contribution and Upgrade Agreement and dispense with the requirement of a bank guarantee.

Background

At the Ordinary Council Meeting held 19 December 2024, Council resolved to approve an extension to the existing road user agreement with Terra Mining for a period of 3 months, aligning with the Commercial Goods Vehicle Licence.

Council Resolution Number: 191224.20

Moved: Cr Fraser

Seconded: Cr Sparkman

That Council:

2. Approve the extension to the existing Road User Agreement for a period of 3 months to the 31 March 2025 aligning with the Commercial Goods Vehicle Licence.

Motion put and carried 7/0

For: Cr Sutherland, Cr Hepworth, Cr Bryant, Cr Sparkman, Cr Bradford, Cr Fraser, Cr Campbell

Against: Nil

The agreement ([attachment 15.7.1](#)) was executed on the 23 December 2024 and is due to expire on 31 March 2025.

On 5 February 2025 the Shire received the following proposal from Terra Mining to extend the current road agreement:

"Jude, Paul and Fellow Councillors

We have been given an extension to the current road agreement by Main Roads until 30 June 2025.

We are requesting for an extension to our current road agreement with Perenjori Shire until March 31 2026 on the condition we have an arrangement with a rail provider by 30 June 2025.

We have been positively progressing negotiations to utilise the Perenjori siding, which has always been our intention and the prospect continues to grow favourably. As the siding has not been used for a number of years we will need some time to be able to bring the loading area and rails cars up to a usable standard. This potentially could take between 6-12 months to complete the process as the siding and rail cars have not been used for a number of years.

We would also request from Council to consider dispensing of the requirement to have a bond. The money has no value other than to secure potential damage to the road that we have been actively maintaining. Terra Mining has honoured all of its agreements to date and have spent \$320,000 on road repairs that had already been damaged prior to our activities. Terra Mining has also made \$127,880 to the community fund as well as making a number of contributions to various organisations within the shire, Blue Light Disco (\$3000), the local school for a steam vacuum cleaner (\$2500) and an ongoing presence at the Perenjori Agricultural Show.

If the Council agree to the extension then the potential value may be as much as \$175,000 in community funds. This will also not cease as we would still require a road agreement for the period of time we are utilising the rail siding.

We do thank Council for their ongoing support and look forward to Councils favourable consideration in this matter."

Clause 23 of the road user agreement requires the Shire and Terra Mining to agree to an extension prior to expiration of the term:

- a) *the Shire and Terra Mining shall negotiate in good further to renew this Agreement, noting that the entry of the Shire into any renewed agreement will be subject to the approval of the Council of the Shire;*
- b) *if the Agreement is renewed, it will be a term of any renewed agreement that the Road Use shall only be permitted outside of School Hours, the Community Infrastructure Fund Contribution will be renegotiated and speed limits through the Perenjori townsite will be imposed as part of the conditions on any Road Use.*

Clause 5.1 of the road user agreement states:

On the execution of this Agreement, and prior to Terra Mining commencing use of the Affected Roads for the Road Use, Terra Mining covenants and agrees to provide to the Shire a bank guarantee in the sum of \$500,000.00 (excluding GST) (Bank Guarantee) to be held by the Shire in accordance with this clause 5.

Clause 5.1 has been in effect since the first road user agreement commenced on 22 December 2023.

At the Ordinary Council Meeting held on 25 July 2024, Council considered the following request from Terra Mining:

"We are requesting that the Council consider the requirement for \$500,000k bond (Bank guarantee for potential future work road damage-repair) to be suspended until January 2025 where our DMS will deliver our beneficiation products to the market. This will provide a secure and long-term viability for the Extension Hill project.

However, if Perenjori Shire considers the suspension of the bond until January 2025 is not acceptable, Terra Mining would alternatively request monthly cash payments of \$50,000 (until the \$500,000 is reached) to be held in a trust account, in the event funding for work on agreed road damaged through our activities is not carried out."

As a show of good faith, and to ensure Council had funds to draw upon should the requirement to maintain roads not be met by Terra Mining, Council resolved the following:

Council Resolution Number: 250724.29

Moved: Cr Fraser

Seconded: Cr Bradford

THAT Council notes the request from Terra Mining in regard to the Bank guarantee requirements contained with the Road User, Contribution and Upgrade Agreement and resolves the following:

Option 2.

The Council accept five payments of \$100,000 per month commencing in August 2024 until the Bank guarantee amount is reached (\$500,000) and subject to the immediate acceptance of a baseline road report provided by Greenfield Technical Services in January 2024.

Motion put and carried 7/0

For: Cr Sutherland, Cr Hepworth, Cr Bradford, Cr Bryant, Cr Campbell, Cr Sparkman, Cr Fraser

Against: Nil

The first instalment of \$100,000 for the bank guarantee (\$500,000) was deposited into Council's bank account in September 2024. Since this date, no further deposits have been received with an outstanding amount of \$400,000 remaining.

Statutory Environment

Main Roads Act 1930

Road Traffic (administration) Act 2008 - s132

Public Works Act 1902

Land Administration Act 1997- s55(2)

Local Government Act 1995 - s3.57

Policy Implications

Nil

Consultation

Elected Members

Barry Cook – Chief Operating Officer, Terra Mining

Damian Lesque – Project Compliance Manager, Terra Mining

Financial Implications

The current road agreement places the costs associated with maintaining the road to an acceptable standard with Terra Mining while they are operating and utilising the affected roads.

Should Council resolve to remove the bank guarantee clause from the agreement, Terra Mining will still be required to maintain Council road assets in an acceptable condition to ensure the safety of all road users.

Removing this clause eliminates the ability for Council to draw upon funds to undertake repairs if the requirement to maintain the roads contained within the agreement are not met by Terra.

The current agreement also includes the provision for a community infrastructure fund contribution per tonne of product from the mining operations. The current balance of the community fund is \$127,880 with the most recent payments received in January 2025. As noted by Terra, the community fund will not cease even if arrangement with a rail provider is met.

The Council in only authorising use for a 12-month period can include a provision in future agreements for an increase in the community contribution.

Strategic Community Plan

Goal 2: Eco-friendly, attractive and well-maintained towns, surrounded by outstanding natural beauty, landscapes, flora and fauna to be protected and enjoyed.

2.6. Roads, footpaths and drainage are appropriately managed according to their need and use.

Goal 4: A strong and diverse Council working closely with the proactive and involved community.

4.3. The Shire advocates and partners effectively on behalf of the community.

4.6. The organisation, assets and finances of the Shire are managed responsibly.

Officer Comment

The overall premise of the agreement is that Terra Mining are fully responsible for maintaining the road in an agreed acceptable condition and have a legal responsibility regarding road use whilst they are carting ore from Extension Hill Minesite.

The officer recommendations have been separated into three parts to minimise confusion.

Council can consider the following options for the extension:

1. Refuse the request for an extension to the road agreement until such time that Terra Mining have fulfilled their bank guarantee requirements. This option is in accordance with the current agreement and Council Resolution 250724.29.
2. Approve the extension to the current road agreement for a period of 3 months to the 30 June 2025. This option allows time for Terra Mining to secure an arrangement with a rail provider and aligns with the extension given by Main Roads.
3. Approve the extension to the current road agreement for a period of 12 months to the 31 March 2026 on the condition that Terra Mining have an arrangement with a rail provider by 30 June 2025.
4. Approve an extension for a period the Council sees fit with the amendments as determined by Council to be negotiated with Terra Mining.

Council can consider the following options for the bank guarantee:

1. Accept Terra Mining's proposal to dispense the requirement of a bank guarantee or;
2. Refuse Terra Mining's proposal to dispense the requirement of a bank guarantee or;
3. Consider reducing the bank guarantee amount to \$250,000 or an amount as determined by Council.

PROCEDURAL MOTION

Council Resolution Number: 200225.27

Moved: Cr Sparkman

Seconded: Cr Hepworth

That Council suspend Standing Orders, *Clause 9.5 Limitation on number of speeches*, at 3.44 pm to allow for open discussion of Item 15.7.

Motion put and carried 7/0

For: Cr Sutherland, Cr Hepworth, Cr Bryant, Cr Sparkman, Cr Bradford, Cr Fraser, Cr Campbell

Against: Nil

PROCEDURAL MOTION

Council Decision Number: 200225.28

Moved: Cr Bradford

Seconded: Cr Fraser

That Council reinstate Standing Orders at 3.51 pm.

Motion put and carried /0

For: Cr Sutherland, Cr Hepworth, Cr Bryant, Cr Sparkman, Cr Bradford, Cr Fraser, Cr Campbell

Against: Nil

COUNCIL DECISION

Council Resolution Number: 200225.29

Moved: Cr Sparkman

Seconded: Cr Bradford

That Council notes the request from Terra Mining to extend the current Road User, Contribution and Upgrade Agreement to 31 March 2026 and the request to dispense with the requirement of a bank guarantee.

Motion put and carried 7/0

For: Cr Sutherland, Cr Hepworth, Cr Bryant, Cr Sparkman, Cr Bradford, Cr Fraser, Cr Campbell

Against: Nil

OFFICER RECOMMENDATION

That Council:

- 1. Advise Terra Mining that Council will not consider any extension to the Road User, Contribution and Upgrade Agreement until such time that the Bank Guarantee amount is reached (\$500,000).**

OR

- 2. Approve the extension to the existing Road User, Contribution and Upgrade Agreement for a period of 3 months to 30 June 2025, aligning with the extension provided by Main Roads and allowing time for Terra Mining to secure an arrangement with a rail provider by 30 June 2025.**

OR

- 3. Approve the extension to the existing Road User, Contribution and Upgrade Agreement for a period of 12 months to the 31 March 2026 on the condition that Terra Mining secure an arrangement with a rail provider by 30 June 2025.**

COUNCIL DECISION

Council Resolution Number: 200225.30

Moved: Cr Fraser

Seconded: Cr Hepworth

That Council resolve option:

- 2. Approve the extension to the existing Road User, Contribution and Upgrade Agreement for a period of 3 months to 30 June 2025, aligning with the extension provided by Main Roads and allowing time for Terra Mining to secure an arrangement with a rail provider by 30 June 2025.**

Motion put and carried 7/0

For: Cr Sutherland, Cr Hepworth, Cr Bryant, Cr Sparkman, Cr Bradford, Cr Fraser, Cr Campbell

Against: Nil

OFFICER RECOMMENDATION

That Council:

- 1. Accept Terra Mining's request to dispense the requirement of a Bank Guarantee.**

OR

- 2. Decline the request to dispense the requirement of a Bank Guarantee and advise Terra Mining that the Bank Guarantee is due and payable in accordance with the Road User, Contribution and Upgrade Agreement and Council Resolution 250724.29.**

OR

- 3. Reduce the Bank Guarantee amount from \$500,000 to \$250,000 inclusive of the \$100,000 already paid by Terra Mining.**

AMENDMENT TO OFFICER RECOMMENDATION

Council made an amendment to the Officer Recommendation to reduce the bond requirement from \$250,000 to \$100,000 inclusive of the \$100,000 already paid in a show of good faith in Terra Mining to continue to work with the Council for the benefit of the community.

COUNCIL DECISION

Council Resolution Number: 200225.31

Moved: Cr Bradford

Seconded: Cr Fraser

That Council resolves to amend option:

- 3. That Council reduce the Bank Guarantee amount from \$500,000 to \$100,000 inclusive of the \$100,000 already paid by Terra Mining.**

Motion put and carried 7/0

For: Cr Sutherland, Cr Hepworth, Cr Bryant, Cr Sparkman, Cr Bradford, Cr Fraser, Cr Campbell

Against: Nil

[Next Item](#)

16. Confidential Reports:

Nil

17. Ordering the Common Seal:

Document	Organisation	Purpose	Date
Sublease Agreement – Perenjori Community Resource Centre, 24 Fowler Street	Shire of Perenjori, Perenjori Community Resource Centre & Kelly Patricia Holland	Sublease Agreement – Perenjori Community Resource Centre Café, 24 Fowler Street	15 December 2024
Road User, Contribution and Upgrade Agreement – Terra Mining Pty Ltd	Shire of Perenjori & Terra Mining Pty Ltd	Road User, Contribution and Upgrade Agreement	23 December 2024

18. Reports of Committees and Members:

Nil

19. Motions of Which Previous Notice Has Been Given:

Nil

20. Notice of Motions:

Nil

21. New Business of an Urgent Nature Admitted by Council:

PROCEDURAL MOTION

Council Resolution Number: 200225.32

Moved: Cr Sparkman Seconded: Cr Bryant

That Council accepts Late Confidential Item 21.1 Chief Executive Officer Recruitment and Late Item 21.2 Variations to the Mid West Secondary Freight Network Program for consideration.

Motion put and carried 7/0

For: Cr Sutherland, Cr Hepworth, Cr Bryant, Cr Sparkman, Cr Bradford, Cr Fraser, Cr Campbell

Against: Nil

PROCEDURAL MOTION

Council Resolution Number: 200225.33

Moved: Cr Fraser Seconded: Cr Bryant

That Council closes the meeting to the public at 3.58 pm to discuss Late Confidential Item 21.1 as a matter affecting an employee and entering into a contract, in accordance with s.5.23(2)(a) and (c) of the *Local Government Act 1995*.

Motion put and carried 7/0

For: Cr Sutherland, Cr Hepworth, Cr Bryant, Cr Sparkman, Cr Bradford, Cr Fraser, Cr Campbell

Against: Nil

21.1 **LATE CONFIDENTIAL ITEM – CHIEF EXECUTIVE OFFICER RECRUITMENT**

Applicant:	Shire of Perenjori
File:	ADM 0938
Date:	20 February 2025
Disclosure of Interest:	Paul Anderson – Impartiality Interest
Voting Requirements:	Simple Majority
Author:	Paul Anderson – Chief Executive Officer
Responsible Officer:	Paul Anderson – Chief Executive Officer
Attachments:	21.1.1 – Model Standards for the CEO Recruitment, Performance & Termination 21.1.2 – Draft CEO Recruitment Panel – Terms of Reference 2025 21.1.3 – Draft CEO Recruitment Panel – Confidential Agreement 2025 21.1.4 – Draft CEO Position Description 2025 21.1.5 – Draft CEO Employment Contract 2025 21.1.6 – RFQ Proposal – Recruitment Consultancy Services (Lydia Highfield)

COUNCIL DECISION

Council Resolution Number: 200225.34

Moved: Cr Bradford

Seconded: Cr Fraser

Council appoints Recruitment Consultancy Services (Lydia Highfield) as the Recruitment Consultant to assist the Shire with the recruitment process for a replacement Chief Executive Officer, in accordance with the submission provided under attachment 21.1.6.

Motion put and carried 7/0

For: Cr Sutherland, Cr Hepworth, Cr Bryant, Cr Sparkman, Cr Bradford, Cr Fraser, Cr Campbell

Against: Nil

COUNCIL DECISION

Council Resolution Number: 200225.35

Moved: Cr Fraser

Seconded: Cr Sparkman

Council endorses the “Draft CEO Recruitment Panel – Terms of Reference 2025” as per attachment 21.1.2 to comply with the model Standards for CEO Recruitment, Performance and Termination and *Local Government (Administration) Amendment Regulations 2021*.

Motion put and carried 7/0

For: Cr Sutherland, Cr Hepworth, Cr Bryant, Cr Sparkman, Cr Bradford, Cr Fraser, Cr Campbell

Against: Nil

COUNCIL DECISION

Council Resolution Number: 200225.36

Moved: Cr Hepworth

Seconded: Cr Campbell

Council endorses the “Draft CEO Recruitment Panel - Confidentiality Agreement 2025” as per attachment 21.1.3 to comply with the Model Standards for CEO Recruitment, Performance and Termination and *Local Government (Administration) Amendment Regulations 2021*.

Motion put and carried 7/0

For: Cr Sutherland, Cr Hepworth, Cr Bryant, Cr Sparkman, Cr Bradford, Cr Fraser, Cr Campbell

Against: Nil

COUNCIL DECISION

Council Resolution Number: 200225.37

Moved: Cr Sparkman Seconded: Cr Bryant

That Council notes the “Draft CEO Position Description 2025” as per attachment 21.1.4 in the knowledge it will be reviewed by the Recruitment Consultant and will subsequently be required to be approved by Absolute Majority by Council prior to advertising of the position.

Motion put and carried 7/0

For: Cr Sutherland, Cr Hepworth, Cr Bryant, Cr Sparkman, Cr Bradford, Cr Fraser, Cr Campbell

Against: Nil

COUNCIL DECISION

Council Resolution Number: 200225.38

Moved: Cr Fraser Seconded: Cr Bradford

That Council note the “Draft Chief Executive Officer Contract 2025” as per attachment 21.1.5 in the knowledge it will be reviewed by the Recruitment Consultant and will subsequently be required to be approved by Absolute Majority by Council at the completion of the negotiation with the successful applicant.

Motion put and carried 7/0

For: Cr Sutherland, Cr Hepworth, Cr Bryant, Cr Sparkman, Cr Bradford, Cr Fraser, Cr Campbell

Against: Nil

PROCEDURAL MOTION

Council Resolution Number: 200225.39

Moved: Cr Sparkman Seconded: Cr Fraser

That Council return to standing orders and re-open the meeting to the public at 4.03 pm.

Motion put and carried 7/0

For: Cr Sutherland, Cr Hepworth, Cr Bryant, Cr Sparkman, Cr Bradford, Cr Fraser, Cr Campbell

Against: Nil

[Next Item](#)

21.2 LATE ITEM – VARIATIONS TO THE MID WEST SECONDARY FREIGHT NETWORK PROGRAM

Applicant:	Shire of Perenjori
File:	RFT-MWSFN-01-23.24 / ADM 0919
Date:	20 February 2025
Disclosure of Interest:	Nil
Voting Requirements:	Simple Majority
Author:	Marty Noordhof – Manager Infrastructure Services
Responsible Officer:	Paul Anderson – Chief Executive Officer
Attachments:	Nil

Summary

This report seeks Council's endorsement for essential variations to the Mid West Secondary Freight Network program to accommodate additional repairs to Morawa South Road, following damage caused by a Water Corporation pipeline leak, which was not initially identified.

Background

The Shire of Perenjori and the Shire of Morawa (jointly) tendered to complete sealed road widening work on various roads throughout the two Shire's as part of the Mid West Secondary Freight Network upgrade project.

At the Special Meeting of Council held 2 May 2024, Council resolved the following:

Council Resolution Number: 020524.8

Moved: Cr Hepworth Seconded: Cr Bryant

Based on the revised prices and with consideration for the available budget, it is recommended that Council:

1. Award RFT-MWSFN-01-2324 to Fulcher Contractors for the estimated total contract sum of \$4,384,425 + GST.
2. Authorise the Shire Chief Executive Officer to approve variations up to 10% of the contract value during the works and engage the necessary engineering technical support and consultancy needed for such works.

Motion put and carried 5/0

For: Cr Sutherland, Cr Hepworth, Cr Sparkman, Cr Bryant, Cr Fraser

Against: Nil

Due to prolonged waterlogging, repairs to Morawa South Road at SLK 1.87 – 2.05 have been re-scoped following further damage to the road surface. During the assessment, pilot holes were put down at the edge of the sealed area to assess the extent of water infiltration, revealing significant water seepage from the surrounding soil.

Subsequent examination identified a previously undetected water leak from a WaterCorp pipeline, which had initially been attributed to unseasonal rainfall in the preceding months. As a result of the water inundation, the subgrade is no longer suitable for the originally planned basic repair, necessitating a more extensive treatment approach.

The revised scope of work to repair this section of road is as follows:

- **Excavation:** Box out the existing pavement to a depth of 600mm and remove material offsite. The excavation will typically extend to the edge of the tapered shoulder (approximately 11m wide), with the base shaped to provide a -3% crown crossfall from the road centreline.
- **Sub-base Layer:** Place and compact a 300mm gravel sub-base using basecourse quality material, shaped to maintain a -3% crown crossfall from the road centreline.
- **Basecourse Layer:** Place and compact a 300mm gravel basecourse layer, also shaped to ensure a -3% crown crossfall from the road centreline.
- **Surface Treatment:** Apply two coats of bitumen seal (14/10).

The quoted price from Fulcher for this work is \$299,930.40 (exclusive of GST).

Statutory Environment

Local Government Act 1995 – Section 3.57 Tenders for providing goods or services

Local Government (Functions and General) Regulations 1996 – Division 2 Requirements for Tendering

Policy Implications

Policy 4003 – Managing Council Budgets

Policy 4006 – Regional Price Preference Policy

Policy 4007 – Purchasing Policy

Consultation

Matt Barns – Greenfield Technical Services

Matt Fulcher – Fulcher Contracting

Paul Anderson – Chief Executive Officer

Financial Implications

The revised scope of work and repairs to Morawa South Road at SLK 1.87-2.05 have resulted in increased costs beyond the original tender figure.

The total funding allocated for this project is \$5,195,511, which is fully sourced externally.

The revised quote for the required repairs at SLK 1.87-2.05 is \$299,930.40.

Costs associated with the modified seal design are still to be determined; however, the project will remain self-funded by reducing the 8-meter topcoat seal and only sealing the widened shoulder as necessary.

Strategic Community Plan

Goal 2: Eco-friendly, attractive and well-maintained towns, surrounded by outstanding natural beauty, landscapes, flora and fauna to be protected and enjoyed

2.6. Roads, footpaths and drainage are appropriately managed according to their need and use

Goal 4: A strong and diverse Council working closely with the proactive and involved community

4.6. The organisation, assets and finances of the Shire are managed responsibly

Officer Comment

Following a comprehensive evaluation of the work required and consideration of previously submitted variation costs, we deem the quoted price from Fulcher of \$299,930.40 (exclusive of GST) to be reasonable. Costs associated with the modified seal design are still to be determined; however, the project will remain self-funded by reducing the 8-meter topcoat seal and only sealing the widened shoulder as necessary.

Given that the WaterCorp pipeline leak is considered the primary cause of the pavement failure, the Shire has submitted a claim to WaterCorp for cost recovery associated with the repair. This claim has yet to be resolved.

For reference, Downer submitted a non-conforming tender valued at \$5,043,903, which reflects an increase of \$659,478, excluding variations. This tender also leaves only \$151,608 in contingency for unforeseen variations, which would have necessitated a significantly more aggressive reduction in the top coat seal.

Despite the unforeseen variations, the Shire is confident that the proposed solution still offers the best value outcome and have approved the works.

Council resolution 020524.8, as outlined above, grants the CEO the authority to approve variations up to 10% of the contract value during the project duration.

Given the additional costs associated with repairs to the water-damaged section of Morawa South Road, the variations will, and have, exceeded the 10% threshold.

These repairs were essential to ensure the scope of works outlined in the original tender is completed. Furthermore, deferring repairs to a later date would incur significant mobilisation and demobilisation costs, resulting in a substantial increase to the overall project cost.

These costs would likely have to be funded from Councils own funds as the Mid West grain freight project funding would have to be acquitted.

COUNCIL DECISION

Council Resolution Number: 200225.40

Moved: Cr Bryant

Seconded: Cr Bradford

That Council endorse the revised scope of works and the Chief Executive Officers actions in undertaking the variations to tender RFT-MWSFN-01-23/24 awarded to Fulcher contractors and the additional repairs to the Morawa South Road in excess of the 10% approved variation limit previously resolved by Council.

Motion put and carried 7/0

For: Cr Sutherland, Cr Hepworth, Cr Bryant, Cr Sparkman, Cr Bradford, Cr Fraser, Cr Campbell

Against: Nil

22. Closure of Meeting:

The Shire President declared the meeting closed at 4.04 pm.

23. Next Meeting:

The Shire President advised that the next Ordinary Meeting of Council will be held on Thursday, 20 March 2025 at the Latham Community Centre, Latham WA 6616, commencing at 3.00 pm.

I certify that this copy of the Minutes is a true and correct record of the meeting held on 20 February 2025.

Signed: _____

Shire President

Date: _____

13. Finance:

13.1 MONTHLY FINANCIAL REPORT – 28 FEBRUARY 2025

Applicant:	Shire of Perenjori
File:	ADM 0082
Report Date:	20 March 2025
Disclosure of Interest:	
Voting Requirements:	Simple Majority
Author:	Ally Bryant – Finance Manager
Responsible Officer:	Paul Anderson – Chief Executive Officer
Attachments:	13.1.1 - Monthly Statement of Financial Activity for 28 February 2025

Summary

This item recommends that Council receives the Financial Activity Statements for the periods ending 28 February 2025.

Background

Regulation 34 of the Local Government (Financial Management) Regulations 1996 requires local governments to prepare a Monthly Statement of Financial Activity.

Recent changes to the Regulations require a focus on reporting and variance analysis by nature and type rather than by program. The Shire's reports have historically been prepared by both nature and type and program with the variance analysis done by program. All reports for 2024-25 to date have been prepared by both nature and type and program, but with the variance analysis being done by nature and type in compliance with the revised Regulations.

Statutory Environment

Regulation 34 of the Local Government (Financial Management) Regulations 1996 requires local governments to prepare a Monthly Statement of Financial Activity.

Policy Implications

Nil

Consultation

Nil

Financial Implications

Shown in the attached data.

Strategic Community Plan

Goal 4: A strong and diverse Council working closely with the proactive and involved community.

Strategic Objectives

4.6. The organisation, assets and finances of the Shire are managed responsibly

Officer Comment

The audited opening surplus for the 2024-25 financial year is \$3,170,411. As previously advised, this surplus is notionally deceiving as the State Government transferred 85% of its 2024-25 financial assistance grants (totalling \$2.496m) to the Shire in June 2024.

Operating revenue YTD is \$6,516,625 over budget by \$172,448

Operating expenditure YTD is \$8,999,043 under budget by \$507,504

Key variances between the YTD budget and actuals are explained in the attached report on pages 2-3.

OFFICER RECOMMENDATION

Council Resolution Number:

Moved:

Seconded:

That the Monthly Financial Report to 28 February 2025 as attached be received.

Motion put and carried / lost

For:

Against:

[Next Item](#)

SHIRE OF PERENJORI

MONTHLY FINANCIAL REPORT

**(Containing the required statement of financial activity and statement of financial position)
For The Period Ended 28 February 2025**

***LOCAL GOVERNMENT ACT 1995
LOCAL GOVERNMENT (FINANCIAL MANAGEMENT) REGULATIONS 1996***

TABLE OF CONTENTS

Statements required by regulation

Statement of Financial Activity and Explanation of Material Variances	2
Statement of Comprehensive Income by Reporting Program	4
Statement of Financial Position	5
Note 1 Basis of Preparation	6
Note 2 Statement of Financial Activity Information	7

SHIRE OF PERENJORI
STATEMENT OF FINANCIAL ACTIVITY
FOR THE PERIOD ENDED 28 FEBRUARY 2025

	Supplemental Information	Adopted Budget Estimates (a) \$	YTD Budget Estimates (b) \$	YTD Actual (c) \$	Variance* \$ (c) - (b)	Variance* % ((c) - (b))/(b)	Var. Explanation of Variance
OPERATING ACTIVITIES							
Revenue from operating activities							
General rates	10	3,467,290	3,467,290	3,469,927	2,637	0.08%	
Grants, subsidies and contributions	13	726,593	629,621	630,414	793	0.13%	
Fees and charges		1,007,267	736,280	882,740	146,460	19.89%	▲ Caravan park over budget by 62k, Private working general mining over budget 47k Standpipe water fees over by 39k
Interest revenue		162,500	123,564	167,496	43,932	35.55%	▲ Interest rate above budget
Other revenue		1,926,224	1,346,722	1,325,348	(21,374)	(1.59%)	▼ Under due to budget timing for reimbursement of MWSGFN Morawa
Profit on asset disposals	6	43,200	40,700	40,700	0	0.00%	
		7,333,074	6,344,177	6,516,625	172,448	2.72%	
Expenditure from operating activities							
Employee costs		(3,574,760)	(2,461,129)	(2,371,297)	89,832	3.65%	▲ Over and unders in various accounts
Materials and contracts		(3,873,581)	(2,786,987)	(2,479,902)	307,085	11.02%	▲ Over and unders in various accounts
Utility charges		(392,725)	(261,680)	(192,169)	69,511	26.56%	▲ Timing of accounts
Depreciation		(5,391,127)	(3,593,920)	(3,569,266)	24,654	0.69%	▲ Feb Shorter month causing timing variance
Finance costs		(66,784)	(34,799)	(39,713)	(4,914)	(14.12%)	▼ Timing of budget for guarantee fee
Insurance		(193,475)	(192,020)	(193,345)	(1,325)	(0.69%)	
Other expenditure		(224,258)	(171,012)	(148,352)	22,660	13.25%	▲ Timing of accounts
Loss on asset disposals	6	(20,950)	(5,000)	(4,999)	1	0.02%	
		(13,737,660)	(9,506,547)	(8,999,043)	507,504	5.34%	
Non-cash amounts excluded from operating activities	note 2(i)	5,368,877	3,558,220	3,533,565	(24,655)	(0.69%)	▼ Feb Shorter month causing timing variance
Amount attributable to operating activities		(1,035,709)	395,850	1,051,147	655,297	165.54%	
INVESTING ACTIVITIES							
Inflows from investing activities							
Proceeds from capital grants, subsidies and contributions	14	6,852,449	3,151,900	3,135,050	(16,850)	(0.53%)	▼ Timing
Proceeds from disposal of assets	6	151,300	56,300	72,700	16,400	29.13%	▲ Timing of disposals
		7,003,749	3,208,200	3,207,750	(450)	(0.01%)	
Outflows from investing activities							
Payments for property, plant and equipment	5	(5,745,999)	(4,068,857)	(4,016,170)	52,687	1.29%	▲ Timing see Capital Works Note
Payments for construction of infrastructure	5	(6,363,777)	(2,831,719)	(2,428,600)	403,119	14.24%	▲ Timing see Capital Works Note
		(12,109,776)	(6,900,576)	(6,444,770)	455,806	6.61%	
Amount attributable to investing activities		(5,106,027)	(3,692,376)	(3,237,020)	455,356	12.33%	

SHIRE OF PERENJORI
STATEMENT OF FINANCIAL ACTIVITY
FOR THE PERIOD ENDED 28 FEBRUARY 2025

	Supplemental Information	Adopted Budget Estimates (a) \$	YTD Budget Estimates (b) \$	YTD Actual (c) \$	Variance* \$ (c) - (b)	Variance* % ((c) - (b))/(b)	Var.	Explanation of Variance
FINANCING ACTIVITIES								
Inflows from financing activities								
	11	2,100,000	1,500,000	1,500,000	0	0.00%		
	4	1,692,144	0	0	0	0.00%		
		3,792,144	1,500,000	1,500,000	0	0.00%		
Outflows from financing activities								
	11	(57,921)	(43,000)	(43,000)	0	0.00%		
	4	(762,898)	(187,724)	(187,724)	0	0.00%		
		(820,819)	(230,724)	(230,724)	0	0.00%		
Amount attributable to financing activities		2,971,325	1,269,276	1,269,276	0	0.00%		
MOVEMENT IN SURPLUS OR DEFICIT								
Surplus or deficit at the start of the financial year		3,170,411	3,170,411	3,170,411	0	0.00%		
Amount attributable to operating activities		(1,035,709)	395,850	1,051,147	655,297	165.54%	▲	
Amount attributable to investing activities		(5,106,027)	(3,692,376)	(3,237,020)	455,356	12.33%	▲	
Amount attributable to financing activities		2,971,325	1,269,276	1,269,276	0	0.00%		
Surplus or deficit after imposition of general rates		0	1,143,161	2,253,819	1,110,658		▲	

KEY INFORMATION

▲ ▼ Indicates a variance between Year to Date (YTD) Budget and YTD Actual data as per the adopted materiality threshold.

* Refer to Note 3 for an explanation of the reasons for the variance.

This statement is to be read in conjunction with the accompanying Financial Statements and Notes.

SHIRE OF PERENJORI
STATEMENT OF COMPREHENSIVE INCOME BY PROGRAM
FOR THE PERIOD ENDED 28 FEBRUARY 2025

	Ref Note	Adopted Budget Estimates (a) \$	YTD Budget Estimates (b) \$	YTD Actual (c) \$	Variance* \$ (c) - (b) \$	Variance* % ((c) - (b))/(b) %
OPERATING ACTIVITIES						
Income excluding grants, subsidies and contributions						
Governance	10	93,200	69,692	97,734	28,042	40.24%
General purpose funding - other		3,720,735	3,628,182	3,646,534	18,352	0.51%
Law, order and public safety	13	191,430	97,890	91,249	(6,641)	(6.78%)
Health		726	488	248	(240)	(49.18%)
Housing		278,149	184,174	182,658	(1,516)	(0.82%)
Community amenities		45,840	42,682	41,173	(1,509)	(3.54%)
Recreation and culture	6	92,773	57,831	6,864	(50,967)	(88.13%)
Transport		50,850	45,796	45,532	(264)	(0.58%)
Economic services		619,838	471,880	573,107	101,227	21.45%
Other property and services		1,512,940	1,169,776	1,201,111	31,335	2.68%
		6,606,481	5,768,391	5,886,210	117,819	2.04%
Grants, subsidies and contributions						
Governance		0	0	0	0	0.00%
General purpose funding - other		1,235,315	739,361	741,118	1,757	0.24%
Law, order and public safety		30,732	20,549	21,844	1,295	6.30%
Health		0	0	0	0	0.00%
Education and welfare		0	0	0	0	0.00%
Housing		0	0	0	0	0.00%
Community amenities		525,650	525,650	525,150	(500)	(0.10%)
Recreation and culture		215,458	61,623	0	(61,623)	(100.00%)
Transport		5,571,887	2,495,961	2,477,352	(18,609)	(0.75%)
Economic services		0	0	0	0	0.00%
Other property and services		0	0	0	0	0.00%
		7,579,042	3,843,144	3,765,464	(77,680)	(2.02%)
Expenditure from operating activities (including depreciation)						
Governance		(634,328)	(518,077)	(389,101)	128,976	24.90%
General purpose funding		(188,338)	(122,132)	(107,802)	14,330	11.73%
Law, order and public safety		(504,742)	(344,576)	(344,516)	60	0.02%
Health		(238,246)	(159,408)	(164,657)	(5,249)	(3.29%)
Education and welfare		(165,690)	(113,078)	(97,987)	15,091	13.35%
Housing		(669,657)	(446,017)	(410,123)	35,894	8.05%
Community amenities		(721,070)	(485,311)	(398,928)	86,383	17.80%
Recreation and culture		(2,084,020)	(1,420,330)	(1,302,072)	118,258	8.33%
Transport		(5,607,119)	(3,735,036)	(3,449,438)	285,598	7.65%
Economic services	6	(1,412,585)	(927,336)	(887,794)	39,542	4.26%
Other property and services		(1,511,865)	(1,235,246)	(1,446,625)	(211,379)	(17.11%)
		(13,737,660)	(9,506,547)	(8,999,043)	507,504	5.34%
Net Operating Result		447,863	104,988	652,631		

SHIRE OF PERENJORI
STATEMENT OF FINANCIAL POSITION
FOR THE PERIOD ENDED 28 FEBRUARY 2025

	Supplementary Information	28 February 2025	30 June 2024
		\$	\$
CURRENT ASSETS			
Cash and cash equivalents	3	6,232,461	7,297,147
Trade and other receivables		335,969	618,994
Inventories	8	83,975	84,036
TOTAL CURRENT ASSETS		6,652,405	8,000,177
NON-CURRENT ASSETS			
Other financial assets		83,171	83,171
Inventories		104,000	104,000
Property, plant and equipment		25,932,945	22,863,429
Infrastructure		145,119,617	145,350,631
TOTAL NON-CURRENT ASSETS		171,239,733	168,401,231
TOTAL ASSETS		177,892,138	176,401,408
CURRENT LIABILITIES			
Trade and other payables	9	246,839	638,382
Other liabilities	12	827,085	1,054,446
Borrowings	11	14,921	57,921
Employee related provisions	12	254,753	254,753
TOTAL CURRENT LIABILITIES		1,343,598	2,005,502
NON-CURRENT LIABILITIES			
Borrowings	11	2,568,594	1,068,594
Employee related provisions		63,933	63,929
TOTAL NON-CURRENT LIABILITIES		2,632,527	1,132,523
TOTAL LIABILITIES		3,976,125	3,138,025
NET ASSETS		173,916,013	173,263,383
EQUITY			
Retained surplus		22,614,394	22,149,489
Reserve accounts	4	3,324,662	3,136,938
Revaluation surplus		147,976,957	147,976,956
TOTAL EQUITY		173,916,013	173,263,383

This statement is to be read in conjunction with the accompanying notes.

NOTES TO THE STATEMENT OF FINANCIAL ACTIVITY
FOR THE PERIOD ENDED 28 FEBRUARY 2025

1 BASIS OF PREPARATION AND SIGNIFICANT ACCOUNTING POLICIES

BASIS OF PREPARATION

This prescribed financial report has been prepared in accordance with the *Local Government Act 1995* and accompanying regulations.

Local Government Act 1995 requirements

Section 6.4(2) of the *Local Government Act 1995* read with the *Local Government (Financial Management) Regulations 1996*, prescribe that the financial report be prepared in accordance with the *Local Government Act 1995* and, to the extent that they are not inconsistent with the Act, the Australian Accounting Standards. The Australian Accounting Standards (as they apply to local governments and not-for-profit entities) and Interpretations of the Australian Accounting Standards Board were applied where no inconsistencies exist.

The *Local Government (Financial Management) Regulations 1996* specify that vested land is a right-of-use asset to be measured at cost, and is considered a zero cost concessionary lease. All right-of-use assets under zero cost concessionary leases are measured at zero cost rather than at fair value, except for vested improvements on concessionary land leases such as roads, buildings or other infrastructure which continue to be reported at fair value, as opposed to the vested land which is measured at zero cost. The measurement of vested improvements at fair value is a departure from AASB 16 which would have required the Shire to measure any vested improvements at zero cost.

Local Government (Financial Management) Regulations 1996, regulation 34 prescribes contents of the financial report. Supporting information does not form part of the financial report.

Accounting policies which have been adopted in the preparation of this financial report have been consistently applied unless stated otherwise. Except for cash flow and rate setting information, the financial report has been prepared on the accrual basis and is based on historical costs, modified, where applicable, by the measurement at fair value of selected non-current assets, financial assets and liabilities.

THE LOCAL GOVERNMENT REPORTING ENTITY

All funds through which the Shire controls resources to carry on its functions have been included in the financial statements forming part of this financial report.

All monies held in the Trust Fund are excluded from the financial statements. A separate statement of those monies appears at Note 15 to these financial statements.

Judgements and estimates

The preparation of a financial report in conformity with Australian Accounting Standards requires management to make judgements, estimates and assumptions that effect the application of policies and reported amounts of assets and liabilities, income and expenses.

The estimates and associated assumptions are based on historical experience and various other factors believed to be reasonable under the circumstances; the results of which form the basis of making the judgements about carrying values of assets and liabilities that are not readily apparent from other sources. Actual results may differ from these estimates.

The balances, transactions and disclosures impacted by accounting estimates are as follows:

- estimated fair value of certain financial assets
- impairment of financial assets
- estimation of fair values of land and buildings, infrastructure and investment property
- estimation uncertainties made in relation to lease accounting
- estimated useful life of intangible assets

SIGNIFICANT ACCOUNTING POLICES

Significant accounting policies utilised in the preparation of these statements are as described within the 2023-24 Annual Budget. Please refer to the adopted budget document for details of these policies.

PREPARATION TIMING AND REVIEW

Date prepared: All known transactions up to 28 February 2025

SHIRE OF PERENJORI
NOTES TO THE STATEMENT OF FINANCIAL ACTIVITY
FOR THE PERIOD ENDED 28 FEBRUARY 2025

2 STATEMENT OF FINANCIAL ACTIVITY INFORMATION

		Adopted Budget Closing	Last Year Closing	Year to Date
	Information	30 June 2025	30 June 2024	28 February 2025
(a) Net current assets used in the Statement of Financial Activity				
Current assets		\$	\$	\$
Cash and cash equivalents	3	1,928,426	7,297,147	6,232,461
Trade and other receivables		583,686	618,994	335,969
Inventories	8	127,103	84,036	83,975
		2,639,215	8,000,177	6,652,405
Less: current liabilities				
Trade and other payables	9	(688,593)	(638,382)	(246,839)
Other liabilities	12	0	(1,054,446)	(827,085)
Borrowings	11	(2,000,000)	(57,921)	(14,921)
Employee related provisions	12	(233,540)	(254,753)	(254,753)
		(2,922,133)	(2,005,502)	(1,343,598)
Net current assets		(282,918)	5,994,675	5,308,807
Less: Total adjustments to net current assets	note 2(c)	25,848	(2,824,264)	(3,054,988)
Closing funding surplus / (deficit)		(257,070)	3,170,411	2,253,819

(b) Non-cash amounts excluded from operating activities

The following non-cash revenue and expenditure has been excluded from operating activities within the Statement of Financial Activity in accordance with *Financial Management Regulation 32*.

		Adopted Budget	YTD Budget (a)	YTD Actual (b)
		\$	\$	\$
Non-cash amounts excluded from operating activities				
Adjustments to operating activities				
Less: Profit on asset disposals	6	(43,200)	(40,700)	(40,700)
Add: Loss on asset disposals	6	20,950	5,000	4,999
Add: Depreciation		5,391,127	3,593,920	3,569,266
Total non-cash amounts excluded from operating activities		5,368,877	3,558,220	3,533,565

(c) Current assets and liabilities excluded from budgeted deficiency

The following current assets and liabilities have been excluded from the net current assets used in the Statement of Financial Activity in accordance with *Financial Management Regulation 32* to agree to the surplus/(deficit) after imposition of general rates.

		Adopted Budget Closing	Last Year Closing	Year to Date
		30-Jun-25	30 June 2024	28 February 2025
		\$	\$	\$
Adjustments to net current assets				
Less: Reserve accounts	4	(2,207,692)	(3,136,938)	(3,324,662)
Add: Current liabilities not expected to be cleared at the end of the year:				
- Current portion of borrowings	11	2,000,000	57,921	14,921
- Current portion of employee benefit provisions	4	233,540	254,753	254,753
Total adjustments to net current assets	note 2(c)	25,848	(2,824,264)	(3,054,988)

CURRENT AND NON-CURRENT CLASSIFICATION

In the determination of whether an asset or liability is current or non-current, consideration is given to the time when each asset or liability is expected to be settled. Unless otherwise stated assets or liabilities are classified as current if expected to be settled within the next 12 months, being the Council's operational cycle.

SHIRE OF PERENJORI
SUPPLEMENTARY INFORMATION

TABLE OF CONTENTS

1	Key Information	9
2	Key Information - Graphical	10
3	Cash and Financial Assets	11
4	Reserve Accounts	12
5	Capital Acquisitions	13
6	Asset Disposals	15
7	Receivables	16
8	Other Current Assets	17
9	Payables	18
10	Rate Revenue	19
11	Borrowings	20
12	Other Current Liabilities	21
13	Grants & Contributions	22
14	Capital Grants	23
15	Trust Fund	24
16	Budget Amendments	25

SHIRE OF PERENJORI
SUPPLEMENTARY INFORMATION
FOR THE PERIOD ENDED 28 FEBRUARY 2025

1 KEY INFORMATION

Funding Surplus or Deficit Components

Funding surplus / (deficit)				
	Adopted Budget	YTD Budget (a)	YTD Actual (b)	Var. \$ (b)-(a)
Opening	\$3.17 M	\$3.17 M	\$3.17 M	\$0.00 M
Closing	\$0.00 M	\$1.14 M	\$2.25 M	\$1.11 M

Refer to Statement of Financial Activity

Cash and cash equivalents		
	\$	% of total
Unrestricted Cash	\$2.91 M	46.7%
Restricted Cash	\$3.32 M	53.3%

Refer to 3 - Cash and Financial Assets

Payables	
	% Outstanding
Trade Payables	
0 to 30 Days	99.5%
Over 30 Days	0.5%
Over 90 Days	(5.4%)

Refer to 9 - Payables

Receivables	
	% Collected
Rates Receivable	95.2%
Trade Receivable	
Over 30 Days	82.2%
Over 90 Days	72.6%

Refer to 7 - Receivables

Key Operating Activities

Amount attributable to operating activities			
Adopted Budget	YTD Budget (a)	YTD Actual (b)	Var. \$ (b)-(a)
(\$1.04 M)	\$0.40 M	\$1.05 M	\$0.66 M

Refer to Statement of Financial Activity

Rates Revenue		
YTD Actual	\$	% Variance
\$3.47 M	\$3.47 M	0.1%
YTD Budget		

Refer to 10 - Rate Revenue

Grants and Contributions		
YTD Actual	\$	% Variance
\$0.63 M	\$0.63 M	0.1%
YTD Budget		

Refer to 13 - Grants and Contributions

Fees and Charges		
YTD Actual	\$	% Variance
\$0.88 M	\$0.74 M	19.9%
YTD Budget		

Refer to Statement of Financial Activity

Key Investing Activities

Amount attributable to investing activities			
Adopted Budget	YTD Budget (a)	YTD Actual (b)	Var. \$ (b)-(a)
(\$5.11 M)	(\$3.69 M)	(\$3.24 M)	\$0.46 M

Refer to Statement of Financial Activity

Proceeds on sale		
YTD Actual	\$	%
\$0.07 M	\$0.15 M	(51.9%)
Adopted Budget		

Refer to 6 - Disposal of Assets

Asset Acquisition		
YTD Actual	\$	% Spent
\$2.43 M	\$6.36 M	(61.8%)
Adopted Budget		

Refer to 5 - Capital Acquisitions

Capital Grants		
YTD Actual	\$	% Received
\$3.14 M	\$6.85 M	(54.2%)
Adopted Budget		

Refer to 5 - Capital Acquisitions

Key Financing Activities

Amount attributable to financing activities			
Adopted Budget	YTD Budget (a)	YTD Actual (b)	Var. \$ (b)-(a)
\$2.97 M	\$1.27 M	\$1.27 M	\$0.00 M

Refer to Statement of Financial Activity

Borrowings	
Principal repayments	(\$0.04 M)
Interest expense	(\$0.04 M)
Principal due	\$2.58 M

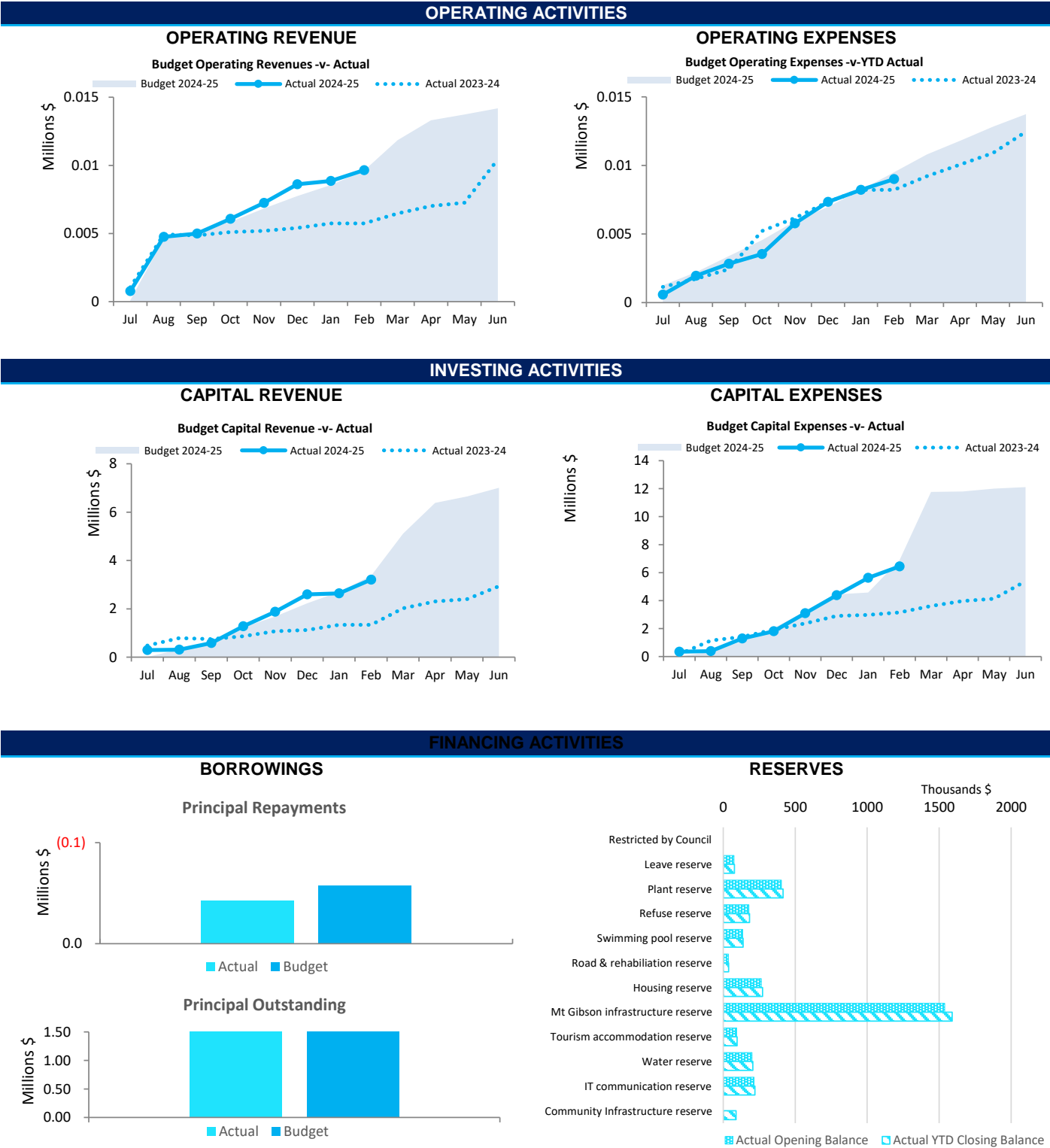
Refer to 11 - Borrowings

Reserves	
Reserves balance	\$3.32 M
Interest earned	\$0.10 M

Refer to 4 - Cash Reserves

This information is to be read in conjunction with the accompanying Financial Statements and notes.

2 KEY INFORMATION - GRAPHICAL



This information is to be read in conjunction with the accompanying Financial Statements and Notes.

SHIRE OF PERENJORI
SUPPLEMENTARY INFORMATION
FOR THE PERIOD ENDED 28 FEBRUARY 2025

3 CASH AND FINANCIAL ASSETS

Description	Classification	Total as per the Financial Statements			Actual Bank Balance	Institution	Interest Rate	Maturity Date
		Unrestricted	Restricted	Cash				
		\$	\$	\$	\$			
Cash on hand								
NAB Municipal Account	Cash and cash equivalents	52,967		52,967	52,824	Bank	0.00%	On Hand
Till Float	Cash and cash equivalents	150		150	150	Bank	0.00%	On Hand
NAB Municipal Telenet Saver Account	Cash and cash equivalents	2,854,681		2,854,681	2,854,681	Bank	3.50%	On Hand
Reserve Funds - Restricted Cash		0	3,324,663	3,324,663				
- Reserve Bank Account 4721	Cash and cash equivalents	0			93,580	Bank	3.50%	On Call
- Mt Gibson Mining Reserve Account 6682	Cash and cash equivalents	0			1,589,947	Bank	3.50%	On Call
- Reserve Term Deposit 0289	Cash and cash equivalents	0			1,641,135	Bank	5.10%	24/06/2025
Total		2,907,798	3,324,663	6,232,461	6,232,318			
Comprising								
Cash and cash equivalents		2,907,798	3,324,663	6,232,461	6,232,318			
		2,907,798	3,324,663	6,232,461	6,232,318			

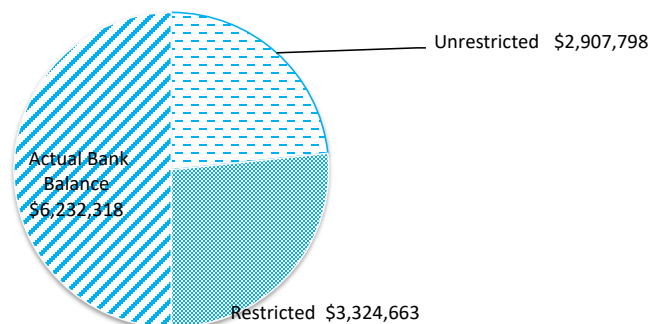
KEY INFORMATION

Cash and cash equivalents include cash on hand, cash at bank, deposits available on demand with banks and other short term highly liquid investments with original maturities of three months or less that are readily convertible to known amounts of cash and which are subject to an insignificant risk of changes in value. Bank overdrafts are reported as short term borrowings in current liabilities in the statement of net current assets.

The local government classifies financial assets at amortised cost if both of the following criteria are met:

- the asset is held within a business model whose objective is to collect the contractual cashflows, and
- the contractual terms give rise to cash flows that are solely payments of principal and interest.

Financial assets at amortised cost held with registered financial institutions are listed in this note other financial assets at amortised cost are provided in Note 4 - Other assets.



SHIRE OF PERENJORI
SUPPLEMENTARY INFORMATION
FOR THE PERIOD ENDED 28 FEBRUARY 2025

4 RESERVE ACCOUNTS

Reserve name	Budget Opening Balance	Budget Interest Earned	Budget Transfer s In (+)	Budget Transfers Out (-)	Budget Closing Balance
	\$	\$	\$	\$	\$
Restricted by Council					
Leave reserve	75,054	2,255	0	(23,614)	53,695
Plant reserve	403,638	10,004	199,053	(250,000)	362,695
Refuse reserve	177,740	3,690	100,000	0	281,430
Swimming pool reserve	134,688	3,362	100,000	0	238,050
Road & rehabilitation reserve	35,567	902	0	0	36,469
Housing reserve	264,726	6,560	0	0	271,286
Mt Gibson infrastructure reserve	1,539,326	49,000	0	(1,357,930)	230,396
Tourism accommodation reserve	93,669	2,378	0	0	96,047
Water reserve	199,086	5,002	0	0	204,088
IT communication reserve	213,444	5,330	95,000	(60,600)	253,174
Community Infrastructure reserve	0	1,517	178,845		180,362
	3,136,938	90,000	672,898	(1,692,144)	2,207,692

Actual Opening Balance	Actual Interest Earned	Actual Transfers In (+)	Actual Transfer s Out (-)	Actual YTD Closing Balance
\$	\$	\$	\$	\$
75,054	2,651	0	0	77,705
403,638	12,413	0	0	416,051
177,740	4,558	0	0	182,298
134,688	3,894	0	0	138,582
35,567	1,059	0	0	36,626
264,726	8,352	0	0	273,078
1,539,326	50,621	0	0	1,589,947
93,669	2,715	0	0	96,384
199,086	5,804	0	0	204,890
213,444	6,273	0	0	219,717
0	1,595	87,789	0	89,384
3,136,938	99,935	87,789	0	3,324,662

5 CAPITAL ACQUISITIONS

Capital acquisitions	Adopted		YTD Actual	YTD Actual Variance
	Budget	YTD Budget		
	\$	\$	\$	\$
Land	20,000	0	0	0
Buildings	4,909,289	3,258,747	3,243,283	(15,464)
Furniture & equipment	70,099	70,099	39,435	(30,664)
Plant & equipment	746,611	740,011	733,452	(6,559)
Acquisition of property, plant and equipment	5,745,999	4,068,857	4,016,170	(52,687)
Roads	5,871,849	2,778,719	2,375,589	(403,130)
Other	491,928	53,000	53,011	11
Acquisition of infrastructure	6,363,777	2,831,719	2,428,600	(508,493)
Total capital acquisitions	12,109,776	6,900,576	6,444,770	(561,181)
Capital Acquisitions Funded By:				
Capital grants and contributions	6,852,449	3,151,900	3,135,050	(16,850)
Borrowings	2,100,000	1,500,000	1,500,000	0
Other (disposals & C/Fwd)	151,300	56,300	72,700	16,400
Reserve accounts				
Leave reserve	23,614	0	0	0
Plant reserve	250,000		0	0
Mt Gibson infrastructure reserve	1,357,930	0	0	0
IT communication reserve	60,600		0	0
Contribution - operations	1,313,883	2,192,376	1,737,019	(455,357)
Capital funding total	12,109,776	6,900,576	6,444,770	(455,806)

SIGNIFICANT ACCOUNTING POLICIES

Each class of fixed assets within either plant and equipment or infrastructure, is carried at cost or fair value as indicated less, where applicable, any accumulated depreciation and impairment losses.

Assets for which the fair value as at the date of acquisition is under \$5,000 are not recognised as an asset in accordance with *Financial Management Regulation 17A (5)*. These assets are expensed immediately.

Where multiple individual low value assets are purchased together as part of a larger asset or collectively forming a larger asset exceeding the threshold, the individual assets are recognised as one asset and capitalised.

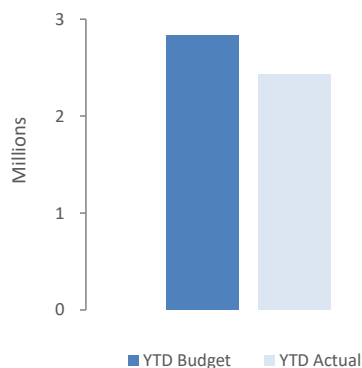
Initial recognition and measurement for assets held at cost

Plant and equipment including furniture and equipment is recognised at cost on acquisition in accordance with *Financial Management Regulation 17A*. Where acquired at no cost the asset is initially recognise at fair value. Assets held at cost are depreciated and assessed for impairment annually.

Initial recognition and measurement between mandatory revaluation dates for assets held at fair value

In relation to this initial measurement, cost is determined as the fair value of the assets given as consideration plus costs incidental to the acquisition. For assets acquired at zero cost or otherwise significantly less than fair value, cost is determined as fair value at the date of acquisition. The cost of non-current assets constructed by the Shire includes the cost of all materials used in construction, direct labour on the project and an appropriate proportion of variable and fixed overheads.

Payments for Capital Acquisitions

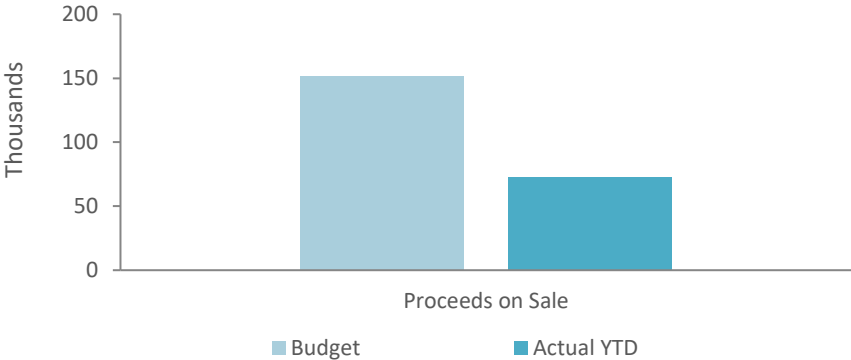


5 CAPITAL ACQUISITIONS - DETAILED

Job -					Original	Amended	YTD			
Account	Job/Account Description	Asset Class	Type	Program	Budget	Budget	Budget	YTD Actual	YTD Actual Variance	Comments
FE03	Altus Payroll	Furniture & Equipment	Governance	New	70,000	-	-	-	-	C/F 23/24
FE04	Library Software	Furniture & Equipment	Recreation and Culture	New	10,600	10,600	10,600	9,100	1,500	C/F 23/24
FE08	Printer Admin	Furniture & Equipment	Administration	New	9,499	9,499	9,499	9,462	37	
FE09	Server - Inhouse	Furniture & Equipment	Administration	New	50,000	50,000	50,000	20,873	29,127	
BC27	Master Key System	Buildings	Administration	New	5,000	5,000	5,000	4,376	624	C/F 23/24
HC28	Additional Housing Timmings & Hirshauer	Buildings	Housing	New	36,000	58,647	58,647	58,627	20	Complete Landscaping C/F23/24
HC32	GROH Housing 2 x 3x2	Buildings	Housing	New	1,200,000	600,000	-	-	-	
I002	Community Hub Project	Buildings	Community Amenities	New	4,107,684	4,107,684	3,172,600	3,159,943	12,657	
BC19	Chalet 3 & 4 Caravan Park	Buildings	Economic Services	Upgrade	30,000	22,500	22,500	20,337	2,163	
HC33	Demolition of gifted property	Infrastructure - Other	Housing	New	40,000	40,000	-	-	-	
I009	Latham Bowls Green	Infrastructure - Other	Recreation and Culture	Upgrade	181,928	181,928	53,000	53,011	(11)	
I008	Latham Refuse Site	Infrastructure - Other	Recreation and Culture	New	50,000	50,000	-	-	-	
LA01	Purchase of Industrial Land	Land	Economic Services	New	20,000	20,000	-	-	-	
I010	Pump Track	Infrastructure - Other	Recreation and Culture	New	150,000	150,000	-	-	-	
I011	Niche Wall Latham Cemetery	Infrastructure - Other	Community Amenities	New	10,000	10,000	-	-	-	
I012	Radio Broadcasting	Infrastructure - Other	Recreation and Culture	New	60,000	60,000	-	-	-	
BC01	Perenjori Pavillion Upgrade	Buildings	Recreation and Culture	Upgrade	-	115,458	-	-	-	
Road Program										
	Warriedar Coppermine Rd SLK 5.2-7.3 Final Seal	Infrastructure - Roads	Transport	Renewal	-	-	-	-	-	
RRG049	Syson Rd SLK 6.00 - 9.00	Infrastructure - Roads	Transport	Renewal	228,799	228,799	228,799	189,112	39,687	
RRG165	Camamah Perenjori Rd SLK 21.39-23.42	Infrastructure - Roads	Transport	Renewal	230,468	230,468	5,468	3,508	1,960	
RRG015	Perenjori Rothsay Rd	Infrastructure - Roads	Transport	Renewal	259,155	259,155	-	-	-	
RRG141	Warriedar Coppermine Rd SLK 6.5-7.30	Infrastructure - Roads	Transport	Renewal	189,191	189,191	-	-	-	
R2R094	Oversby Rd	Infrastructure - Roads	Transport	Renewal	207,146	207,146	29,494	82,268	(52,774)	
R2R014	Grant Rd	Infrastructure - Roads	Transport	Renewal	152,789	152,789	106,162	-	106,162	
R2R009	Hill Rd	Infrastructure - Roads	Transport	Upgrade	261,255	261,255	163,775	79,036	84,739	
R2R123	Crossing Rd Reconstruction/Seal	Infrastructure - Roads	Transport	Upgrade	382,758	382,758	-	-	-	
RC196	Iona Rd Seal	Infrastructure - Roads	Transport	Upgrade	18,810	9,696	9,696	-	9,696	
RC005	Lochada Rd	Infrastructure - Roads	Transport	Upgrade	148,860	148,860	148,860	110,315	38,545	
RC001	Bowgada Rd	Infrastructure - Roads	Transport	Upgrade	62,373	62,373	62,373	37,432	24,941	
MWF039	Morawa South Rd	Infrastructure - Roads	Transport	Upgrade	-	1,054,395	919,128	902,002	17,126	
MWF009	Hill Rd	Infrastructure - Roads	Transport	Upgrade	155,425	155,425	75,425	33,491	41,934	
MWF047	Boundary Rd	Infrastructure - Roads	Transport	Upgrade	2,529,539	2,529,539	1,029,539	938,424	91,115	
Plant replacement program										
04259	Small SUV	Plant & Equipment	Administration	Renewal	40,000	44,531	44,531	44,531	0	
CP36	Utility - Works Crew	Plant & Equipment	Transport	Renewal	65,000	61,505	61,505	61,505	0	
CP37	Works Supervisors Utility	Plant & Equipment	Transport	Renewal	65,000	46,025	46,025	46,025	(0)	
CP39	New Truck	Plant & Equipment	Transport	Renewal	360,000	366,000	366,000	365,091	909	
CP40	Tri Axle Water Tank	Plant & Equipment	Transport	Renewal	150,000	150,000	150,000	143,850	6,150	
CP41	Multi Tyre Roller Attachermt	Plant & Equipment	Transport	New	70,000	71,950	71,950	72,450	(500)	
CP42	Multi Tyre Roller Attachermt	Plant & Equipment	Transport	New	70,000	-	-	-	-	
CP38	Bobcat Trailer	Plant & Equipment	Transport	New	20,000	-	-	-	-	
CP34	EV Charging Station	Plant & Equipment	Transport	New	6,600	6,600	-	-	-	
					11,703,879	12,109,776	6,900,576	6,444,770	455,806	

6 DISPOSAL OF ASSETS

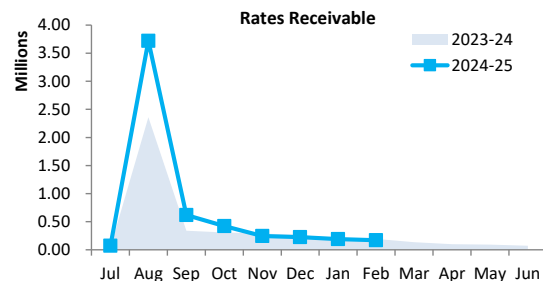
Asset Ref.	Asset description	Budget				YTD Actual			
		Net Book		Profit	(Loss)	Net Book		Profit	(Loss)
		Value	Proceeds			Value	Proceeds		
		\$	\$			\$	\$		
	Plant and equipment								
PE019	Nissan Xtrail	24,000	15,000	19,700	0	0	19,700	19,700	0
PE322	2023 Mitsubishi Triton	32,750	25,000	0	(5,000)	36,999	32,000	0	(4,999)
PE214	Howard Porter Water Tanker	60,950	45,000	0	(15,950)	0	0	0	0
PE145	UD Truck	47,500	50,000	2,500	0	0	0	0	0
PE209	Mitsubishi Triton	19,475	16,300	21,000	0	0	21,000	21,000	0
		184,675	151,300	43,200	(20,950)	36,999	72,700	40,700	(4,999)



7 RECEIVABLES

Rates receivable

	30 June 2024	28 Feb 2025
	\$	\$
Opening arrears previous years	71,227	71,228
Levied this year		3,469,926
Less - collections to date	1	(3,371,999)
Gross rates collectable	71,228	169,155
Net rates collectable	71,228	169,155
% Collected	0.0%	95.2%



Receivables - general

	Credit	Current	30 Days	60 Days	90+ Days	Total
	\$	\$	\$	\$	\$	\$
Receivables - general	(1,563)	31,638	16,094	0	122,099	168,268
Percentage	(0.9%)	18.8%	9.6%	0.0%	72.6%	
Balance per trial balance						
Sundry debtors						168,268
GST receivable						(1,459)
Total receivables general outstanding						166,814

Amounts shown above include GST (where applicable)

KEY INFORMATION

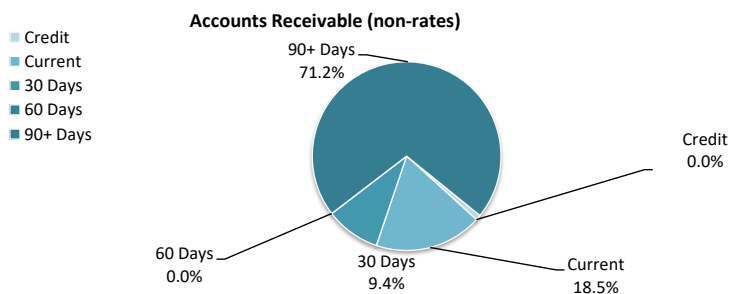
Trade and other receivables include amounts due from ratepayers for unpaid rates and service charges and other amounts due from third parties for goods sold and services performed in the ordinary course of business.

Trade receivables are recognised at original invoice amount less any allowances for uncollectable amounts (i.e. impairment). The carrying amount of net trade receivables is equivalent to fair value as it is due for settlement within 30 days.

Classification and subsequent measurement

Receivables which are generally due for settlement within 30 days except rates receivables which are expected to be collected within 12 months are classified as current assets. All other receivables such as, deferred pensioner rates receivable after the end of the reporting period are classified as non-current assets.

Trade and other receivables are held with the objective to collect the contractual cashflows and therefore the Shire measures them subsequently at amortised cost using the effective interest rate method.



**SHIRE OF PERENJORI
SUPPLEMENTARY INFORMATION
FOR THE PERIOD ENDED 28 FEBRUARY 2025**

OPERATING ACTIVITIES

8 OTHER CURRENT ASSETS

	Opening Balance 1 July 2024	Asset Increase	Asset Reduction	Closing Balance 28 February 202
Other current assets	\$	\$	\$	\$
Inventory				
Stock inventories	84,036	0	(61)	83,975
Total other current assets	84,036	0	(61)	83,975

Amounts shown above include GST (where applicable)

KEY INFORMATION

Inventory

Inventories are measured at the lower of cost and net realisable value.

Net realisable value is the estimated selling price in the ordinary course of business less the estimated costs of completion and the estimated costs necessary to make the sale.

10 RATE REVENUE

General rate revenue

RATE TYPE	Rate in	Number of	Rateable	Rate	Budget	Total	Rate	YTD Actual	Total
	\$ (cents)	Properties	Value	Revenue	Interim	Revenue	Revenue	Interim	Revenue
				\$	\$	\$	\$	\$	\$
Gross rental value									
Townsite	0.076182	106	1,159,333	113,377	0	113,377	111,872	0	111,872
Mining	0.107240	1	3,144,000	304,025	0	304,025	304,025	0	304,025
Unimproved value									
Rural	0.012152	258	155,050,000	2,343,702	0	2,343,702	2,342,918	0	2,342,918
Mining	0.295015	39	2,600,348	840,603	0	840,603	840,603	0	840,603
Exploration	0.179321	35	268,702	59,046	0	59,046	58,324	(11,733)	46,591
Sub-Total		439	162,222,383	3,660,753	0	3,660,753	3,657,743	(11,733)	3,646,009
Minimum payment									
Minimum Payment \$									
Gross rental value									
Townsite	389	32	27,601	11,424	0	11,424	13,056	0	13,056
Mining	389	2	20	408	0	408	408	0	408
Unimproved value									
Rural	0	11	102,300	4,488	0	4,488	5,304	0	5,304
Mining	0	7	2,801	2,040	0	2,040	2,040	0	2,040
Exploration	0	24	23,162	11,424	0	11,424	12,240	287	12,527
Sub-total		76	155,884	29,784	0	29,784	33,048	287	33,335
Discount						(237,790)			(236,915)
Amount from general rates						3,452,747			3,442,429
Ex-gratia rates						27,543	27,496		27,496
Total general rates						3,480,290			3,469,926

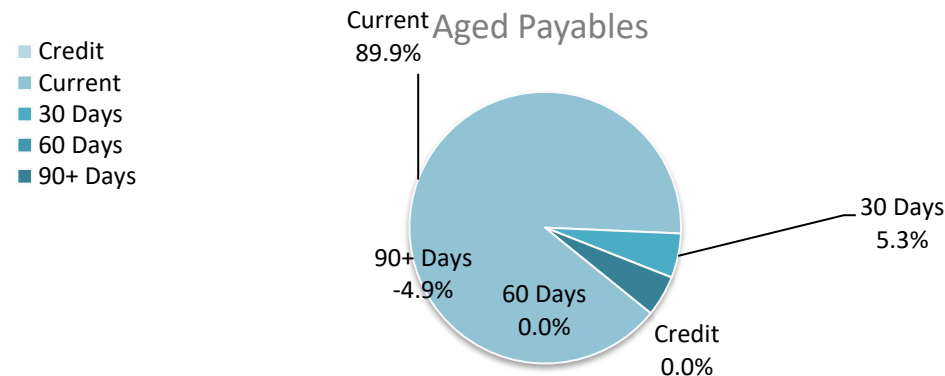
9 PAYABLES

Payables - general	Credit	Current	30 Days	60 Days	90+ Days	Total
	\$	\$	\$	\$	\$	\$
Payables - general	0	24,344	1,434	0	(1,315)	24,464
Percentage	0.0%	99.5%	5.9%	0.0%	-5.4%	
Balance per trial balance						
Payables, current	0	141,430	0	0	0	141,430
ATO liabilities	0	21,041	0	0	0	21,041
Prepaid rates	0	55,950	0	0	0	55,950
Licencing	0	17,151	0	0	0	17,151
Total payables general outstanding						246,839

Amounts shown above include GST (where applicable)

KEY INFORMATION

Trade and other payables represent liabilities for goods and services provided to the Shire prior to the end of the period that are unpaid and arise when the Shire becomes obliged to make future payments in respect of the purchase of these goods and services. The amounts are unsecured, are recognised as a current liability and are normally paid within 30 days of recognition. The carrying amounts of trade and other payables are considered to be the same as their fair values, due to their short-term nature.



11 BORROWINGS

Repayments - borrowings

Information on borrowings Particulars		Loan No.	New Loans		Principal Repayments		Principal Outstanding		Interest Repayments		
			1 July 2024	Actual	Budget	Actual	Budget	Actual	Budget	Actual	Budget
			\$	\$	\$	\$	\$	\$	\$	\$	
CHA Housing	96	0	0	0	0	0	0	0	5	0	
John Street Subdivision	98	126,515	0	0	(28,456)	(28,456)	98,059	98,059	(9,300)	(8,450)	
Council Housing	99	1,000,000	0	0	(14,544)	(29,465)	985,456	970,535	(30,407)	(58,334)	
Shop Development	100	0	1,500,000	1,500,000	0	0	1,500,000	1,500,000	0	0	
GROH Housing	101	0	0	600,000	0	0	0	600,000	0	0	
Total			1,126,515	1,500,000	2,100,000	(43,000)	(57,921)	2,583,515	3,168,594	(39,702)	(66,784)
Current borrowings			57,921				14,921				
Non-current borrowings			1,068,594				2,568,594				
			1,126,515				2,583,515				

All debenture repayments were financed by general purpose revenue.

KEY INFORMATION

Borrowing costs are recognised as an expense when incurred except where they are directly attributable to the acquisition, construction or production of a qualifying asset. Where this is the case, they are capitalised as part of the cost of the particular asset until such time as the asset is substantially ready for its intended use or sale.

Fair values of borrowings are not materially different to their carrying amounts, since the interest payable on those borrowings is either close to current market rates or the borrowings are of a short term nature. Non-current borrowings fair values are based on discounted cash flows using a current borrowing rate.

12 OTHER CURRENT LIABILITIES

	Note	Opening Balance 1 July 2024 \$	Liability transferred from/(to) non current \$	Liability Increase \$	Liability Reduction \$	Closing Balance 28 February 2025 \$
Other current liabilities						
Other liabilities						
Contract liabilities		1,040,935	0	967,394	(1,213,438)	794,891
Capital grant/contributions liabilities		13,511	0	18,683	0	32,194
Total other liabilities		1,054,446	0	986,077	(1,213,438)	827,085
Employee Related Provisions						
Employee provisions		254,753	0	0	0	254,753
Total Provisions		254,753	0	0	0	254,753
Total other current liabilities		1,309,199	0	986,077	(1,213,438)	1,081,838

Amounts shown above include GST (where applicable)

A breakdown of contract liabilities and associated movements is provided on the following pages at Note 13 and 14

KEY INFORMATION

Provisions

Provisions are recognised when the Shire has a present legal or constructive obligation, as a result of past events, for which it is probable that an outflow of economic benefits will result and that outflow can be reliably measured.

Provisions are measured using the best estimate of the amounts required to settle the obligation at the end of the reporting period.

Employee Related Provisions

Short-term employee benefits

Provision is made for the Shire's obligations for short-term employee benefits. Short-term employee benefits are benefits (other than termination benefits) that are expected to be settled wholly before 12 months after the end of the annual reporting period in which the employees render the related service, including wages, salaries and sick leave. Short-term employee benefits are measured at the (undiscounted) amounts expected to be paid when the obligation is settled.

The Shire's obligations for short-term employee benefits such as wages, salaries and sick leave are recognised as a part of current trade and other payables in the calculation of net current assets.

Other long-term employee benefits

The Shire's obligations for employees' annual leave and long service leave entitlements are recognised as employee related provisions in the statement of financial position.

Long-term employee benefits are measured at the present value of the expected future payments to be made to employees. Expected future payments incorporate anticipated future wage and salary levels, durations of service and employee departures and are discounted at rates determined by reference to market yields at the end of the reporting period on government bonds that have maturity dates that approximate the terms of the obligations. Any remeasurements for changes in assumptions of obligations for other long-term employee benefits are recognised in profit or loss in the periods in which the changes occur. The Shire's obligations for long-term employee benefits are presented as non-current provisions in its statement of financial position, except where the Shire does not have an unconditional right to defer settlement for at least 12 months after the end of the reporting period, in which case the obligations are presented as current provisions.

Contract liabilities

An entity's obligation to transfer goods or services to a customer for which the entity has received consideration (or the amount is due) from the customer.

Capital grant/contribution liabilities

Grants to acquire or construct recognisable non-financial assets to identified specifications be constructed to be controlled by the Shire are recognised as a liability until such time as the Shire satisfies its obligations under the agreement.

SHIRE OF PERENJORI
SUPPLEMENTARY INFORMATION
FOR THE PERIOD ENDED 28 FEBRUARY 2025

OPERATING ACTIVITIES

13 GRANTS, SUBSIDIES AND CONTRIBUTIONS

Provider	Program	Unspent grant, subsidies and contributions liability					Grants, subsidies and contributions revenue		
		Liability	Increase in	Decrease in	Liability	Current	Adopted	YTD	YTD
		1 July 2024	Liability	Liability	28 Feb 2025	Liability	Budget	Budget	Revenue
		\$	\$	\$	\$	\$	\$	\$	\$
Grants and subsidies									
DFES Operating Grant	Law, Order & Public Safety	0	0	0	0	0	30,732	20,549	21,844
Direct Grant	Transport	0	0	0	0	0	333,103	333,103	333,103
Grants Commission Grant	General purpose funding	0	0	0	0	0	228,150	171,112	171,112
Untied Road Grant	General purpose funding	0	0	0	0	0	105,608	79,208	79,205
Doantion Bush Telegraph Committee	General purpose funding	13,511	0	0	13,511	13,511	0	0	0
Volunteers & Seniors Grants	Community Services	0	0	0	0	0	25,650	25,650	25,150
Main Roads Street Lighting Subsidy	Transport	0	0	0	0	0	3,350	0	0
Regional Childcare (Reed Perenjori Project)	Education	0	18,684	0	18,684	18,684	0	0	0
		13,511	18,684	0	32,195	32,195	726,593	629,621	630,414

14 CAPITAL GRANTS, SUBSIDIES AND CONTRIBUTIONS

Provider	Program	Capital grant/contribution liabilities				Capital grants, subsidies and contributions revenue		
		Liability	Increase in Liability	Decrease in Liability	Liability	Adopted Budget	YTD	YTD Revenue
		1 July 2024		(As revenue)	28 Feb 2025	Revenue	Budget	Actual
		\$	\$	\$	\$	\$	\$	\$
Capital grants and subsidies								
LR&CIP Funding	General Purpose Funding	540,935	0	(490,801)	50,134	901,557	489,042	490,801
Department of Industry (Evacuation Centre Grant)	General Purpose Funding	0	0	0	0	0	0	0
DFES Resilience Grant	Community Amenities	500,000	0	(500,000)	(0)	500,000	500,000	500,000
Regional Road Group Funding	Transport	0	280,533	(122,666)	157,867	548,000	122,666	122,666
Roads To Recovery Funding	Transport	0	621,190	(161,304)	459,886	941,475	163,824	161,304
Mid West Secondary Grain Freight Network Grant	Transport	0	0	0	0	3,739,359	1,876,368	1,860,279
EV Charging Ststion	Transport	0	0	0	0	6,600	0	0
Deparment Sport and Rec	Recreation and Culture	0	0	0	0	100,000	0	0
Lottery West	Recreation and Culture	0	127,004	0	127,004	115,458	0	0
		1,040,935	1,028,727	(1,274,771)	794,890	6,852,449	3,151,900	3,135,050

SHIRE OF PERENJORI
SUPPLEMENTARY INFORMATION
FOR THE PERIOD ENDED 28 FEBRUARY 2025

15 TRUST FUND

Funds held at balance date which are required by legislation to be credited to the trust fund and which are not included in the financial statements are as follows:

Description	Opening Balance	Amount	Amount	Closing Balance
	1 July 2024	Received	Paid	28 Feb 2025
	\$	\$	\$	\$
Mount Gibson Public Benefit Fund	276,889	1,462	0	278,351
	276,889	1,462	0	278,351

SHIRE OF PERENJORI
SUPPLEMENTARY INFORMATION
FOR THE PERIOD ENDED 28 FEBRUARY 2025

16 BUDGET AMENDMENTS

Amendments to original budget since budget adoption. Surplus/(Deficit)

Description	Council Resolution	Classification	Non Cash Adjustment	Increase in Available Cash	Decrease in Available Cash	Amended Budget Running Balance
			\$	\$	\$	\$
Budget adoption		Surplus/(Deficit)		42,453		42,453
Budget Review Operating Revenue		Operating revenue			(790,395)	(747,942)
Budget Review Operating Expenditure		Operating expenses		838,453		90,511
Budget Review Capital Grants and Subsidies		Operating revenue		1,191,470		1,281,981
Budget Review Capital Works		Capital expenses			(1,069,286)	212,695
Budget Review Proceeds from New Borrowings		Operating revenue		100,000		312,695
Budget Review Transfers from Reserves		Operating revenue			(262,070)	50,625
Budget Review Non Cash Depreciation, Profit, Loss		Operating revenue		5,000		55,625
Budget Review Transfers to Reserves		Non cash item			(55,625)	0
				2,177,376	(2,177,376)	0

13.2 SCHEDULE OF FINANCIAL ACTIVITY FOR THE PERIOD ENDED 28 FEBRUARY 2025

Applicant:	Shire of Perenjori
File:	ADM 0082
Report Date:	20 March 2025
Disclosure of Interest:	
Voting Requirements:	Simple Majority
Author:	Gypsie Douglas – Finance Officer
Responsible Officer:	Ally Bryant – Finance Manager
Attachments:	13.2.1 - Accounts for Payment 28 February 2025 13.2.2 – Corporate Credit Card Statement & Breakdown (NAB)

Summary

Council is presented the list of payments made from the Municipal, Trust and Reserve Accounts under delegation since the last Ordinary Council Meeting.

Background

Council delegates authority to the Chief Executive Officer annually:

To make payments from Trust, Reserve and Municipal Fund;

To purchase goods and services to a value of not more than \$250,000;

Legal Compliance

Local Government Act 1995

S6.5. Accounts and records

Local Government (Financial Management) Regulations 1996

R11. Payments, procedures for making etc.

R12. Payments from municipal fund or trust fund, restrictions on making

(1) A payment may only be made from the municipal fund or the trust fund —

(a) if the local government has delegated to the CEO the exercise of its power to make payments from those funds — by the CEO; or

(b) otherwise, if the payment is authorised in advance by a resolution of the council.

(2) The council must not authorise a payment from those funds until a list prepared under regulation 13(2) containing details of the accounts to be paid has been presented to the council.

S13. Payments from municipal fund or trust fund by CEO, CEO's duties as to etc.

(1) If the local government has delegated to the CEO the exercise of its power to make payments from the municipal fund or the trust fund, a list of accounts paid by the CEO is to be prepared each month showing for each account paid since the last such list was prepared —

(a) the payee's name; and

(b) the amount of the payment; and

(c) the date of the payment; and

(d) sufficient information to identify the transaction.

(2) A list of accounts for approval to be paid is to be prepared each month showing —

(a) for each account which requires council authorisation in that month —

(i) the payee's name; and

(ii) the amount of the payment; and

(iii) sufficient information to identify the transaction; and

(b) the date of the meeting of the council to which the list is to be presented.

(3) A list prepared under sub-regulation (1) or (2) is to be —

- (a) presented to the council at the next ordinary meeting of the council after the list is prepared; and
- (b) recorded in the minutes of that meeting.

Policy Implications

Nil

Council Policy Compliance

Payments are checked to ensure compliance with Council's Purchasing Policy Number 4007 – Procurement Policy.

Financial Implications

All payments are made in accordance with the adopted annual budget.

Strategic Community Plan

Goal 4: A strong and diverse Council working closely with the proactive and involved community.

4.6. The organisation, assets and finances of the Shire are managed responsibly.

Consultation

Paul Anderson – Chief Executive Officer

Ally Bryant – Finance Manager

Officer Comment

Accounts paid for the month ending 28 February 2025.

Municipal Account	
EFT 18643 - 18728	\$941,604.32
Direct Debits	\$80,399.02
Cheques	\$0.00
Corporate MasterCard	\$2,523.12
Bank Fees	\$ 329.68
Total	\$1,024,856.14

Trust Account – Mt Gibson Public Benefit Funds	
EFT – Transfer to another account (Close Term Deposit)	\$0.00
Cheques	\$0.00
Bank Fees	\$0.00
Total	\$0.00

Totalling **\$1,024,856.14** from *Municipal* and *Trust Accounts* for the month ending **28 February 2025**.

OFFICER RECOMMENDATION

Council Resolution Number:

Moved:

Seconded:

That the cheques and electronic payments as per the attached schedules of accounts for payment totaling \$1,024,856.14 (One million and twenty-four thousand, eight hundred and fifty six dollars and fourteen cents) be accepted.

Motion put and carried / lost

For:

Against:

[Next Item](#)

Shire of Perenjori
Local Government Act 1995
Accounts for Payment for Month Ended 28 February 2025

Cheque /EFT No	Date	Name	Invoice Description	Bank Code	INV Amount	Amount
482	28/02/2025	EFT GST - EFTPOS FEES WITH GST	EFTPOS FEES WITH GST	M		93.59
482	28/02/2025	EFT GST - EFTPOS FEES WITH GST	EFTPOS FEES WITH GST	M		137.14
482	28/02/2025	FEE GST - BANK FEES WITH GST	NAB CONNECT FEE	M		24.59
482	28/02/2025	FEE GST - BANK FEES WITH GST	BPAY CHARGE	M		41.36
482	28/02/2025	BANK FEES - BANK FEES NO GST	ACCOUNT FEE	M		33.00
EFT18643	06/02/2025	AVON WASTE	Replacement bin x 6 plus delivery	M		1,284.00
EFT18644	06/02/2025	BOB WADDELL & ASSOCIATES PTY LTD	Rates services w/e 26/01/25	M		396.00
EFT18645	06/02/2025	BOC LIMITED	Monthly Gas Bottle Rental for Depot & Medical Centre - January 25	M		23.15
EFT18646	06/02/2025	CANINE CONTROL	Ranger Services - 23/01/25	M		909.92
EFT18647	06/02/2025	CHILD SUPPORT AGENCY	Payroll deductions	M		310.55
EFT18648	06/02/2025	CNW PTY LTD	Corromount stock sheet x 3 plus freight - 25 Timmings	M		196.30
EFT18649	06/02/2025	CRANECORP AUSTRALIA PTY LTD	Operator 15T Crane DS plus travel - Depot	M		337.19
EFT18650	06/02/2025	DEPARTMENT OF MINES, INDUSTRY REGULATION AND SAFETY	BSL fees collected for October & November 24	M		4,750.56
EFT18651	06/02/2025	DONGARA MID WEST WASTE	Pump septic tanks including travel & disposal - Caravan Park	M		1,700.00
EFT18652	06/02/2025	GFG TEMP ASSIST	Glen Flood Support for design & construction for Supermarket - January 25	M		922.35
EFT18653	06/02/2025	GH COUNTRY COURIER	Freight – Reece & CNW	M		132.66
EFT18654	06/02/2025	HERSEY'S SAFETY PTY LTD	Xmas tree clips, blanking grommets, wire brush plus freight - Depot	M		171.05

Shire of Perenjori
Local Government Act 1995
Accounts for Payment for Month Ended 28 February 2025

Cheque /EFT No	Date	Name	Invoice Description	Bank Code	INV Amount	Amount
EFT18655	06/02/2025	HITACHI	Gasket & 1000hr filter kit - PJ1577	M		1,928.19
EFT18656	06/02/2025	INDEPENDENT RURAL PTY LTD	Hardware & Garden supplies – January 25	M		494.82
EFT18657	06/02/2025	INTEGRATED ICT	Monthly subscriptions – December 24, January 25	M		5,814.44
EFT18658	06/02/2025	IT VISION (READYTECH)	Migration of SynergySoft, Universe & Web Service - New Server	M		3,395.70
EFT18659	06/02/2025	KINGS WA PTY LTD	Modifications to MIS Ute Tray - 1500PJ	M		385.00
EFT18660	06/02/2025	MOORA GLASS SERVICE	Supply & fit sliding door with fly door plus travel - Unit 3, North Rd	M		4,111.80
EFT18661	06/02/2025	MOORE AUSTRALIA (WA) PTY LTD	2025 Budget Workshop for Finance Manager - 28/02/24	M		1,430.00
EFT18662	06/02/2025	MORAWA REFRIGERATION	Supply & install Split System - 5 Hirshauer & Chalet 1	M		4,313.70
EFT18663	06/02/2025	O'BRIEN SMASH REPAIRS	Repair Ford Ranger - Insurance Claim - 1HIP871 CESM Vehicle	M		5,735.44
EFT18664	06/02/2025	PERENJORI ROADHOUSE	Cleaning supplies - Hall, Medical Centre, Admin, C/Park, Village	M		138.62
EFT18665	06/02/2025	PETER EGAN CARPENTRY	Remove & replace laundry handrail includes painting & installation - 2 John St. Plumb rainwater tank – 25 Timmings	M		561.00
EFT18666	06/02/2025	PJC SERVICES & CO PLUMBING & GAS	Replace drains & leach drains - Wellness Centre	M		3,850.00
EFT18667	06/02/2025	REECE PTY LTD	Anode aluminium - Pool	M		297.00
EFT18668	06/02/2025	REPCO AUTO PARTS	Various filters & freight – Various Plant	M		668.80
EFT18669	06/02/2025	RJ & LJ KING	Tyre puncture repair - 43PJ	M		49.50
EFT18670	06/02/2025	SHIRE OF IRWIN	EHO Services - October to December 24	M		132.00
EFT18671	06/02/2025	SHIRE OF MINGENEW	Damstra Online Training Platform & usage fee - Quarterly Fee - October to December 24	M		241.18

Shire of Perenjori
Local Government Act 1995
Accounts for Payment for Month Ended 28 February 2025

Cheque /EFT No	Date	Name	Invoice Description	Bank Code	INV Amount	Amount
EFT18672	06/02/2025	STEVEN TWEEDIE	Consultant to undertake a full review of the Delegation Register	M		3,300.00
EFT18673	06/02/2025	STRATUM CUTTINGS EDGES	Grader blade x 10	M		2,200.00
EFT18674	06/02/2025	TEAM GLOBAL EXPRESS PTY LTD	Freight - State Library & Westrac	M		111.38
EFT18675	06/02/2025	TOTALLY WORKWEAR	Steel Blue Argle Boots & Oliver Boots - Depot	M		392.49
EFT18676	06/02/2025	TRUCK CENTRE (WA) PTY LTD	Blower motor - PJ1578	M		1,005.50
EFT18677	06/02/2025	WESTRAC PTY LTD	Valve & Seal - PJ1585. Filter & Cover – PJ1524. Harness – PJ1585 M & freight			1,060.29
EFT18678	06/02/2025	WINC AUSTRALIA PTY LIMITED	Monthly Meter Charges - 16/12/24 to 20/01/25	M		248.46
EFT18679	12/02/2025	BREFFNI GROUP PTY LTD	Progress Claim 07 - RFT02-2023/24 Design and Construction of a 500 sqm Supermarket	M		446,726.48
EFT18680	12/02/2025	FULCHER CONTRACTORS	MWSFN - Road Widening Work - January 2025 Claim	M		175,485.14
EFT18681	13/02/2025	Fleetcare Pty Ltd	Ford Ranger Lease (1IHC839) – November 24 to January 25	M		6,476.10
EFT18682	20/02/2025	ANDREW FRASER	Welcome to Country - Australia Day 2025	M		500.00
EFT18683	20/02/2025	APPLIED SATELLITE TECHNOLOGY AUSTRALIA PTY LTD	Monthly Subscription & AST Service Fee - February 2025	M		79.95
EFT18684	20/02/2025	AQUATIC SERVICES WA PTY LTD	Paliontest Service & Calibration - Pool	M		605.00
EFT18685	20/02/2025	AUSTRALIA POST	Monthly postage fees - January 25	M		98.20
EFT18686	20/02/2025	AVON WASTE	Waste removal - January 25	M		3,807.00
EFT18687	20/02/2025	BATAVIA FURNITURE & BEDDING	Black Aston Dining Chair x 3 - Chalet 3 & 4	M		237.00
EFT18688	20/02/2025	BOB WADDELL & ASSOCIATES PTY LTD	Rates services week ending 09/02/25	M		792.00
EFT18689	20/02/2025	BURGESS RAWSON (WA) PTY LTD	L3206 Fowler St Perenjori (OPP #32 & OPP #44) - water usage	M		2,438.58

Shire of Perenjori
Local Government Act 1995
Accounts for Payment for Month Ended 28 February 2025

Cheque /EFT No	Date	Name	Invoice Description	Bank Code	INV Amount	Amount
			10/12/24-11/02/25. L6808 Opp Allan Britt St – water usage 11/12/24-11/02/25			
EFT18690	20/02/2025	CANINE CONTROL	Ranger Services – 31/01/25 & 04/02/2025	M		1,819.84
EFT18691	20/02/2025	CHILD SUPPORT AGENCY	Payroll deductions	M		310.55
EFT18692	20/02/2025	CJD EQUIPMENT	Hosetenna - PJ6034	M		146.40
EFT18693	20/02/2025	DIAMANDIA PTY LTD	Rates refund for assessment A15324 E70/06524 MINING TENEMENT PERENJORI WA 6620	M		8,745.21
EFT18694	20/02/2025	FVS FIRE PTY LTD	Replace fire equipment due five yearly testing - various buildings	M		7,795.70
EFT18695	20/02/2025	GH COUNTRY COURIER	Freight - Reece Plumbing	M		81.18
EFT18696	20/02/2025	GREAT SOUTHERN FUEL	Transmax Agri Trans Plus 80W - PJ1503. Grease – Depot. Oil - PJ1526 & 1500PJ	M		2,319.46
EFT18697	20/02/2025	GREENFIELD TECHNICAL SERVICES	Provide Superintendent Representative services - MWSFN work program	M		14,998.50
EFT18698	20/02/2025	IKONYX MEDICAL SERVICES PTY LTD	Medical Practice Support Retainer - Feb 25	M		2,291.66
EFT18699	20/02/2025	INDEPENDENT RURAL PTY LTD	Hardware & Garden supplies – Feb 25	M		447.26
EFT18700	20/02/2025	INTEGRATED ICT	Monthly subscriptions - January 25	M		5,776.65
EFT18701	20/02/2025	J'S HARDWARE & GIFTS	Garden & Hardware Supplies - January 25	M		3,032.55
EFT18702	20/02/2025	JANICE KAYE GELLATLY	Rates refund for assessment A726 58 FOWLER STREET PERENJORI 6620	M		470.29
EFT18703	20/02/2025	KINGS WA PTY LTD	Tree Removal on Hill Rd - January 25	M		8,984.25
EFT18704	20/02/2025	LEAH C LEOPOLD	Christmas Lights Competition - 1st Prize	M		300.00
EFT18705	20/02/2025	MCLEODS LAWYERS	Road User Agreement: Extension Hill Mine Site (Mount Gibson)	M		652.79

Shire of Perenjori
Local Government Act 1995
Accounts for Payment for Month Ended 28 February 2025

Cheque /EFT No	Date	Name	Invoice Description	Bank Code	INV Amount	Amount
EFT18706	20/02/2025	MIDWEST DIESEL	to Rothsay - Terra Mining. Supermarket Lease on Fowler St Replace fan motor - PJ1577	M		401.50
EFT18707	20/02/2025	ON HOLD ONLINE	Monthly on Hold Messages - January 25	M		77.00
EFT18708	20/02/2025	PERENJORI COMMUNITY RESOURCE CENTRE	Advertising & promotion for LIFE Program, Subscription for Rates advertising & Bush Telegraph Issues	M		2,025.00
EFT18709	20/02/2025	PERENJORI MEDICAL CENTRE	Pre-employment medical for Robert Gronow & Chloe Heard	M		627.00
EFT18710	20/02/2025	PERENJORI PRIMARY SCHOOL P&C ASSOCIATION INC	Catering for Senior Citizens Dinner 2024	M		500.00
EFT18711	20/02/2025	PERENJORI ROADHOUSE	Supplies for Depot, C/Park, Admin, CDO events	M		1,857.96
EFT18712	20/02/2025	PETER EGAN CARPENTRY	Install new skirting - Chalet 3 & 4. Supply & install skylights - Fowler St Toilets	M		3,294.50
EFT18713	20/02/2025	PJC SERVICES & CO PLUMBING & GAS	Repair sewer leak - 8A John. Repair sewer & replace IO - 38A Russell. Unblock urinal drain - Old Ablutions C/Park. Replace shower taps - 24 Livingstone	M		797.50
EFT18714	20/02/2025	RAY WHITE GERALDTON	Routine Housing Inspection – Oct 24 & Management Fee - Nov 24	M		9,163.00
EFT18715	20/02/2025	REDFISH TECHNOLOGIES PTY LTD	Supply & commission Audio Recording system - Council	M		1,509.22
EFT18716	20/02/2025	REPCO AUTO PARTS	Oil filter x 5 & freight - Various vehicles. Resistor – PJ1569	M		128.47
EFT18717	20/02/2025	RJ & LJ KING	Tyre x 4 & tyre rotation - PJ1574. Battery – PJ1579, 1516PJ. Strip & fit tyre – PJ1559	M		4,043.60
EFT18718	20/02/2025	SHERMAC AUSTRALIA PTY LTD	2025 RS3000 Semi Water Cart - 1UAR277	M		158,225.00
EFT18719	20/02/2025	SINCH MESSAGEMEDIA	Monthly Messaging Service - January 25	M		539.56
EFT18720	20/02/2025	STEWART & HEATON CLOTHING CO. PTY LTD	Uniform - Fire Brigade Volunteers	M		5,726.81

Shire of Perenjori
Local Government Act 1995
Accounts for Payment for Month Ended 28 February 2025

Cheque /EFT No	Date	Name	Invoice Description	Bank Code	INV Amount	Amount
EFT18721	20/02/2025	SUZANNE NOELLE TATE	Christmas Lights Competition - 3rd Prize	M		100.00
EFT18722	20/02/2025	TEAM GLOBAL EXPRESS PTY LTD	Freight - Totally workwear, Geraldton Mowers, Winc, Westrac & Sunny Industrial Brushware	M		393.33
EFT18723	20/02/2025	TOWN PLANNING INNOVATIONS PTY LTD	Planning Services - 0.75hrs	M		82.50
EFT18724	20/02/2025	TRUCKLINE	Bush, Airbrake tube - PJ6034 & PJ1527	M		141.93
EFT18725	20/02/2025	WALLACE PLUMBING AND GAS PTY LTD	Installed kitchen sink, sink mixer, shower rose - 34 Livingstone. Replaced washing machine taps & hoses – C/Park Village	M		826.27
EFT18726	20/02/2025	WALLIS COMPUTER SOLUTIONS	Technician Support for Audio Recording - Chambers	M		1,590.88
EFT18727	20/02/2025	WESTRAC PTY LTD	Belt x 2 - PJ1585	M		83.23
EFT18728	20/02/2025	WINC AUSTRALIA PTY LIMITED	Stationery supplies - Admin & Depot	M		1,053.28
DD15283.1	11/02/2025	AWARE SUPER	Payroll deductions	M		9,893.68
DD15283.2	11/02/2025	UNISUPER	Superannuation contributions	M		285.62
DD15283.3	11/02/2025	HUB24 SUPERANNUATION	Superannuation contributions	M		69.60
DD15283.4	11/02/2025	REST INDUSTRY SUPERANNUATION	Superannuation contributions	M		600.12
DD15283.5	11/02/2025	THE RL & JMA RYAN SUPERANNUATION FUND	Superannuation contributions	M		1,143.86
DD15283.6	11/02/2025	AUSTRALIAN RETIREMENT TRUST (SUPER SAVINGS)	Superannuation contributions	M		744.05
DD15283.7	11/02/2025	EXPAND EXTRA SUPER	Superannuation contributions	M		612.88
DD15283.8	11/02/2025	AUSTRALIAN SUPER	Superannuation contributions	M		2,402.29
DD15283.9	11/02/2025	MERCER SUPER TRUST	Superannuation contributions	M		657.25
DD15290.1	07/02/2025	SYNERGY	Electricity usage 25/11/2024-24/12/2024 - Streetlights	M		3,406.48

Shire of Perenjori
Local Government Act 1995
Accounts for Payment for Month Ended 28 February 2025

Cheque /EFT No	Date	Name	Invoice Description	Bank Code	INV Amount	Amount
DD15290.2	10/02/2025	SYNERGY	Electricity usage 20/12/2024-15/01/2025 - Oval	M		54.57
DD15290.3	10/02/2025	TELSTRA CORPORATION LIMITED	Main Account to 15/01/25	M		3,098.54
DD15290.4	03/02/2025	WESTNET	Monthly Internet Charge for Museum, Library & Depot - Jan to Feb 25	M		123.90
DD15290.5	11/02/2025	RMS (Aust) P/L	RMS Online Fees - January 2025	M		66.00
DD15290.6	10/02/2025	RMS (Aust) P/L	RMS Cloud & Support - February 25	M		484.00
DD15290.7	03/02/2025	NODE ONE	N1 Business Fibre - February 25	M		1,100.00
DD15290.8	05/02/2025	SYNERGY	Electricity usage 22/10/24-18/12/24 - C/Park & House	M		1,643.12
DD15290.9	06/02/2025	SYNERGY	Electricity usage 13/12/24-15/01/25 - Pool	M		3,227.30
DD15297.1	25/02/2025	AWARE SUPER	Payroll deductions	M		9,848.41
DD15297.2	25/02/2025	UNISUPER	Superannuation contributions	M		293.98
DD15297.3	25/02/2025	HUB24 SUPERANNUATION	Superannuation contributions	M		311.14
DD15297.4	25/02/2025	REST INDUSTRY SUPERANNUATION	Superannuation contributions	M		600.12
DD15297.5	25/02/2025	THE RL & JMA RYAN SUPERANNUATION FUND	Superannuation contributions	M		1,143.86
DD15297.6	25/02/2025	AUSTRALIAN RETIREMENT TRUST (SUPER SAVINGS)	Superannuation contributions	M		744.05
DD15297.7	25/02/2025	EXPAND EXTRA SUPER	Superannuation contributions	M		521.39
DD15297.8	25/02/2025	AUSTRALIAN SUPER	Superannuation contributions	M		2,547.74
DD15297.9	25/02/2025	MERCER SUPER TRUST	Superannuation contributions	M		657.25

Shire of Perenjori
Local Government Act 1995
Accounts for Payment for Month Ended 28 February 2025

Cheque /EFT No	Date	Name	Invoice Description	Bank Code	INV Amount	Amount
DD15299.1	14/02/2025	REFUEL AUSTRALIA	Fuel Card Purchases for January 2025	M		25,956.15
DD15299.2	17/02/2025	SG FLEET AUSTRALIA PTY LIMITED	CESM FORD RANGER - Fuel charges	M		265.62
DD15299.3	24/02/2025	NAB CREDIT CARD	Credit Card purchases 28/12/24 to 29/01/25	M		2,523.12
DD15302.1	28/02/2025	WATER CORPORATION	Water usage 10/12/24-11/02/25 - Housing	M		6,483.67
DD15283.10	11/02/2025	HOST PLUS SUPER	Superannuation contributions	M		395.05
DD15283.11	11/02/2025	AMP SUPER FUND	Superannuation contributions	M		311.14
DD15297.10	25/02/2025	HOST PLUS SUPER	Superannuation contributions	M		395.05
DD15297.11	25/02/2025	AMP SUPER FUND	Superannuation contributions	M		311.14
			TOTAL			1,024,856.14



Statement for

NAB Low Rate Business Card

NAB Commercial Cards Centre - GPO Box 9992 Melbourne Victoria 3001

Tel 13 10 12 8am - 8pm AEST & AEDT Monday to Friday, 9am - 6pm AEST & AEDT Saturday and Sunday

Fax 1300 363 658

Lost & Stolen cards: 1800 033 103 (24 hours within Australia only)



//057

SHIRE OF PERENJORI

PO BOX 22

PERENJORI WA 6620

Statement Period

30 January 2025 to 28 February 2025

Company Account No:

4336 8799 1062 5536

Facility Limit:

\$22,000

Your Account Summary

Balance from previous statement	\$2,523.12 DR
Payments and other credits	\$2,523.12 CR
Purchases, cash advances and other debits	\$4,585.00 DR
Interest and other charges	\$0.00
Closing Balance	\$4,585.00 DR

Monthly payment - due by **25 March 2025** \$114.63

Total minimum payment \$114.63

**YOUR DIRECT DEBIT PAYMENT OF \$4,585.00 WILL BE
CHARGED TO ACCOUNT 000086643- 0000975069649 ON
25/03/2025 AS PER OUR AGREEMENT.**

see reverse for transaction details



4336879910625536 / E-7057 S-10665 I-21330

Transaction record for: **Billing account**

Date	Amount A\$	Details	Reference
24 Feb 2025	\$2,523.12 CR	DIRECT DEBIT PAYMENT	74336875052
Total for this Period:	\$2,523.12 CR		



NAB Telephone Banking: transfer funds by phone from your nominated NAB accounts to your NAB Low Rate Business Card account. Phone 13 10 12, between 7am and 9pm AEST, Monday to Friday, 8am and 6pm AEST, Saturday and Sunday



NAB Internet Banking: transfer funds from your NAB cheque or savings account to your NAB Low Rate Business Card account using NAB Internet Banking at nab.com.au



NAB ATM: Transfer funds from your linked NAB accounts to your NAB Credit Card account. You must have a Personal Identification Number (PIN)



Billers Code: 1008. Ref: Select the card number you are making the payment to. Contact your participating bank, credit union or building society to make this payment from your cheque or savings account. BPAY payments may be delayed until the next banking business day, due to processing cut-off times. Maximum BPAY payment amount is AU \$100,000 per payment.

Cardholder summary

If you have recently switched to a new product or had a Lost/Stolen replacement of your card, your cardholder summary may not reconcile with the account balance. The closing balance in “Your Account Summary” section of this statement reflects your correct balance and amount payable. Please login to your Internet Banking or NAB Connect account to review your most up to date transaction listing.

Cardholder account	Cardholder name	Credit limit	Payments and other credits (A)	Purchases and cash advances (B)	Interest and other charges (C)	Net Totals (B + C - A)
4336-8757-3662-2053	MR PAUL GREGORY ANDE	\$15,000	\$0.00	\$199.18	\$0.00	\$199.18
4336-8757-3662-2061	MS NOLA LEANNE COMER	\$5,000	\$0.00	\$2,885.59	\$0.00	\$2,885.59
4336-8757-3662-3853	RICHARD LAWRENCE RYA	\$2,000	\$0.00	\$1,500.23	\$0.00	\$1,500.23
4336-8799-1062-5536	BILLING ACCOUNT	\$0	\$2,523.12 CR	\$0.00	\$0.00	\$2,523.12 CR
			\$2,523.12 CR	\$4,585.00 DR	\$0.00	\$2,061.88 DR

Transaction type

Promotional Offer

Annual percentage rate

0.0000%

Daily percentage rate

0.000000%



Statement for
NAB Low Rate Business Card
NAB Commercial Cards Centre - GPO Box 9992 Melbourne Victoria 3001
Tel 13 10 12 8am - 8pm AEST & AEDT Monday to Friday, 9am - 6pm AEST & AEDT Saturday and Sunday
Fax 1300 363 658
Lost & Stolen Cards: 1800 033 103 (24 hours, 7 days a week)

Cardholder Details

Cardholder Name: MR PAUL GREGORY ANDERSON
Account No: 4336 8757 3662 2053
Statement Period: 30 January 2025 to 28 February 2025
Cardholder Limit: \$15,000

Transaction record for: MR PAUL GREGORY ANDERSON

Date	Amount A\$	Details	Explanation	Amount NOT subject to GST	Amount subject to GST	GST component (1/11th of the amount subject to GST)	Reference
3 Feb 2025	\$90.00	Garmin Eastern Creek					74071915032
21 Feb 2025	\$109.18	PERENJORI HOTEL PERENJORI					74564725051
Total for this period	\$199.18		Totals				

Employee declaration

I verify that the above charges are a true and correct record in accordance with company policy
Cardholder signature: _____ Date: _____

0097402 0594/336879910625536 / E-13835 S-24557 L49113



Statement for
NAB Low Rate Business Card
NAB Commercial Cards Centre - GPO Box 9992 Melbourne Victoria 3001
Tel 13 10 12 8am - 8pm AEST & AEDT Monday to Friday, 9am - 6pm AEST & AEDT Saturday and Sunday
Fax 1300 363 658
Lost & Stolen Cards: 1800 033 103 (24 hours, 7 days a week)

Cardholder Details

Cardholder Name: MS NOLA LEANNE COMERFORD
Account No: 4336 8757 3662 2061
Statement Period: 30 January 2025 to 28 February 2025
Cardholder Limit: \$5,000

Transaction record for: MS NOLA LEANNE COMERFORD

Date	Amount A\$	Details	Explanation	Amount NOT subject to GST	Amount subject to GST	GST component (1/11th of the amount subject to GST)	Reference
30 Jan 2025	\$19.40	SHIRE OF PERENJORI PERENJORI					01142108990
30 Jan 2025	\$409.70	Military Shop 61261232908					01715047309
10 Feb 2025	\$100.00	SMP*Indah Health and B 55Geraldton					74126915037
14 Feb 2025	\$160.00	SQ *DAPHNE'S TIMELESS TREPerenjori					74064145044
17 Feb 2025	\$124.99	PDJ GERALDTON PTY LT GERALDTON					74564455046
18 Feb 2025	\$123.00	BUNNINGS 308000 GERALDTON					74940525047
19 Feb 2025	\$772.00	THE GOOD GUYS WEB STORE SOUTHBANK					74564455049
21 Feb 2025	\$352.30	DEPT OF JUSTICE-CTG PA PERTH					74940525050

Continued next page

0097402 0594/336879910625536 /E-13835 S-24558 I-49115

Transaction record for: MS NOLA LEANNE COMERFORD (continued)

Date	Amount A\$	Details	Explanation	Amount NOT subject to GST	Amount subject to GST	GST component (1/11th of the amount subject to GST)	Reference
24 Feb 2025	\$235.00	DEPT OF JUSTICE-CTG PA PERTH					74940525051
24 Feb 2025	\$180.00	SQ *DAPHNE'S TIMELESS TREPerenjori					74064145052
26 Feb 2025	\$313.20	THE GRANTS HUB COOLUM BEACH					74201335056
27 Feb 2025	\$96.00	SQ *ONE L OF A GOOD FEED Camamah					74064145057
Total for this period	\$2,885.59	Totals					

Employee declaration

I verify that the above charges are a true and correct record in accordance with company policy Cardholder signature: _____ Date: _____



Statement for
NAB Low Rate Business Card
NAB Commercial Cards Centre - GPO Box 9992 Melbourne Victoria 3001
Tel 13 10 12 8am - 8pm AEST & AEDT Monday to Friday, 9am - 6pm AEST & AEDT Saturday and Sunday
Fax 1300 363 658
Lost & Stolen Cards: 1800 033 103 (24 hours, 7 days a week)

Cardholder Details

Cardholder Name: RICHARD LAWRENCE RYAN
Account No: 4336 8757 3662 3853
Statement Period: 30 January 2025 to 28 February 2025
Cardholder Limit: \$2,000

Transaction record for: RICHARD LAWRENCE RYAN

Date	Amount A\$	Details	Explanation	Amount NOT subject to GST	Amount subject to GST	GST component (1/11th of the amount subject to GST)	Reference
30 Jan 2025	\$60.00	SHIRE OF MURCHISON MURCHISON					74564725029
3 Feb 2025	\$11.30	YALGOO GENERAL STORE YALGOO					74940525030
3 Feb 2025	\$498.21	BETTER CHOICE WOORAMEL WOORAMEL					74211985032
3 Feb 2025	\$492.25	BETTER CHOICE WOORAMEL WOORAMEL					74211985032
3 Feb 2025	\$319.39	BETTER CHOICE WOORAMEL WOORAMEL					74211985032
6 Feb 2025	\$66.81	OVERLANDER ROADHOUSE HAMELIN BAY					74940525035
7 Feb 2025	\$8.40	AKADD PTY LTD MINGENEW					74466025037
14 Feb 2025	\$29.87	PERENJORI HOTEL PERENJORI					74564725044
21 Feb 2025	\$14.00	SQ *DAPHNE'S TIMELESS TREPerenjori					74064145051
Total for this period	\$1,500.23		Totals				

Employee declaration

I verify that the above charges are a true and correct record in accordance with company policy Cardholder signature: _____ Date: _____

Shire of Perenjori
CREDIT CARD SUMMARY ONLY
NATIONAL AUSTRALIA BANK

Corporate Mastercard - 30 January 2025 to 28 February 2025 - Paul Anderson - CEO

DATE	DESCRIPTION	ACCOUNT DESCRIPTION	PERSON CONTACTING SELLER	AMOUNT
3/02/2025	Garmin	Monthly GPS with SOS Capability for Graders - Lone worker Safety	MIS	\$ 90.00
21/02/2025	Perenjori Hotel	Council Refreshments	Gov	\$ 109.18
CEO Corporate Credit Card Purchases for 30 January 2025 to 28 February 2025				\$ 199.18

Corporate Mastercard - 30 January 2025 to 28 February 2025 - Nola Comerford - MCCS

DATE	DESCRIPTION	ACCOUNT DESCRIPTION	PERSON CONTACTING SELLER	AMOUNT
30/01/2025	Shire of Perenjori	DOT - swap plates between vehicles - PJ1578 to 1ILY262	CSO	\$ 19.40
30/01/2025	Military Shop	Artificial Wreath x 2, Poppy pins x 100 - Anzac Day 2025	CDO	\$ 409.70
10/02/2025	Indah Health & Beauty	Gift Voucher for Stacey Spencer - Maternity Leave	CSO	\$ 100.00
14/02/2025	Daphne's Timeless Treats	Afternoon tea on 10 & 12/02/25 - Meeting & Karara Mining	CSO	\$ 160.00
17/02/2025	PDJ Geraldton Pty Ltd	Con's Liquor - Council refreshments	MCCS	\$ 124.99
18/02/2025	Bunnings Geraldton	Floor rug - Chalet 1 C/Park	MCCS	\$ 123.00
19/02/2025	The Good Guys	Chest Freezer - Perenjori Bush Fire Brigade	MCCS	\$ 772.00
21/02/2025	Department of Justice	Application for Means Inquirt Summons - J Duthie (to be refunded)	MCCS	\$ 352.30
24/02/2025	Department of Justice	Application for Means Inquirt Summons - J Duthie	MCCS	\$ 235.00
24/02/2025	Daphne's Timeless Treats	Council Meeting lunch	MCCS	\$ 180.00
26/02/2025	The Grants Hub	Annual Subscription - Grants Sourcing Tool	MCCS	\$ 313.20
27/02/2025	One L of a Good Feed	Morning tea for Seniors - LIFE Program Grant	CDO	\$ 96.00
MCCS Corporate Credit Card Purchases for 30 January 2025 to 28 February 2025				\$ 2,885.59

Corporate Mastercard - 30 January 2025 to 28 February 2025 - Richard Ryan - CESM

DATE	DESCRIPTION	ACCOUNT DESCRIPTION	PERSON CONTACTING SELLER	AMOUNT
30/01/2025	Shire of Murchison	Lunch - fuel run, SKAO, CSIRO visits - Charge to DFES	CESM	\$ 60.00
3/02/2025	Yalgoo General Store	Lunch - fuel run & visit	CESM	\$ 11.30
3/02/2025	Better Choice Wooramel	Fuel for Fire Bug - 3 x Drums ULP - charge to DFES Incident No 708450	CESM	\$ 498.21
3/02/2025	Better Choice Wooramel	Fuel for Fire Bug - 3 x Drums ULP - charge to DFES Incident No 708450	CESM	\$ 492.25
3/02/2025	Better Choice Wooramel	Fuel for Fire Bug - 3 x Drums ULP - charge to DFES Incident No 708450	CESM	\$ 319.39
6/02/2025	Overlander Roadhouse	Breakfast - Charge to DFES Incident No 708450	CESM	\$ 66.81
7/02/2025	Mingenew Bakery	Breakfast	CESM	\$ 8.40
14/02/2025	Perenjori Hotel	Dinner	CESM	\$ 29.87
21/02/2025	Daphne's Timeless Treats	Lunch	CESM	\$ 14.00
CESM Corporate Credit Card Purchases for 30 January 2025 to 28 February 2025				\$ 1,500.23
Total Payments of Corporate Credit Card				\$ 4,585.00

Interest & Other Charges	\$ -
Grand Total	\$ 4,585.00

14. Community Development and Services:

14.1 FUTURE OF LATHAM HALL

Applicant:	Shire of Perenjori
File:	A766
Date:	20 March 2025
Disclosure of Interest:	
Voting Requirements:	Simple Majority
Author:	Nola Comerford – Manager Corporate and Community Services
Responsible Officer:	Paul Anderson – Chief Executive Officer
Attachments:	14.1.1 – Latham Hall Community Meeting Notes

Summary

This report seeks Council's guidance as to the future of the Latham Hall.

Background

The Latham Hall is located upon the 3,356m² Reserve 24146 on the corner of Britt Street and Richards Street in the Latham townsite.

Reserve 24146 has a management order issued to it by the Department of Planning, Lands & Heritage (DPLH) for the purpose of 'Hall Site' that was issued on 28 October 1955.

At the July 2024 Ordinary Council Meeting, Council requested a community survey be conducted and a public meeting held to gauge public opinion on the future of the Latham Hall.

Council Resolution Number: 250724.22

Moved: Cr Bradford Seconded: Cr Fraser

That Council request the Administration to progress option four – Conduct a Community Survey followed by a Community Meeting held in Latham to discuss the future of Latham Hall.

Motion put and carried 7/0

For: Cr Sutherland, Cr Hepworth, Cr Bradford, Cr Bryant, Cr Campbell, Cr Sparkman, Cr Fraser

Against: Nil

A Latham Hall Community Survey was subsequently distributed within the Shire of Perenjori via social media, website and print. The responses in the survey varied although the majority of opinions indicated that demolition of the Latham Hall was the preferred option. There were also suggestions of retaining the front façade brick work to serve as a memorial of the hall.

At the October 2024 Ordinary Council Meeting, Council was presented with results from the community survey and a date set for a public meeting to discuss the Latham Hall.

Council Resolution Number: 241024.14

Moved: Cr Bryant Seconded: Cr Sparkman

That Council:

1. Accepts the results from the Latham Hall Community Survey which was conducted between 12 August and 13 September 2024.
2. Requests a Community Meeting to be scheduled for February 2025 date to be confirmed in Latham for the purpose of open discussion regarding the future of Latham Hall.

Motion put and carried by Absolute Majority 5/0

For: Cr Hepworth, Cr Bryant, Cr Bradford, Cr Sparkman, Cr Fraser

Against: Nil

In February 2025, a public meeting was held to discuss the future of the Latham Hall ([attachment 14.1.1](#))

Figure 1 – Location Map for Latham Hall (Reserve 24146)



Figure 2 – Aerial photo of Reserve 24146 corner Britt Street and Richards Street



Figure 3 – Latham Hall



Statutory Environment

Local Government Act 1995

Policy Implications

Nil

Consultation

Elected Members – Shire of Perenjori

Paul Anderson – Chief Executive Officer

Bianca Plug – Governance and Executive Support Officer

Adim Hajat – Community Development Officer

Members of the public

Financial Implications

As outlined in the quantity surveyors report, the cost to repair the building is significant and budget allocations and grants would need to be sourced to undertake these works.

A cost to demolish the building has not been established at this point however given demolition costs on other buildings and the degree of useable material i.e. wooden floorboards, there could be a cost in the vicinity of \$50,000.

Strategic Community Plan

Goal 2: Eco-friendly, attractive and well-maintained towns, surrounded by outstanding natural beauty, landscapes, flora and fauna to be protected and enjoyed.

2.2. Land use and building regulations are designed and administered to meet the current and future needs of the community.

2.3. The Shire's buildings and leases are administered to an appropriate standard for the benefit of the community according to their need and use.

Goal 4: A strong and diverse Council working closely with the proactive and involved community.

4.2. The Shire listens to and works closely with the community and its decision-making is transparent and accountable.

Officer Comment

A building structural integrity report was commissioned and undertaken on the Latham Hall in late September 2023 and an indicative cost estimate to undertake the repairs as indicated by the structural integrity report commissioned in January 2024.

The estimated cost of repairs at that time was \$554,000 to rectify all the defects listed in the structural report. There may be some cost savings with a reduced scope of works savings. The structural report was to bring the building back to meet current Australian design standards for a public building.

Following receipt of the structural integrity, further damage has occurred with sections of the internal ceiling and walls falling down making the hall unfit for use.

A request was made by attendees at the public meeting for Council to defer making any decisions on the future of the Latham Hall until community member investigations into potential volunteer renovation work could be obtained.

OFFICER RECOMMENDATION

Council Resolution Number:

Moved:

Seconded:

That Council:

- 1. Note the Latham Hall Public Meeting notes.**
- 2. Request a report be presented to Council at the July 2025 Ordinary Council Meeting regarding findings from community members on Latham Hall repairs.**

Motion put and carried / lost

For:

Against:

[Next Item](#)



Shire of
Perenjori
Embrace Opportunity

Shire of Perenjori

Latham Hall Meeting Notes
Thursday, 13 February 2025

Present

Councillors:

- Cr Jude Sutherland – Shire President
- Cr Les Hepworth – Deputy President
- Cr Daniel Bradford
- Cr Colin Bryant
- Cr Brian Campbell
- Cr Andrew Fraser

Staff:

- Paul Anderson – CEO
- Nola Comerford – MCCC
- Marty Noordhof - MIS
- Bianca Plug – Governance Officer
- Adim Hajat – CDO

Members of the Public:

Brian Baxter	Jacqui Benton
Annette Blaxell	Ros Campbell
Steve Campbell	Terina Campbell
Kerry Foote	Lee Hudson
Elaine King	Paddy King
Carol Lewinski	Eddie Lewinski
Maureen McManus	Merideth Morton
Sue Pendergrast	Peter Wolski

The Shire President opened the meeting at 5.00 pm and asked for comments regarding the future of the Latham Hall.

Kerry Foote: Thanked Council for calling the meeting. Talked about Track Care WA, a non-profit organisation with 200 volunteers that assists local governments in repairing and restoring buildings. Kerry thanked Paul for organising the Hall to be opened for the Track Care builder to inspect the property and take photos which will be presented at their next meeting at the end of February. Kerry asked if Council could please wait until after the meeting before making a decision.

Kerry read aloud a letter from Marion Harrington, a former Latham resident regarding the hall. Marion spoke of the hall's history and significance to Latham residents.

Kerry noted that in 2023 she had spoken to the CEO, Paul Anderson who advised that there was \$75,000 allocated for maintenance of the Latham Hall in the Annual Budget. Recently, Paul advised Kerry that the \$75,000 had been re-allocated throughout the Budget.

Annette Blaxell: Referred to the surveyor's report which stated that the condition of the hall was due to lack of maintenance. Annette suggested that there are several ways in which funds could be raised to repair the hall – e.g. a Go Fund Me page on Facebook or grants.

Cr Hepworth: Asked the Latham residents how they envisaged usage of the hall, when the hall was last used, and why it was not often used.

Some Latham residents responded to Cr Hepworth that the Latham Hall hasn't been used because it was unsafe. In the past two decades it has been used for birthday parties, a wedding, wakes etc.

Cr Campbell: Asked what is the timeframe for Track Care?

Kerry Foote: Track Care have said they would come back once the weather is cooler.

Cr Sutherland: Asked whether the hall was within Track Cares scope of works.

Kerry Foote: Yes. There are many tradies who volunteer with Track Care.

Paul Anderson: Advised that building standards need to be met when renovating Council property.

Sue Pendergrast: Enquired on how long it had been since the Shire had undertaken maintenance on the hall, and why the Shire hasn't maintained it. Advised that when a door was kicked in, the replacement was the wrong size. Meanwhile, the Perenjori Hall is maintained.

Cr Sutherland: Advised that the Perenjori Hall does get regularly used so it is hard to compare. Suggested that the meeting focus on Latham Hall.

Kerry Foote: Noted that the Latham community has connections to the hall and needs it to be maintained in a usable state.

Sue Pendergrast: The hall was built by the Latham people, for the Latham people.

Maureen McManus: A working hall adds to the community. It is insulting that people want to pull it down, to the people who put in their hard work and money. Shire workers didn't do maintenance to the property. The history is part of Latham.

Paul Anderson: It is difficult for the current Council to comment. Maintenance has not been done for many years. The Perenjori Hall had an asbestos roof which needed to be replaced. The Latham Community Centre is in good condition. There are limited funds

available to complete all the maintenance required on the Latham Hall. Any suggestions to Council are welcome.

Kerry Foote: Someone took part of the asbestos roof near the entry away and never replaced it.

Paul Anderson: Advised that he had organised the removal of asbestos for safety reasons. It had not been replaced due to uncertainty of the future of Latham Hall.

Annette Blaxell: Suggested that 'being used' isn't important. The history is important. People will come back to Latham for the hall, funerals will be held there.

Cr Bradford: Enquired how often the Community Centre was used.

A few comments were made about the Community Centre only being used once a year for a Council Meeting.

Cr Campbell: Asked if Track Care and grants were unsuccessful, would people think it was worth spending \$600k to get the hall up to scratch.

Steve Campbell: No, we wouldn't expect the Shire to cover it all.

Elaine King: Enquired if events that are held in the Perenjori Hall, could be rechanneled to Latham so that the Latham Hall can be used once it is brought up to standard?

Paul Anderson: It would be difficult to transfer static events. Other events could be considered if the hall was renovated.

Meredith Morton: The Shire could help with utilising the hall.

Cr Sutherland: Asked if anyone was interested in private purchase of the hall.

Meredith Morton: It would have to be advertised to see if there is a wider interest from people who live outside of Latham.

Cr Sutherland: Specific procedures need to be followed to advertise the hall for sale.

Kerry Foote: Could the Shire hold off on advertising the hall for sale to give people a chance to determine how to renovate. Would there be another informal meeting?

Paul Anderson: Advised that findings could be presented to Council at a future Council Meeting.

Colin Bryant: Council would want to know direction prior to allocating budget.

Sue Pendergrast: Latham doesn't get a mention in the Budget.

Cr Sutherland: Advised that Council are interested in all opinions and information. There is no rush.

Sue Pendergrast: 98% of people here want the hall to be fixed.

Jacqui Benton: Mentioned she is part of the 2% who doesn't want to see so much money spent on a building that doesn't get used. Suggested that if Council were to invest budget allocation into Latham, it should be utilised to plant trees, build gazebos, and on functional buildings such as the Community Centre and Sports Club.

Maureen McManus: History is important. Losing the hall would be disrespectful to the Latham people who funded it.

Cr Sutherland: No decision has been made. As a local government, any projects over \$250,000 need to go through a tender process. Certain companies have limited time and resources to undertake this type of project.

Maureen McManus: Questioned the cost of estimated repairs as they seem high.

Paul Anderson: Advised that in January 2024, a Quantity Surveyor provided a quote of \$554,000 for repairs.

Maureen McManus: There is not a lot of repairs to do to the hall.

Paul Anderson: As per the Quantity Surveyor's report, there is actually an extensive amount of work to do on the hall.

Cr Sutherland: Advised that, as a local government, the Shire is governed and regulated by many Acts and Regulations, particularly with regard to its buildings and finances.

Paul Anderson: We are aware that the hall has lacked maintenance over many years, well before the current Council were involved. That is why we are having a meeting now, to determine a way forward.

Cr Sutherland: Reiterated that the Latham Hall Survey provided four options:

1. Upgrade
2. Relinquish
3. Demolish
4. Sell

Cr Campbell: Asked if the Latham Progress Association would be interested in being gifted the hall.

Meredith Morton: No.

Terina Campbell: After all options have been exhausted, and if there is nothing else that can be done, it would be nice to keep something for our history, a part of the building where we can display books, pictures of the hall and where people can still have something to visit.

Cr Sutherland: Would the Latham community be interested in demolishing the hall but retaining the front façade.

Terina Campbell: As a last resort.

Roz Campbell: If the hall was demolished and the front façade kept, a marquee could be installed behind the façade when holding an event.

Cr Hepworth: Advised that Council are required to make hard decisions. Cr Hepworth recently spoke with other Latham residents who suggested bringing in loaders to demolish the building. Commented on the divided opinions of the local community.

Kerry Foote: People will come forward with ideas.

Cr Sutherland: Suggested a timeframe be considered for information to be presented to Council.

Paul Anderson: Advised that there are limited options for sourcing external funding for maintenance. A funding application from commencement to outcome can be 6-12 months.

Cr Bryant: Asked Kerry Foote the timeframe for options to be presented to Council. Suggested that it is hard to justify committing a lot of funds for the hall upgrade. There need to be guidelines in place.

Cr Sutherland: Proposed that the end of June 2025 (4 months) be the deadline for options to be presented to Council.

Cr Sutherland: Thanked everyone for their attendance and closed the meeting at 6.15 pm.

14.2 PERENJORI AGRICULTURAL SHOW 2025

Applicant:	Perenjori Agricultural Society Inc.
File:	ADM 0408
Date:	20 March 2025
Disclosure of Interest:	
Voting Requirements:	Absolute Majority
Author:	Nola Comerford – Manager Corporate Community Services
Responsible Officer:	Paul Anderson – Chief Executive Officer
Attachments:	14.2.1 - Request for financial and in-kind support

Summary

For Council to consider sponsorship of the 83rd Perenjori Agricultural Show, to be held Saturday 16 August 2025, in the form of financial and in-kind assistance.

Background

The Perenjori Agricultural Society is in the process of organising the 83rd Perenjori Agricultural Show. The show is a popular district event which has previously drawn record crowds and were considered a great success.

Correspondence was received from the Agricultural Society President, Dakota Curtin, requesting support as follows:

- 'Gold Donor Partnership' of \$10,000 (ten thousand dollars);
- Waived fees for the Perenjori Pavilion, Oval, and associated facilities;
- Waived accommodation fees at the Perenjori Caravan Park for approximately fourteen entertainers and medics;
- Allowing use of the vacant block on Crossing Road as an overflow camping area, once the Caravan Park is fully booked;
- Providing use of various Council resources and in-kind assistance to set up and pack up.

Statutory Environment

Nil

Policy Implications

Council Policy No. 1011 – Donations and Grants

Consultation

Paul Anderson – Chief Executive Officer

Ally Bryant – Finance Manager

Marty Noordhof – Manager Infrastructure Services

Dakota Curtin – President, Perenjori Agricultural Society

Financial Implications

An allocation of \$10,000 was included in the 2024/25 Annual Budget for the Agricultural Show.

An estimation of Council contribution based on previous Agricultural Shows and current requests is provided in the table below:

ITEM	COUNCIL CONTRIBUTION	CASH/IN-KIND
Gold Sponsorship	\$10,000	Cash
Pavilion Hire	\$540	In-kind
Staff	\$9,887	In-kind
Equipment Hire	\$1,386	In-kind
Accommodation	\$3,108	In-kind

Cleaning (accommodation)	\$1,960	In-kind
TOTAL	\$26,881	

Strategic Community Plan

Goal 1: An inclusive community and a great place to live for all ages and stages of life.

1.2. Community life is enhanced and nurtured with well supported clubs, community groups, and essential volunteer-based services.

Officer Comment

The Perenjori Agricultural Show is a major event for the region, organised by volunteers from the Perenjori Agricultural Society. The Show attracts visitors from the region, bringing economic advantage to local retailers. Financial assistance from the Shire in previous years has included 'Gold Donor' sponsorship of \$10,000.

As per Council decision 201022.5, hire fees of community facilities are waived for Shire of Perenjori not-for-profit organisations (excluding bonds).

OFFICER RECOMMENDATION

Council Resolution Number:

Moved:

Seconded:

That Council:

1. Endorse the 'Gold Donor Partnership' donation of \$10,000 (ten thousand dollars) to the Perenjori Agricultural Society for their 83rd Agricultural Show to be held on Saturday, 16 August 2025 and;
2. Waive charges for up to fourteen (14) Accommodation Village single units for entertainers for the 15 and 16 August 2025 and;
3. Waive reimbursable Bond charge of \$269 for Pavilion use and;
4. Offer in-kind support of Shire Staff members to assist with setting up during the week commencing Monday, 11 August 2025 and packing up the week following the event and;
5. Endorse a free overflow camping area to be situated on the vacant lot on corner of Hirshauer Rd and Crossing St.

Motion put and carried / lost by Absolute Majority

For:

Against:

[Next Item](#)



Perenjori Agricultural Society Inc.

A: PO Box 46, Perenjori WA 6620
E: pjag@hotmail.com.au
ABN: 31 163 849 680
IARN: A100 54 84R

4th March 2025

Paul Anderson - Chief Executive Officer
Shire of Perenjori
56 Fowler Street, Perenjori
E: ceo@perenjori.wa.gov.au

Dear Paul & Councillors,

The Perenjori Agricultural Show is an event that takes pride of place on many local community members' calendars. It is the only country show of its scale in our district and people from within the Mid-West region and beyond, come to enjoy the day.

This year we will be holding our 83rd show on Saturday the 16th August. We, the Agricultural Society volunteers, are only able to run this community event because of the contributions made by our generous donors.

In the past, The Shire of Perenjori has contributed significantly towards the running of the show, and we would like to ask you to consider supporting our event again in 2025.

As with previous years, The Shire of Perenjori has kindly assisted our show through:

- Contributing towards the running costs of the show in return for 'Gold Donor Partnership' which will acknowledge the Shire of Perenjori in all our promotional materials & advertising formats, for which we would like to ask for \$10,000.
- Allowing us the use, without charge nor bond of the Stan Cannon Sports Complex and associated facilities.
- Allowing the use of vacant block on Crossing Road, on the far west side of the park as an overflow camping area, once the caravan park is fully booked.
- Providing free accommodation at the caravan park for approximately 14 of our entertainers & medics.
- Allowing the use and providing delivery of tables, chairs, benches, PA System, roadside signage and a few other items that the Shire owns, which can then be utilised for the event. Please see following request list.

We thank you for your ongoing support, and with your partnership, look forward to hosting another very successful show in 2025.

Best regards,

Dakota Curtin
President - Perenjori Agricultural Society Inc.
M: 0484 255 616

Page 1 of 2



Shire of
Perenjori
Embrace Opportunity



Extension Hill Pty Ltd



Mount Gibson Iron



silverlake
RESOURCES



agwest
machinery

carbonneutral
Turn Emissions into Trees



INDEPENDENT
FARMERS FIRST
Rural

KARARA
MINING LTD



PERENJORI
SEED CLEANERS





Perenjori Agricultural Society Inc.

A: PO Box 46, Perenjori WA 6620

E: pjag@hotmail.com.au

ABN: 31 163 849 680

IARN: A100 54 84R

1. Borrow a set of master keys for the pavilion & access to the pool (for power) – we require access from approx. Wednesday before the show to the following Wednesday. We will ensure their safe return, as always.
2. Borrow the 65kva genset from the pool. Relocated for use by our food vans.
3. Borrow the two PA Systems.
4. Borrow the Pavilion TV Remote
5. Event ahead signs, Reduce Speed, 40kms/hr (or lower if have) – Can these be put out by a Shire representative around 9am on Saturday morning at each end of town?
6. Any movable tables, chairs and benches are required for show day.
7. As many clean wheelie bins as available
8. 2 x vacuum cleaners made available for Pavilion cleaning.
9. Sanitary waste bins in all ladies toilets please.
10. Toilets/Septics (Pavilion & Netball) to be pumped out/checked prior to the show.
11. Provide extra toilet roll, hand towels & soap so that we can replace when required also.
12. Bin liner replacements for the Pavilion bins
13. Oval & general area to be mown Monday before the show.
14. Reticulation lines to be marked out on oval and around pavilion area on Tuesday before the show.
15. Oval sprinklers to be turned off from Tues before to Tues after the show please.
16. Can the hot-mixed carpark area at the front of the gymnasium/pool entry be swept clear of rocks etc to make a tidy entrance.
17. Can the bougainvillea please be well trimmed alongside the wall to the toilets?
18. Toilet & Showers location/identification signage

Page 2 of 2



Shire of
Perenjori
Embrace Opportunity



Extension Hill Pty Ltd



Mount Gibson Iron



agwest
machinery



INDEPENDENT
FARMERS FIRST
Rural

KARARA
MINING LTD



MERKANOOKA
HAULAGE



15. Governance:

15.1 ADOPTION OF COMPLIANCE AUDIT RETURN 2024

Applicant:	Shire of Perenjori
File:	ADM 0540
Date:	20 March 2025
Disclosure of Interest:	
Voting Requirements:	Absolute Majority
Author:	Bianca Plug – Governance Officer
Responsible Officer:	Paul Anderson – Chief Executive Officer
Attachments:	15.1.1 – Compliance Audit Return 2024

Summary

This report presents the 2024 Compliance Audit Return (CAR) for review and adoption, in preparation of submission to the Department of Local Government.

Background

In accordance with section 7.13(1) of the *Local Government Act 1995* and regulation 14 of the *Local Government (Audit) Regulations 1996*, the Shire is required to carry out a compliance audit for the period 1 January 2024 to 31 December 2024 and prepare a Compliance Audit Return (CAR) in a form approved by the Minister.

The 2024 CAR is provided to the Shire by the Department of Local Government, Sport and Cultural Industries (the Department) and the areas of compliance are restricted to those considered high risk.

The CAR is required to be reviewed by the Audit Committee then presented to Council for adoption. The CAR must be adopted, signed and provided to the Department by the 31 March of each year.

Statutory Environment

Section 7.13(1) of the *Local Government Act 1995*

Local Government (Audit) Regulations 1996 r. 14

Financial Implications

Nil

Strategic Community Plan

Goal 4: A strong and diverse Council working closely with the proactive and involved community

4.6. The organisation, assets and finances of the Shire are managed responsibly.

Consultation

Paul Anderson – Chief Executive Officer

Ally Bryant – Finance Manager

Nexia Australia

Officer Comment

Following the internal review of the 2024 Compliance Audit Return, there were no non-compliance areas identified.

AUDIT COMMITTEE RECOMMENDATION

Council Resolution Number:

Moved:

Seconded:

The Audit Committee recommends that Council:

- 1. Adopts the Compliance Audit Return for the period 1 January 2024 to 31 December 2024, as attached.**
- 2. Authorise the President and Chief Executive Officer to sign and submit the Compliance Audit Return to the Department of Local Government, Sport and Cultural Industries by 31 March 2025.**

Motion put and carried / lost by Absolute Majority

For:

Against:

[Next Item](#)



COMPLIANCE AUDIT RETURN 2024

Commercial Enterprises by Local Governments				
No	Reference	Question	Response	Comments
1	s3.59(2)(a) F&G Regs 7,9,10	Has the local government prepared a business plan for each major trading undertaking that was not exempt in 2024?	N/A	
2	s3.59(2)(b) F&G Regs 7,8A, 8, 10	Has the local government prepared a business plan for each major land transaction that was not exempt in 2024?	N/A	
3	s3.59(2)(c) F&G Regs 7,8A, 8,10	Has the local government prepared a business plan before entering into each land transaction that was preparatory to entry into a major land transaction in 2024?	N/A	
4	s3.59(4)	Has the local government complied with public notice and publishing requirements for each proposal to commence a major trading undertaking or enter into a major land transaction or a land transaction that is preparatory to a major land transaction for 2024?	Yes	Notice of Proposal to Lease Property for a Supermarket advertised in the West Australian, website, Bush Telegraph, notice boards and social media on 9 November 2024.
5	s3.59(5)	During 2024, did the council resolve to proceed with each major land transaction or trading undertaking by absolute majority?	Yes	Lease and Operation of a Supermarket endorsed by Council 19 December 2024. Council Decision – 191224.17.

Delegation of Power/Duty				
No	Reference	Question	Response	Comments
1	s5.16 (1)	Were all delegations to committees resolved by absolute majority?	N/A	No delegations to Committees.
2	s5.16 (2)	Were all delegations to committees in writing?	N/A	No delegations to Committees.
3	s5.17	Were all delegations to committees within the limits specified in section 5.17 of the <i>Local Government Act 1995</i> ?	N/A	No delegations to Committees.
4	s5.18	Were all delegations to committees recorded in a register of delegations?	N/A	No delegations to Committees.
5	s5.18	Has council reviewed delegations to its committees in the 2023/2024 financial year?	N/A	No delegations to Committees.
6	s5.42(1) & s5.43 Admin Reg 18G	Did the powers and duties delegated to the CEO exclude those listed in section 5.43 of the <i>Local Government Act 1995</i> ?	Yes	
7	s5.42(1)	Were all delegations to the CEO resolved by an absolute majority?	Yes	



Department of
**Local Government, Sport
and Cultural Industries**

8	s5.42(2)	Were all delegations to the CEO in writing?	Yes	
9	s5.44(2)	Were all delegations by the CEO to any employee in writing?	Yes	Delegations to employees were recorded on an Authorised Persons Register and individual personnel files.
10	s5.16(3)(b) & s5.45(1)(b)	Were all decisions by the Council to amend or revoke a delegation made by absolute majority?	N/A	No delegations were amended or revoked in 2024.
11	s5.46(1)	Has the CEO kept a register of all delegations made under Division 4 of the Act to the CEO and to employees?	Yes	Register of Delegations adopted at the Ordinary Council Meeting held 21 November 2024. Council Decision 211124.9.
12	s5.46(2)	Were all delegations made under Division 4 of the Act reviewed by the delegator at least once during the 2023/2024 financial year?	Yes	As above.
13	s5.46(3) Admin Reg 19	Did all persons exercising a delegated power or duty under the Act keep, on all occasions, a written record in accordance with <i>Local Government (Administration) Regulations 1996</i> , regulation 19?	Yes	

Disclosure of Interest

No	Reference	Question	Response	Comments
1	s5.67	Where a council member disclosed an interest in a matter and did not have participation approval under sections 5.68 or 5.69 of the <i>Local Government Act 1995</i> , did the council member ensure that they did not remain present to participate in discussion or decision making relating to the matter?	Yes	
2	s5.68(2) & s5.69(5) Admin Reg 21A	Were all decisions regarding participation approval, including the extent of participation allowed and, where relevant, the information required by the <i>Local Government (Administration) Regulations 1996</i> regulation 21A, recorded in the minutes of the relevant council or committee meeting?	Yes	
3	s5.73	Were disclosures under sections 5.65, 5.70 or 5.71A(3) of the <i>Local Government Act 1995</i> recorded in the minutes of the meeting at which the disclosures were made?	Yes	
4	s5.75 Admin Reg 22, Form 2	Was a primary return in the prescribed form lodged by all relevant persons within three months of their start day?	Yes	
5	s5.76 Admin Reg 23, Form 3	Was an annual return in the prescribed form lodged by all relevant persons by 31 August 2024?	Yes	



Department of
**Local Government, Sport
and Cultural Industries**

6	s5.77	On receipt of a primary or annual return, did the CEO, or the Mayor/President, give written acknowledgment of having received the return?	Yes	
7	s5.88(1) & (2)(a)	Did the CEO keep a register of financial interests which contained the returns lodged under sections 5.75 and 5.76 of the <i>Local Government Act 1995</i> ?	Yes	https://www.perenjori.wa.gov.au/annual-and-primary-returns.aspx
8	s5.88(1) & (2)(b) Admin Reg 28	Did the CEO keep a register of financial interests which contained a record of disclosures made under sections 5.65, 5.70, 5.71 and 5.71A of the <i>Local Government Act 1995</i> , in the form prescribed in the Local Government (Administration) Regulations 1996, regulation 28?	Yes	https://www.perenjori.wa.gov.au/registers/declarations
9	s5.88(3)	When a person ceased to be a person required to lodge a return under sections 5.75 and 5.76 of the <i>Local Government Act 1995</i> , did the CEO remove from the register all returns relating to that person?	Yes	
10	s5.88(4)	Have all returns removed from the register in accordance with section 5.88(3) of the <i>Local Government Act 1995</i> been kept for a period of at least five years after the person who lodged the return(s) ceased to be a person required to lodge a return?	Yes	
11	s5.89A(1), (2) & (3) Admin Reg 28A	Did the CEO keep a register of gifts which contained a record of disclosures made under sections 5.87A and 5.87B of the <i>Local Government Act 1995</i> , in the form prescribed in the <i>Local Government (Administration) Regulations 1996</i> , regulation 28A?	Yes	
12	s5.89A(5) & (5A)	Did the CEO publish an up-to-date version of the gift register on the local government's website?	Yes	https://www.perenjori.wa.gov.au/registers/gifts
13	s5.89A(6)	When people cease to be a person who is required to make a disclosure under section 5.87A or 5.87B of the <i>Local Government Act 1995</i> , did the CEO remove from the register all records relating to those people?	Yes	
14	s5.89A(7)	Have copies of all records removed from the register under section 5.89A(6) <i>Local Government Act 1995</i> been kept for a period of at least five years after the person ceases to be a person required to make a disclosure?	Yes	
15	s5.70(2) & (3)	Where an employee had an interest in any matter in respect of which the employee provided advice or a report directly to council or a committee, did that person disclose the nature and extent of that interest when giving the advice or report?	Yes	



Department of
**Local Government, Sport
and Cultural Industries**

16	s5.71A & s5.71B(5)	Where council applied to the Minister to allow the CEO to provide advice or a report to which a disclosure under section 5.71A(1) of the <i>Local Government Act 1995</i> relates, did the application include details of the nature of the interest disclosed and any other information required by the Minister for the purposes of the application?	N/A	
17	s5.71B(6) & s5.71B(7)	Was any decision made by the Minister under section 5.71B(6) of the <i>Local Government Act 1995</i> , recorded in the minutes of the council meeting at which the decision was considered?	N/A	
18	s5.104(1)	Did the local government prepare and adopt, by absolute majority, a code of conduct to be observed by council members, committee members and candidates that incorporates the model code of conduct?	Yes	Adopted at the Ordinary Council Meeting held 22 September 2022. Council Decision 210922.19.
19	s5.104(3) & (4)	Did the local government adopt additional requirements in addition to the model code of conduct? If yes, does it comply with section 5.104(3) and (4) of the <i>Local Government Act 1995</i> ?	No	
20	s5.104(7)	Has the CEO published an up-to-date version of the code of conduct for council members, committee members and candidates on the local government's website?	Yes	https://www.perenjori.wa.gov.au/documents/95/code-of-conduct-for-council-members-committee-members-and-candidates
21	s5.51A(1) & (3)	Has the CEO prepared and implemented a code of conduct to be observed by employee of the local government? If yes, has the CEO published an up-to-date version of the code of conduct for employees on the local government's website?	Yes	https://www.perenjori.wa.gov.au/documents/94/employee-code-of-conduct



Disposal of Property				
No	Reference	Question	Response	Comments
1	s3.58(3)	Where the local government disposed of property other than by public auction or tender, did it dispose of the property in accordance with section 3.58(3) of the <i>Local Government Act 1995</i> (unless section 3.58(5) applies)?	N/A	There was no disposal of property in 2024 other than by public auction or tender.
2	s3.58(4)	Where the local government disposed of property under section 3.58(3) of the <i>Local Government Act 1995</i> , did it provide details, as prescribed by section 3.58(4) of the Act, in the required local public notice for each disposal of property?	N/A	As above.

Elections				
No	Reference	Question	Response	Comments
1	Elect Regs 30G(1) & (2)	Did the CEO establish and maintain an electoral gift register and ensure that all disclosure of gifts forms completed by candidates and donors and received by the CEO were placed on the electoral gift register at the time of receipt by the CEO and in a manner that clearly identifies and distinguishes the forms relating to each candidate in accordance with regulations 30G(1) and 30G(2) of the <i>Local Government (Elections) Regulations 1997</i> ?	N/A	No elections were held during 2024.
2	Elect Regs 30G(3) & (4)	Did the CEO remove any disclosure of gifts forms relating to an unsuccessful candidate, or a successful candidate that completed their term of office, from the electoral gift register, and retain those forms separately for a period of at least two years in accordance with regulation 30G(4) of the <i>Local Government (Elections) Regulations 1997</i> ?	N/A	No elections were held during 2024.
3	Elect Regs 30G(5) & (6)	Did the CEO publish an up-to-date version of the electoral gift register on the local government's official website in accordance with regulation 30G(5) of the <i>Local Government (Elections) Regulations 1997</i> ?	N/A	No elections were held during 2024.



Finance				
No	Reference	Question	Response	Comments
1	s7.1A	Has the local government established an audit committee and appointed members by absolute majority in accordance with section 7.1A of the <i>Local Government Act 1995</i> ?	Yes	Appointed at the Special Meeting of Council held 26 October 2023. Council Decision 261023.1.
2	s7.1B	Where the council delegated to its audit committee any powers or duties under Part 7 of the <i>Local Government Act 1995</i> , did it do so by absolute majority?	N/A	No duties or powers were delegated.
3	s7.9(1)	Was the auditor's report for the financial year ended 30 June 2024 received by the local government by 31 December 2024?	Yes	Received 5 December 2024.
4	s7.12A(3)	Where the local government determined that matters raised in the auditor's report prepared under section 7.9(1) of the <i>Local Government Act 1995</i> required action to be taken, did the local government ensure that appropriate action was undertaken in respect of those matters?	Yes	The Auditor General's Report identified 4 findings. These findings were presented to Council at the Audit Committee Meeting and Ordinary Council Meetings held 19 December 2024. The manner in which the administration plans to address these issues were detailed in the Auditor's Management Letter.
5	s7.12A(4)(a) & (4)(b)	Where matters identified as significant were reported in the auditor's report, did the local government prepare a report that stated what action the local government had taken or intended to take with respect to each of those matters? Was a copy of the report given to the Minister within three months of the audit report being received by the local government?	N/A	No significant findings were identified.
6	s7.12A(5)	Within 14 days after the local government gave a report to the Minister under section 7.12A(4)(b) of the <i>Local Government Act 1995</i> , did the CEO publish a copy of the report on the local government's official website?	N/A	As above.
7	Audit Reg 10(1)	Was the auditor's report for the financial year ending 30 June 2024 received by the local government within 30 days of completion of the audit?	Yes	Received 5 December 2024.



Integrated Planning and Reporting				
No	Reference	Question	Response	Comments
1	Admin Reg 19C	Has the local government adopted by absolute majority a strategic community plan? If yes, please provide the adoption date or the date of the most recent review in the Comments section?	Yes	<p>The Strategic Community Plan and Corporate Business Plan (the Plan) 2022/23 – 2032/33 was adopted at the Ordinary Council Meeting held on 18 August 2022. Council Decision 180822.7.</p> <p>The 'Council Plan' was reviewed throughout 2024:</p> <ul style="list-style-type: none"> - Quarterly Report January – March 2024 (OCM held 18 April 2024. Council Decision 180424.7) - Annual Report June 2023 – June 2024 (OCM held 25 July 2024. Council Decision 250724.8) - Quarterly Report July – September 2024 (OCM held 24 October 2024. Council Decision 241024.11) - Quarterly Report October – December 2024 (OCM held 19 December 2024. Council Decision 191224.9).
2	Admin Reg 19DA(1) & (4)	Has the local government adopted by absolute majority a corporate business plan? If yes, please provide the adoption date or the date of the most recent review in the Comments section?	Yes	As above.
3	Admin Reg 19DA(2) & (3)	Does the corporate business plan comply with the requirements of <i>Local Government (Administration) Regulations 1996</i> 19DA(2) & (3)?	Yes	



Local Government Employees				
No	Reference	Question	Response	Comments
1	s5.36(4) & s5.37(3) Admin Reg 18A	Were all CEO and/or senior employee vacancies advertised in accordance with <i>Local Government (Administration) Regulations 1996</i> , regulation 18A?	N/A	There were no CEO or senior employee vacancies during 2024.
2	Admin Reg 18E	Was all information provided in applications for the position of CEO true and accurate?	N/A	As above.
3	Admin Reg 18F	Was the remuneration and other benefits paid to a CEO on appointment the same remuneration and benefits advertised for the position under section 5.36(4) of the <i>Local Government Act 1995</i> ?	N/A	As above.
4	s5.37(2)	Did the CEO inform council of each proposal to employ or dismiss senior employee?	N/A	As above.
5	s5.37(2)	Where council rejected a CEO's recommendation to employ or dismiss a senior employee, did it inform the CEO of the reasons for doing so?	N/A	No senior employees were employed or dismissed during 2024.

Official Conduct				
No	Reference	Question	Response	Comments
1	s5.120	Has the local government designated an employee to be its complaints officer?	Yes	The Chief Executive Officer.
2	s5.121(1) & (2)	Has the complaints officer for the local government maintained a register of complaints which records all complaints that resulted in a finding under section 5.110(2)(a) of the <i>Local Government Act 1995</i> ?	Yes	No complaints were received during 2024.
3	s5.121(2)	Does the complaints register include all information required by section 5.121(2) of the <i>Local Government Act 1995</i> ?	Yes	
4	s5.121(3)	Has the CEO published an up-to-date version of the register of the complaints on the local government's official website?	Yes	https://www.perenjori.wa.gov.au/registers/complaints



Optional Questions				
No	Reference	Question	Response	Comments
1	Financial Management Reg 5(2)(c)	Did the CEO review the appropriateness and effectiveness of the local government's financial management systems and procedures in accordance with the <i>Local Government (Financial Management) Regulations 1996</i> regulations 5(2)(c) within the three financial years prior to 31 December 2024? If yes, please provide the date of council's resolution to accept the report.	Yes	Reviewed at the Ordinary Council Meeting held 15 December 2022. Council Decision 151222.4.
2	Audit Reg 17	Did the CEO review the appropriateness and effectiveness of the local government's systems and procedures in relation to risk management, internal control and legislative compliance in accordance with <i>Local Government (Audit) Regulations 1996</i> regulation 17 within the three financial years prior to 31 December 2024? If yes, please provide date of council's resolution to accept the report.	Yes	As above.
3	s5.87C	Where a disclosure was made under sections 5.87A or 5.87B of the <i>Local Government Act 1995</i> , were the disclosures made within 10 days after receipt of the gift? Did the disclosure include the information required by section 5.87C of the Act?	Yes	
4	s5.90A(2) & (5)	Did the local government prepare, adopt by absolute majority and publish an up-to-date version on the local government's website, a policy dealing with the attendance of council members and the CEO at events?	Yes	Policy adopted at the Ordinary Council Meeting held 19 November 2020. Council Decision 16.3.2. Policy reviewed at the Special Meeting of Council held 2 May 2024. Council Decision 020524.2.
5	s5.96A(1), (2), (3) & (4)	Did the CEO publish information on the local government's website in accordance with sections 5.96A(1), (2), (3), and (4) of the <i>Local Government Act 1995</i> ?	Yes	
6	s5.128(1)	Did the local government prepare and adopt (by absolute majority) a policy in relation to the continuing professional development of council members?	Yes	Policy adopted at the Ordinary Council Meeting held 19 November 2020. Council Decision 16.5.2. Policy reviewed at the Special Meeting of Council held 2 May 2024. Council Decision 020524.2.



7	s5.127	Did the local government prepare a report on the training completed by council members in the 2022/2023 financial year and publish it on the local government's official website by 31 July 2024?	Yes	https://www.perenjori.wa.gov.au/registers/training
8	s6.4(3)	By 30 September 2024, did the local government submit to its auditor the balanced accounts and annual financial report for the year ending 30 June 2024?	Yes	26 September 2024.
9	s.6.2(3)	When adopting the annual budget, did the local government take into account all its expenditure, revenue and income?	Yes	

Tenders for Providing Goods and Services

No	Reference	Question	Response	Comments
1	F&G Reg 11A(1) & (3)	Did the local government comply with its current purchasing policy, adopted under the <i>Local Government (Functions and General) Regulations 1996</i> , regulations 11A(1) and (3) in relation to the supply of goods or services where the consideration under the contract was, or was expected to be, \$250,000 or less or worth \$250,000 or less?	Yes	
2	s3.57 F&G Reg 11	Subject to <i>Local Government (Functions and General) Regulations 1996</i> , regulation 11(2), did the local government invite tenders for all contracts for the supply of goods or services where the consideration under the contract was, or was expected to be, worth more than the consideration stated in regulation 11(1) of the Regulations?	Yes	<ul style="list-style-type: none"> - RFT-MWSFN-01-23.24 – Midwest Secondary Freight Network (Joint Tender, Shire of Morawa) - RFT01-2024.25 – Design & Construct 2x GROH Houses - RFT02-2024.25 – One Prime Mover - RFT03-2024.25 Pavement Repairs & Asphalt Works - RFT04-2024.25 – Panel of Pre-Qualified Suppliers - RFT04-2024.25 – Supply & Delivery of Supermarket Fit Out Items



Department of
**Local Government, Sport
and Cultural Industries**

3	F&G Regs 11(1), 12(2), 13, & 14(1), (3), and (4)	When regulations 11(1), 12(2) or 13 of the <i>Local Government Functions and General) Regulations 1996</i> , required tenders to be publicly invited, did the local government invite tenders via Statewide public notice in accordance with Regulation 14(3) and (4)?	Yes	Copies of Statewide public notices are contained within each Tender Register.
4	F&G Reg 12	Did the local government comply with <i>Local Government (Functions and General) Regulations 1996</i> , Regulation 12 when deciding to enter into multiple contracts rather than a single contract?	N/A	The Shire did not intend to enter into 2 or more contracts with the desire to avoid the requirements of regulation 11(1).
5	F&G Reg 14(5)	If the local government sought to vary the information supplied to tenderers, was every reasonable step taken to give each person who sought copies of the tender documents, or each acceptable tenderer notice of the variation?	N/A	No tenders were varied during 2024.
6	F&G Regs 15 & 16	Did the local government's procedure for receiving and opening tenders comply with the requirements of <i>Local Government (Functions and General) Regulations 1996</i> , Regulation 15 and 16?	Yes	
7	F&G Reg 17	Did the information recorded in the local government's tender register comply with the requirements of the <i>Local Government (Functions and General) Regulations 1996</i> , Regulation 17 and did the CEO make the tenders register available for public inspection and publish it on the local government's official website?	Yes	https://www.perenjori.wa.gov.au/business/tenders/past-tenders.aspx
8	F&G Reg 18(1)	Did the local government reject any tenders that were not submitted at the place, and within the time, specified in the invitation to tender?	Yes	One tenderer for RFT04-2024.25 was unsuccessful due to submitting after the deadline.
9	F&G Reg 18(4)	Were all tenders that were not rejected assessed by the local government via a written evaluation of the extent to which each tender satisfies the criteria for deciding which tender to accept?	Yes	
10	F&G Reg 19	Did the CEO give each tenderer written notice containing particulars of the successful tender or advising that no tender was accepted?	Yes	
11	F&G Regs 21 & 22	Did the local government's advertising and expression of interest processes comply with the requirements of the <i>Local Government (Functions and General) Regulations 1996</i> , Regulations 21 and 22?	Yes	EOI 01-2024 – Lease and Operation of a Supermarket.
12	F&G Reg 23(1) & (2)	Did the local government reject any expressions of interest that were not submitted at the place, and within the time, specified in the notice or that failed to comply with any other requirement specified in the notice?	No	



Department of
**Local Government, Sport
and Cultural Industries**

13	F&G Reg 23(3) & (4)	Were all expressions of interest that were not rejected under the <i>Local Government (Functions and General) Regulations 1996</i> , Regulation 23(1) & (2) assessed by the local government? Did the CEO list each person as an acceptable tenderer?	Yes	
14	F&G Reg 24	Did the CEO give each person who submitted an expression of interest a notice in writing of the outcome in accordance with <i>Local Government (Functions and General) Regulations 1996</i> , Regulation 24?	Yes	
15	F&G Regs 24AD(2) & (4) and 24AE	Did the local government invite applicants for a panel of pre-qualified suppliers via Statewide public notice in accordance with <i>Local Government (Functions & General) Regulations 1996</i> regulations 24AD(4) and 24AE?	Yes	RFT04-2024/25 - Advertised in the West Australian on Wednesday, 2 October 2024.
16	F&G Reg 24AD(6)	If the local government sought to vary the information supplied to the panel, was every reasonable step taken to give each person who sought detailed information about the proposed panel or each person who submitted an application notice of the variation?	N/A	No information supplied to the RFT04-2024/25 panel was varied.
17	F&G Reg 24AF	Did the local government's procedure for receiving and opening applications to join a panel of pre-qualified suppliers comply with the requirements of <i>Local Government (Functions and General) Regulations 1996</i> , Regulation 16, as if the reference in that regulation to a tender were a reference to a pre-qualified supplier panel application?	Yes	
18	F&G Reg 24AG	Did the information recorded in the local government's tender register about panels of pre-qualified suppliers comply with the requirements of <i>Local Government (Functions and General) Regulations 1996</i> , Regulation 24AG?	Yes	https://www.perenjori.wa.gov.au/documents/145/rft04-202425-panel-of-pre-qualified-suppliers-supply-of-plant-and-equipment-for-occasional-hire
19	F&G Reg 24AH(1)	Did the local government reject any applications to join a panel of pre-qualified suppliers that were not submitted at the place, and within the time, specified in the invitation for applications?	Yes	One tenderer for RFT04-2024.25 was unsuccessful due to submitting after the deadline.
20	F&G Reg 24AH(3)	Were all applications that were not rejected assessed by the local government via a written evaluation of the extent to which each application satisfies the criteria for deciding which application to accept?	Yes	
21	F&G Reg 24AI	Did the CEO send each applicant written notice advising them of the outcome of their application?	Yes	



22	F&G Regs 24E & 24F	Where the local government gave regional price preference, did the local government comply with the requirements of <i>Local Government (Functions and General) Regulations 1996</i> , Regulation 24E and 24F?	Yes	
----	--------------------	--	-----	--

Chief Executive Officer

Date

Mayor/President

Date

15.2 PROPOSED RAIL SIDING & ASSOCIATED WORKS – LOT 101 MULLEWA – WUBIN ROAD, PERENJORI

Applicant:	Cooperative Bulk Handling Ltd (CBH)
File:	A15014
Date:	20 March 2025
Disclosure of Interest:	
Voting Requirements:	Simple Majority
Author:	Simon Lancaster – Planning Advisor
Responsible Officer:	Paul Anderson – Chief Executive Officer
Attachments:	15.2.1 – Rail Siding Application 15.2.2 – Weighbridge Application

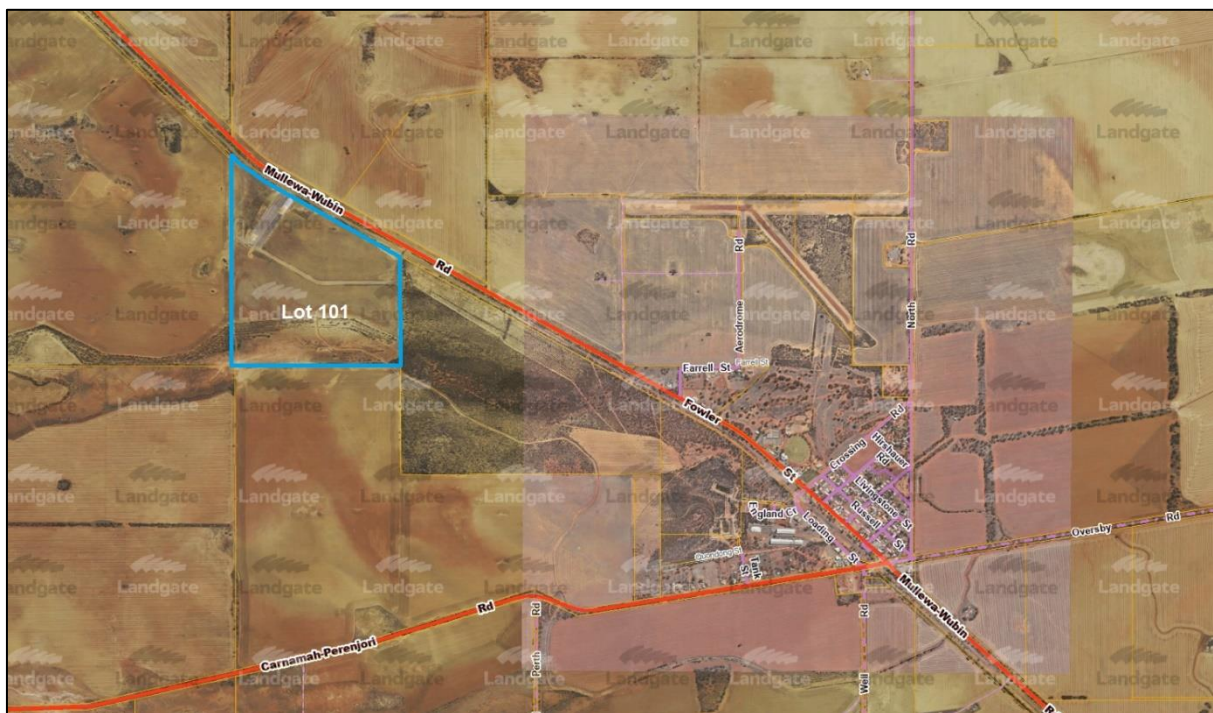
Summary

Council is in receipt of an application from CBH seeking to construct a new rail siding and associated works at its grain receival and storage facility upon Lot 101 Mullewa-Wubin Road to the north of the Perenjori townsite. This report recommends that Council approve the application subject to conditions.

Background

Lot 101 Mullewa-Wubin Road, Perenjori is a flat, cleared 97.3607ha property with a 1.1682km frontage onto the rail line and Mullewa-Wubin Road. The property is located 1.8km north-west of the northern edge of the Perenjori townsite.

Figure 15.2.1 – Location map of Lot 101 Mullewa-Wubin Road, Perenjori



Lot 101 contains CBH's Perenjori North grain receival and storage facility which occupies the northern 38ha area of the property, and consists of 6 open bulkheads, weighbridge, 2 drainage basins and heavy vehicle manoeuvring network.

CBH are seeking approval to develop the following within its existing Perenjori North footprint:

- a 1.3km rail siding to allow in future for rail outloading to occur upon Lot 101;
- building pad and footings for future rail outloading facility;
- maintenance access track west/alongside the railway siding;
- install a new weighbridge and sampling hut;
- install a 5 bay car parking area and at a later stage an amenities building for CBH staff working at the weighbridge;

- additions to the existing heavy vehicle manoeuvring and marshalling network;
- undertake surface drainage works to direct stormwater runoff into the 2 existing drainage basins and construct a 3rd drainage basin in the north-west corner of Lot 101.

Background information relating to the rail siding component that includes covering letter, detailed plans, drainage report, noise assessment and clearing information is provided as **separate Attachment 15.2.1**. The complete document is 127MB in size so some of the methodology behind the drainage calculations and noise assessment and civil engineering plans relating to the exempted rail works have been edited out to reduce the file size for the purposes of this attachment. Should Councillors wish to view the complete 203 page application report this can be provided upon request.

A copy of the weighbridge and sample platform application that includes covering letter and detailed plans is provided as **separate Attachment 15.2.2**.

Figure 15.2.2 – Aerial photo of subject portion of Lot 101 Mullewa-Wubin Road, Perenjori



Figure 15.2.3 – Proposed Rail Siding site plan

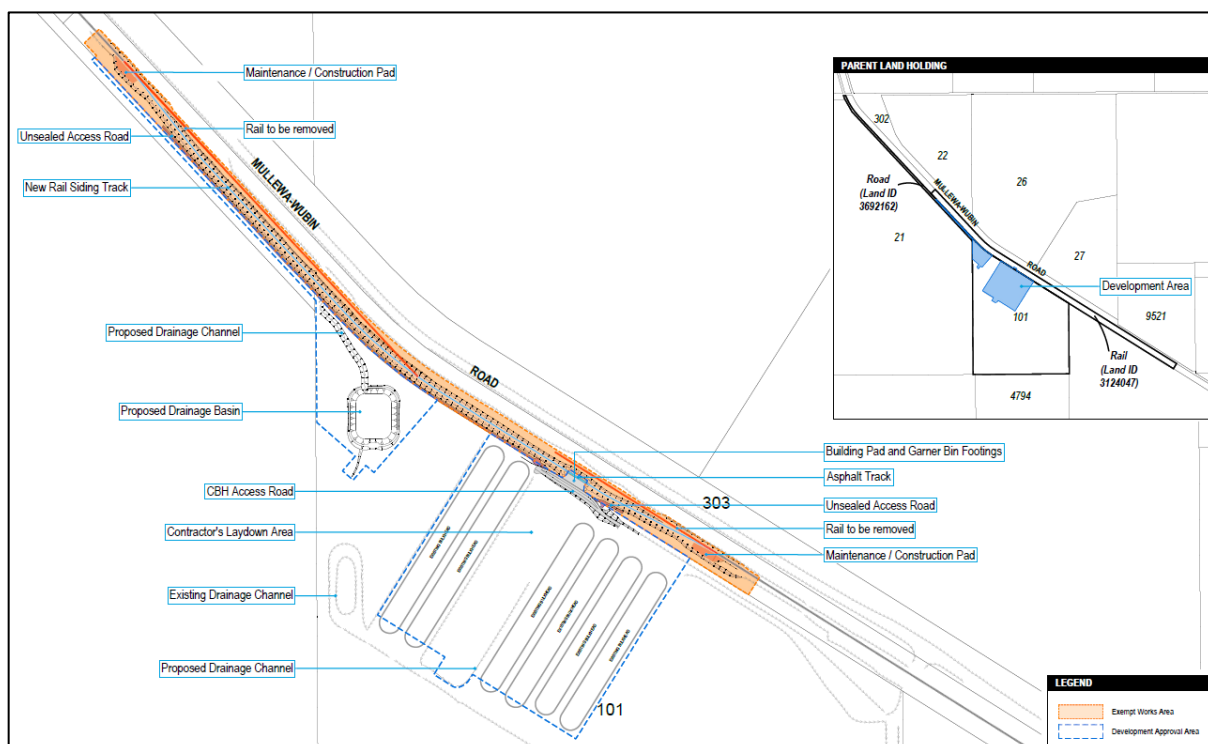
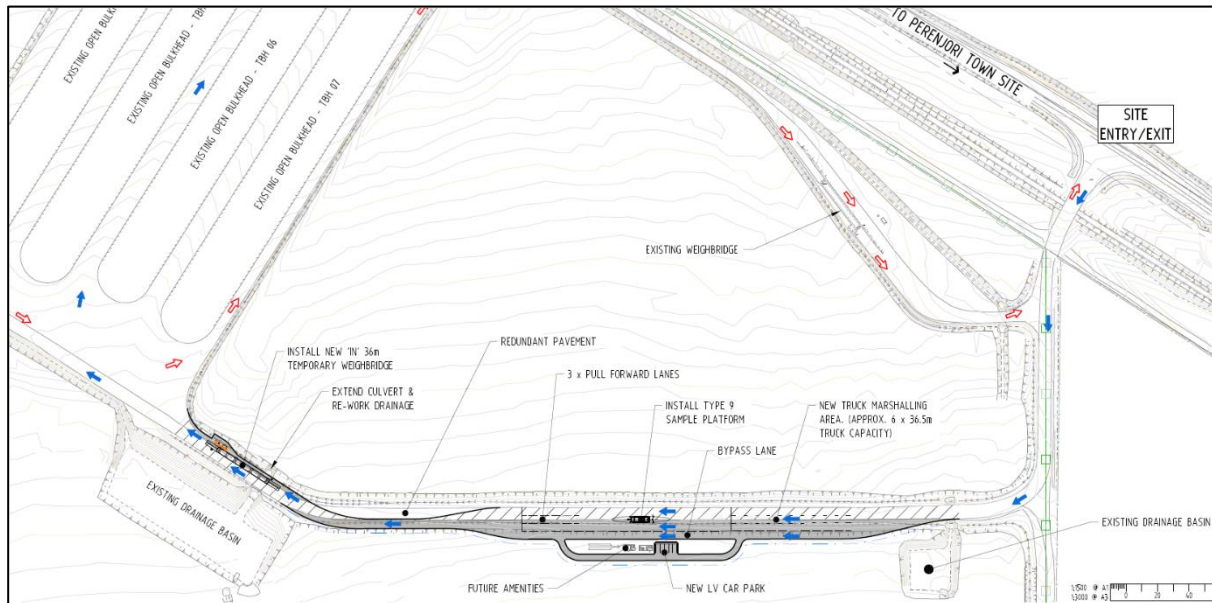


Figure 15.2.4 – Proposed Weighbridge and Sample Platform site plan



Statutory Environment

This application, and Council's determination, only relates to the aspects of the overall rail siding development that are located within Lot 101 Mullewa-Wubin Road, Perenjori, and not the rail corridor/reserve. The portion of the development within the rail corridor is deemed a public work pursuant to the *Public Works Act 1902* and *Rail Freight Systems Act 2000* and is therefore exempted.

Lot 101 Mullewa-Wubin Road, Perenjori is zoned 'Rural' under the Shire of Perenjori Local Planning Scheme No.3 ('the Scheme') which lists the objectives for this zone as being:

- *To provide for the maintenance or enhancement of specific local rural character.*
- *To protect broad acre agricultural activities such as cropping and grazing and intensive uses such as horticulture as primary uses, with other rural pursuits and rural industries as secondary uses in circumstances where they demonstrate compatibility with the primary use.*
- *To maintain and enhance the environmental qualities of the landscape, vegetation, soils and water bodies, to protect sensitive areas especially the natural valley and watercourse systems from damage.*
- *To provide for the operation and development of existing, future and potential rural land uses by limiting the introduction of sensitive land uses in the Rural zone.*
- *To provide for a range of non-rural land uses where they have demonstrated benefit and are compatible with surrounding rural uses."*

The modification of the existing grain receival and storage facility is considered to meet the definition of 'Industry-Rural' which is defined in the Scheme as follows:

"industry - rural means -

- (a) supports and/or is associated with primary production; or*
- (b) services plant or equipment used in primary production"*

'Industry-Rural' is listed as a 'P' use within the 'Rural' zone under the Scheme, that is a use that is permitted if it complies with all relevant development standards and requirements of Council.

Schedule 2 Part 9 Clause 67 of the *Planning and Development (Local Planning Schemes) Regulations 2015* lists the following relevant matters to be considered by local government in considering a development application:

- "(a) the aims and provisions of this Scheme and any other local planning scheme operating within the Scheme area;...*
- (fa) any local planning strategy for this Scheme endorsed by the Commission;*
- (g) any local planning policy for the Scheme area;...*
- ...(m) the compatibility of the development with its setting, including -*
 - (i) the compatibility of the development with the desired future character of its setting; and*

- (ii) *the relationship of the development to development on adjoining land or on other land in the locality including, but not limited to, the likely effect of the height, bulk, scale, orientation and appearance of the development;*
- (n) *the amenity of the locality including the following —*
 - (i) *environmental impacts of the development;*
 - (ii) *the character of the locality;*
 - (iii) *social impacts of the development;*
- (o) *the likely effect of the development on the natural environment or water resources and any means that are proposed to protect or to mitigate impacts on the natural environment or the water resource;...*
- ...(q) *the suitability of the land for the development taking into account the possible risk of flooding, tidal inundation, subsidence, landslip, bush fire, soil erosion, land degradation or any other risk;*
- (r) *the suitability of the land for the development taking into account the possible risk to human health or safety;*
- (s) *the adequacy of –*
 - (i) *the proposed means of access to and egress from the site; and*
 - (ii) *arrangements for the loading, unloading, manoeuvring and parking of vehicles;*
- (t) *the amount of traffic likely to be generated by the development, particularly in relation to the capacity of the road system in the locality and the probable effect on traffic flow and safety;...*
- ... (w) *the history of the site where the development is to be located;*
- (x) *the impact of the development on the community as a whole notwithstanding the impact of the development on particular individuals;...*
- ...(zb) *any other planning consideration the local government considers appropriate.”*

Policy Implications

Nil

Strategic Implications

Nil

Consultation

Council has the ability, should it wish to do so, to advertise the application to other authorities under Schedule 2 Part 8 Clause 66 of the *Planning and Development (Local Planning Schemes) Regulations 2015* and then return the matter to a future meeting of Council for consideration of any responses, prior to making its determination.

Financial Implications

Nil

Strategic Community Plan

The reduction in heavy vehicle trips would assist with the Shire of Perenjori Strategic Community Plan 2022/2023-2032/2033 priority of delivering and maintaining a high standard of roads.

Officer Comment

The application for the rail siding, weighbridge and sampling hut is supported based upon the following:

- the lot's access point onto Mullewa-Wubin Road is along a flat, straight section that was previously assessed by a traffic engineer report;
- the rail siding will allow for rail outloading to occur upon Lot 101, rather than at the CBH facility in Perenjori, this would result in an overall reduction of approximately 780 truck movements annually as 59kt of grain would no longer be required to be transported by road from the Perenjori North CBH site to the CBH Perenjori townsite where rail outloading currently occurs;
- the development is ancillary to the existing CBH grain receival and storage facility onsite;
- the development has direct access onto the existing rail network and the RAV7 Mullewa-Wubin Road.
- the rail siding development would be 2km, and the weighbridge/sampling hut 1.8km from the edge of the Perenjori townsite;
- the applicant will be required to obtain separate approvals of Main Roads WA, the Public Transport Authority and the Department of Water & Environmental Regulation and undertake the works to their satisfaction and requirements.

OFFICER RECOMMENDATION

Council Resolution Number:

Moved:

Seconded:

That Council grant formal planning approval for a rail siding and associated works (including weighbridge, sampling hut, car parking, drainage and access works) to the existing grain handling and storage facility upon Lot 101 Mullewa-Wubin Road, Perenjori subject to the following:

Conditions

1. Development shall be in accordance with the plans included as Attachments 15.2.1 and 15.2.2 to the 20 March 2025 Council Agenda and subject to any modifications required as a consequence of any condition(s) of this approval. The endorsed plans shall not be modified or altered without the prior written approval of the local government.
2. Any additions to or change of use of any part of the building or land (not the subject of this consent/approval) considered by the Chief Executive Officer to represent significant variation from the approved development plan requires further application and planning approval for that use/addition.
3. The applicant is to ensure that the location, design, construction/upgrading and ongoing maintenance of the access point from the development site onto the Mullewa-Wubin Road shall be to the requirements and approval of Main Roads WA.
4. The repairing of any damage to the Mullewa-Wubin Road including the surface required by reason of use of the road in connection with the development shall be to the requirements and approval of Main Roads WA with all costs met by the applicant.
5. The installation of any directional/traffic/advanced warning/safety signage in the vicinity of the development's vehicle crossover/access-point(s) onto Mullewa-Wubin Road and relating to the development shall be to the requirements of Main Roads WA, with all costs met by the applicant.
6. All parking of vehicles associated with the development shall be provided for within the property boundary and not the road reserve.
7. The internal road network and vehicle manoeuvring and parking areas shall be constructed and maintained to the approval of the local government.
8. The rail crossing and use thereof associated with the development shall be to the requirements of the Public Transport Authority.
9. The works to, and about, the rail network and use thereof, and about, associated with the development shall be to the requirements of the Public Transport Authority.
10. All stormwater is to be disposed of on-site to the approval of the local government.
11. Any soils disturbed or deposited on site shall be stabilised to the approval of the local government.
12. The use hereby permitted shall not cause injury to or prejudicially affect the amenity of the locality by reason of the emission of dust, noise, light, odour, vibration, waste product or otherwise.
13. The applicant is to implement and maintain reporting mechanisms for complaints concerning the operation of the development. In the event of a substantiated complaint being received the applicant is required to demonstrate mitigation response(s) to the approval of the local government.
14. Vegetation about the development must be maintained in accordance with the Asset Protection Zone requirements as contained in the Department for Planning, Lands and Heritage's Guidelines for Planning in Bushfire Prone Areas (document and appendices) and relevant requirements of the Department of Fire & Emergency Services.

Notes:

- (a) In relation to Conditions 3, 4 & 5 the applicant is required to consult with Main Roads WA's Mid West Region Office to confirm and meet with any approval requirements that may be

required relating to the construction phase and in relation to the intended operational heavy vehicle combinations and transport routes.

- (b) In relation to Conditions 8 & 9 the applicant is required to consult with the Public Transport Authority to confirm and meet with any approval requirements that may be required for their proposed operations and transport movements (construction phase and ongoing).
- (c) The applicant is advised that the proposed works should occur in a safe manner at all times and in accordance with all applicable legal and safety requirements (including the 'duty of care' under the laws of negligence, Worksafe requirements and guidelines, Australian Standards and Western Power policies and procedures).
- (d) The applicant is advised that this planning approval does not negate the requirement for any additional approvals, and adherence to due diligence, which may be required under separate legislation. This including, but not limited to, the obtaining of any required approvals from the Department of Health, the Department of Energy, Mines, Industry Regulation & Safety, the Department of Water & Environment Regulation, Public Transport Authority and Main Roads WA and consulting of Before You Dig Australia. It is the applicant's responsibility to obtain any additional approvals, and undertaking of due diligence, required before the development/use lawfully commences.
- (e) If an applicant is aggrieved by this determination there is a right pursuant to the *Planning and Development Act 2005* to have the decision reviewed by the State Administrative Tribunal. Such application must be lodged within 28 days from the date of determination.

Motion put and carried / lost

For:

Against:

[Next Item](#)

*Our Reference: 3244Ltr125B
Enquiries: Harry Norman*

28 February 2025

Paul Anderson
Chief Executive Officer
Shire of Perenjori
PO Box 22
Perenjori, WA, 6620

ATTENTION: Paul Anderson

Dear Paul

**RE: NEW RAIL SIDING
LOT 101 (NO STREET ADDRESS), LANDGATE ID NUMBER 3124047 (RAIL RESERVE)
AND LANDGATE ID NUMBER 3692162 (ROAD RESERVE), PERENJORI**

On behalf of CBH Group (CBH), CLE Town Planning + Design (CLE) are lodging a Development Application for a new rail siding located within sections of rail reserve (Landgate ID 3124047), unmade road reserves (Landgate ID 3692162) and a portion of CBH's landholding (Lot 101) in Perenjori.

It is noted that whilst CBH are listed as the applicant on the application forms, CLE are acting on behalf of CBH for this application.

Please find enclosed the following within this application for planning approval:

- Application Forms (Shire of Perenjori).
- Certificate of Title.
- Site Context Plan (CLE Ref. 3244-126C-01).
- Detailed Design Plans (BG&E).
- Environmental Memo (CBH Group).
- Drainage Report (BG&E).
- Environmental Noise Assessment (Herring Storer).
- PTA Letter of Consent.

It is our understanding that the application fees are payable once the application has been lodged with the Shire. Upon the Shire receiving and confirming receipt of the Development Application, if this amount could be confirmed, payment will be arranged.

BACKGROUND

CBH has over 150 sites across the State which receive, handle, store and outload approximately 90 percent of Western Australia's grain harvest. The current rail loading facilities at the Perenjori town site are no longer sufficient to accommodate a growing average crop size, and the relocation of rail loading activities to the Perenjori (north) site will deliver operational efficiencies for CBH and significantly reduce the impact of CBH's operations to the community in town.

The new rail siding represents the first phase of new infrastructure at the Perenjori (north) site with a future rail out loading facility to be used in conjunction with the siding. The loading facility is currently undergoing design refinements and will be subject to a separate development application at a future date.

The rail siding upgrade is one of the projects within the first package of the WA Agricultural Supply Chain Improvements (ASCI) funding program. Along with other rail siding and out loading upgrades at Brookton (complete), Cranbrook (under construction) and Konnongorring (under construction), this proposal aims to maximise efficiencies to deliver more tonnes to port. The use of rail to deliver tonnes to Port further reduces reliance on road transport which has benefits to road users and the community.

For CBH and the grain growing industry, these works will further assist the effort to move grain rapidly to port, to capitalise on market demand and deliver value to Western Australian grain growers.

Once constructed the rail siding will be owned by the State Government and managed and maintained by Arc Infrastructure.

Site Details

The application area covers three land parcels, including Lot 101 which is under the ownership of CBH. The only development proposed within CBH's landholding relates to the construction of internal access roads and provision of a new drainage basin to support the siding and future loading facility.

Land ID 3124047 forms part of the rail reserve and will be the site of the proposed rail siding. In addition to the new 1.3km rail siding, a new maintenance access track, a temporary maintenance and construction pad and the footings for the future garner bins will also be located within rail corridor.

A portion of unmade road reserves (Land ID 3692162) has been included with this application as a portion of the proposed maintenance access track and associated earthworks encroach into this section of unmade road.

There are no Certificates of Title available for the road and rail reserves (Land ID 3124047 and 3692162).

PROPOSAL

The application proposes the installation of a new rail siding within the rail reserve corridor (and a portion of Lot 101), inclusive of an associated maintenance access track and temporary construction pad. The summary details of the proposal include the following:

- A new 1.3km (approx.) rail siding within the rail reserve corridor;
- Building pad and footings for the future loading facility;
- A new rail maintenance access track along the western portion of the rail reserve boundary (Landgate ID 3124047);
- Temporary turnout installation pads and laydown areas; and
- Associated earthworks and drainage works.

Further detail regarding the extent of the proposed works are enclosed within the detailed design drawings prepared by BG&E (refer Attachment 3).

Public Works Exemption

CBH Group are of the view, and having received advice from the PTA, that the proposed rail siding works located within the area defined as 'Corridor Land' under the *Rail Freight Systems Act 2000* are considered as 'public works' in accordance with s.6 of the *Planning and Development Act 2005 (Act)* and therefore exempt from Development Approval under the local planning scheme.

This is consistent with the recent rail siding development in Konnongorring where the PTA and the Shire of Goomalling considered the works as 'public works'.

The rail siding works proposed are entirely within the 'Corridor Land' and thus exempt from Development Approval. The image below shows data from PTA showing Corridor Land in blue shading.



Figure 1: Corridor Land

This application therefore only seeks Development Approval for the works located outside the 'Corridor Land' and for the earthworks associated with the future loading facility, with the extent of these works generally depicted within the 'Development Area' as shown in Attachment 4 – Site Context Plan.

In terms of the Public Works exemption, Section 6 of the Act states the following:

6. Act does not interfere with public works

- (1) *Subject to subsections (2) to (4), nothing in this Act interferes with the right of the Crown, or the Governor, or a public authority, or a local government —*
 - (a) *to undertake, construct or provide any public work; and*
 - (b) *to take land for the purposes of that public work.*
- (2) *Rights referred to in subsection (1) are to be exercised having due regard to —*
 - (a) *the purpose and intent of any planning scheme that has effect in the locality where, and at the time when, the right is exercised; and*
 - (b) *the orderly and proper planning, and the preservation of the amenity, of that locality at that time; and*
 - (c) *any advice provided by the responsible authority in the course of the consultation required under subsection (3) in respect of the exercise of the right.*

(3) *At the time when a proposal for any public work, or for the taking of land for a public work, is being formulated, the responsible authority is to be consulted as to whether the undertaking, construction or provision of, or the taking of land for, the public work will be consistent with the matters referred to in subsection (2)(a) and (b).*

(4) *This section does not affect —*

- (a) *the application of section 5(2) and (3); or*
- (b) *the application of a region planning scheme or an improvement scheme in relation to anything done, or proposed to be done, by a public authority that is not an agency of the Crown.*

In accordance with s.6 (1) Act, works within the land denoted as 'Corridor Land' are considered as a public works which are exempt the need for Development Approval from the responsible authority (the Shire) under the relevant local planning scheme, Shire of Perenjori's Local Planning Scheme 3 (LPS 3).

CBH has liaised with the Public Transport Authority (PTA) in respect to the relevant siding works, who have confirmed that the works are to be considered as 'public works' (refer Attachment 8).

Works outside of the 'Corridor Land' are not subject to an exemption from Development Approval. A separate Development Application is therefore required for the civil and earthworks for loading facility and rail siding related subsidiary works outside of 'Corridor Land', which is the purpose of this application. As advised previously a separate Development Application will be lodged for the over rail out loading facility at a future date.

In exercising a Public Works right under s.6 (1) of the Act, CBH is required to have due regard to the matters raised in s.6 (2) and consult with the Shire in accordance with s.6 (3) of the Act.

The matters listed under s.6 (2) of the Act are listed in the table on the following table with CBH's response.

Item to be given due regard (s.6 (2))	CBH Response
a) <i>the purpose and intent of any planning scheme that has effect in the locality where, and at the time when, the right is exercised; and</i>	<p>The siding is to be constructed on land reserved for 'Railway' under the Shire's Local Planning Scheme No.3 (LPS3).</p> <p>The proposed siding is entirely consistent with the ultimate purpose intended for the reserve having regard to Clause 61 (2) of LPS3.</p> <p>Clause 14 (3)(j) of LPS3 set out the objective for the Railways reserve as follows:</p> <p style="padding-left: 40px;">a) <i>"to set aside land required for passenger rail and rail freight services."</i></p> <p>The siding is part of a railway and therefore consistent with LPS3.</p>
b) <i>the orderly and proper planning, and the preservation of the amenity, of that locality at that time; and</i>	<p>The siding will be constructed within a 'Railways' reserve which is entirely consistent with the intent of its reservation and therefore consistent with the principles of orderly and proper planning.</p> <p>With respect to amenity, noise from the operation of the siding complies with the requirements of <i>State Planning Policy 5.4. Road and Rail Noise (SPP5.4)</i>. Herring Storer have prepared an Environmental Noise Assessment for the siding (Attachment 7), which confirms compliance with SPP5.4. It should be emphasised that the proposal doesn't materially alter the level of rail noise and only looks to realign the rail line away towards the future rail loading infrastructure.</p> <p>Having regard to the above, it is considered that the existing amenity of the locality is retained. This is in the context of CBH existing operations within the Perenjori townsite, which will see</p>

	grain loading operations transition to the proposed Perenjori (North) site away from the town. This will ultimately improve the overall amenity of local residents within the town site as the transition occurs, and the wider community in reducing the number of trucks on the road.
c) <i>any advice provided by the responsible authority in the course of the consultation required under subsection (3) in respect of the exercise of the right.</i>	Refer to comments below.

Section 6 (3) of the Act also provides the following:

“At the time when a proposal for any public work, or for the taking of land for a public work, is being formulated, the responsible authority is to be consulted as to whether the undertaking, construction or provision of, or the taking of land for, the public work will be consistent with the matters referred to in subsection (2)(a) and (b).”

The letter of support from the PTA (Attachment 8) is provided to satisfy s.6 (3) of the Act and should be taken as notice of consultation with the relevant authority that CBH intends to construct the rail siding within the railway corridor as public works. For the reasons set out in the table above, CBH are of the view that the works are consistent with the matters raised in s.6 (2) (a) and (b) of the Act.

Development Approval Area

Notwithstanding the public works exemption, development approval is sought for the associated works located outside the ‘Corridor Land’. The following works subject to planning approval are set out below:

- Building pad and footings for the future loading facility within the rail reserve;
- Temporary maintenance and construction pad within the rail reserve;
- Maintenance access track along the south-west portion of the rail reserve boundary and a portion of the unmade road reserve;
- Contractor laydown area within Lot 101; and
- Associated earthworks and drainage works.

The extent of these works is depicted within the ‘Development Approval Area’ as shown in Attachment 4 – Site Overview Plan.

PLANNING ASSESSMENT

Notwithstanding the public works exemption, the following outlines how due regard has been given to the purpose and intent of LPS 3 and relevant planning policy, orderly and proper planning and the preservation of amenity of the locality.

Land Use and Zoning

Land ID 3124047 is reserved as ‘Railway’ in LPS 3. The objectives for the ‘Railway’ reserve is “*to set aside land required for passenger rail and rail freight services*”.

The proposed rail siding satisfies the objectives of LPS 3 and reflects the purpose of the ‘Railway’ reserve by facilitating the efficient movement and loading of grain in the locality to support the farming and rural pursuits in Perenjori and its surrounds.

Having regard to the above, the proposal siding is entirely consistent with the ultimate purpose intended for the reserve. The managing body Public Transport Authority have provided their consent to construct the new siding in the ‘Railways’ reserve, as outlined in the application form (Attachment 1).

Lot 101 is zoned 'Rural' in the Shire of Perenjori's LPS 3. The objectives of LPS 3 state that this zone is to maintain the rural character of the locality and protect the rural landscape and environment generally. In this regard, the works subject to Development Approval continue to not only support the rural pursuits for CBH's landholding but also the wider Perenjori locality by ensuring efficiencies in grain transport.

The associated earthworks and drainage design does not affect the viability of rural pursuits on CBH's landholding and is critical to the delivery of the proposed rail siding and intends to enhance the grain loading capacities for the site and the broader region. These works will ultimately support a future development application for a new grain loading facility on site.

This application is proposing a much-needed upgrade to the rail network to help alleviate these current shortcomings, which will provide for a far more efficient and productive local agricultural industry in the locality.

ENVIRONMENTAL

An Environmental Memo (refer Attachment 5) has been prepared by CBH to outline the extent of flora and vegetation within the rail reserve and on site. The memo outlines that 2.5ha clearing of native vegetation required for the project, has an insignificant impact given the low likelihood of Threatened Ecological Communities occurring across the site, or any other priority species.

The key outcome of the survey is that there is no evidence of breeding and foraging habitat for the Carnaby's Black Cockatoo and no threatened or priority flora species recorded in the application area. Additionally, the majority of the application area has been historically cleared for road and rail infrastructure within the rail corridor, with the proposed rail siding alignment located within areas that have already been cleared. As such, the clearing warrants no referral under the Environment Protection and Biodiversity Conservation Act 1999 (EPBC Act).

CBH are currently seeking the necessary approvals through Department of Water and Environmental Regulation concurrently with this Development Application.

ACOUSTIC

An Environmental Noise Assessment report has been prepared by Herring Storer acoustic consultants which demonstrates that the new rail siding will meet the requirements of *State Planning Policy 5.4 Road and Rail Noise (SPP 5.4)* and the *Environmental Protection (Noise) Regulations 1997* (refer Attachment 7). These policies aim to manage the impact of noise on sensitive land uses which is relevant given the two nearby residential properties.

The assessment demonstrates that the rail siding proposal and the future rail loading facility complies with all noise and vibration criteria for railway / movements and concludes that the site can operate to meet all applicable noise targets and will not pose any additional noise impacts to nearby residential properties.

DRAINAGE

A preliminary Drainage Design Report (DAR) has been prepared by BG&E Engineers to understand the drainage conditions of the site and to mitigate the increase in peak flows resulting from the proposed rail siding.

The DAR demonstrates that the proposed drainage infrastructure which is located within the northern corner of lot 101 can adequately accommodate and convey site and upstream run off without any impact to the existing main rail line or any other surrounding infrastructure or properties.

It is noted that there may be minor design developments on the site drainage plan, however they will not be material.

CONCLUSION

As explained above, the proposed rail siding is the first tranche of rail out loading upgrades at CBH's Perenjori (north) receival site. The new siding will see rail loading operations transition away from the Perenjori town site helping to reduce any perceived amenity impacts which will not only have a significant benefit for road users but also the general community. A further Development Application will be lodged in the future for the over rail out loading facility which will complement the new rail siding.

In summary, the proposed new rail siding and associated works are consistent with the purpose and intent of the rail reserve and is entirely appropriate for the location. We would appreciate if you would consider this application in a favourable manner. Should you have any queries regarding this application please contact Harry Norman on 9382 1233 or via email harry@cleplan.com.au.

Yours faithfully



HARRY NORMAN
SENIOR PLANNER
CLE TOWN PLANNING + DESIGN

- Enc. Attachment 1 – Application Forms
 Attachment 2 – Certificate of Titles
 Attachment 3 – Site Context Plan (CLE 3244-126C-01)
 Attachment 4 – Detailed Design Plans (BG&E)
 Attachment 5 – Environmental Memo (CBH Group)
 Attachment 6 – Drainage Report (BG&E)
 Attachment 7 – Environmental Noise Assessment (Herring Storer)
 Attachment 8 – PTA Letter of Consent
- cc. Kellie Todman, CBH Group
 Charlotte Martin, CBH Group
 Simon Lancaster, Chapman Valley



17 December 2024

Shire of Perenjori
PO Box 22,
Perenjori WA 6620

To Whom it may concern,

Landowner Consent for CBH Development Application for Rail Loading Facility and Associated Works at Mullewa-Wubin Road in Perenjori, identifiable as UCL on Landgate ID 3124047.

The Public Transport Authority of WA (PTA) acknowledges receipt of Co-operative Bulk Handling Limited's ("CBH") application for development approval to the Shire of Northam, to facilitate a rail loading facility and associated works on land managed by PTA in Perenjori.

The proposed development works, the subject of this development application, fall within PTA reserve and a portion of land within the Rail Freight Corridor that is leased by PTA to Arc Infrastructure on a 49-year lease.

The PTA manages, maintains, and controls all government railway land (Government Railway's Act 1904 s.13) and, as the management body for the parcel of land, consents to CBH's lodgement of this development application over the rail reserve. PTA's consent is conditional that all costs incurred are borne by CBH Group.

Yours sincerely

Michael Parker
EXECUTIVE DIRECTOR
INFRASTRUCTURE PLANNING AND LAND SERVICES

Tel: (08) 9326 2603

Email: Michael.Parker@pta.wa.gov.au



14 February 2024

Mr Zachary Gonzales
Head of Project Delivery – Rail Outloading
CBH Group
Level 6, 240 St Georges Terrace
PERTH WA 6000

Dear Zachary,

Australian Supply Chain Improvement Program

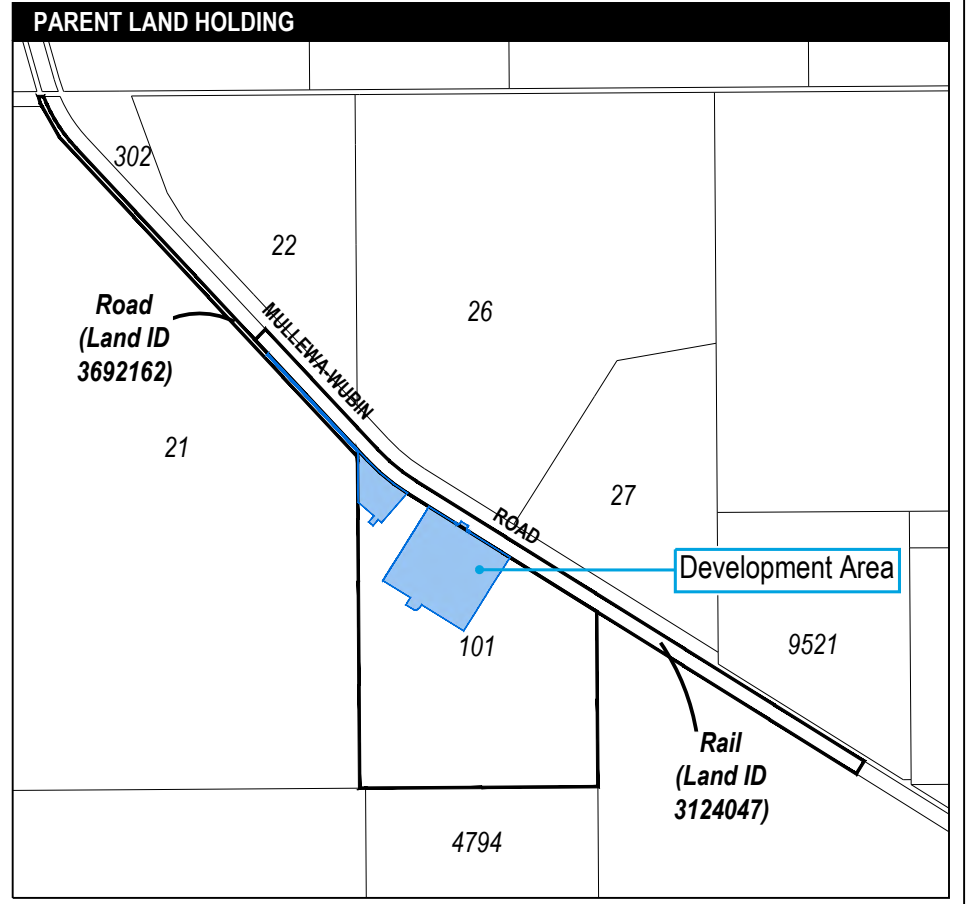
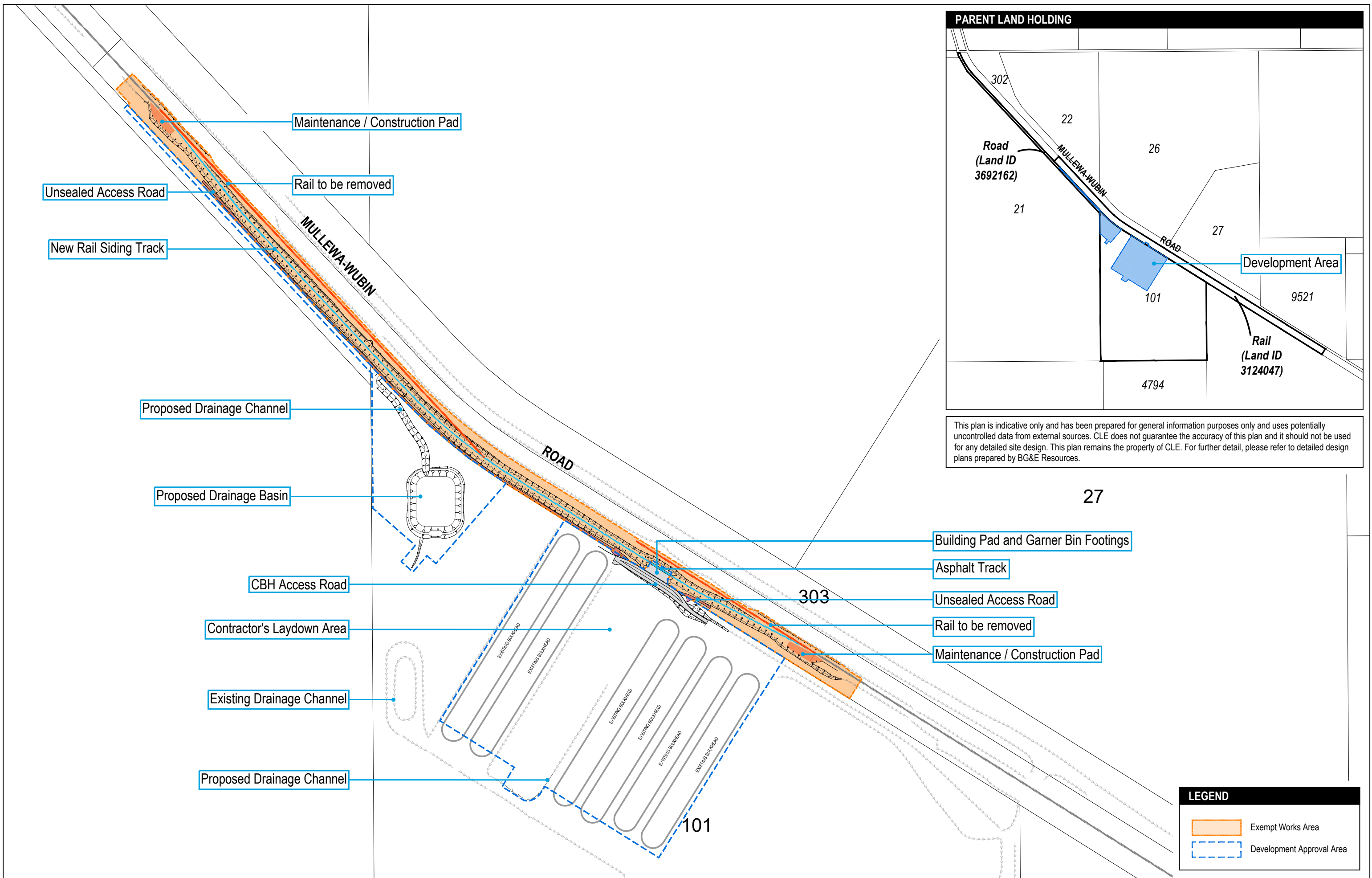
The Government of Western Australia wishes to construct new rail sidings and CBH wishes to construct associated over rail grain loading infrastructure, including at Co-operative Bulk Handling Limited's (CBH) receival site at Perenjori (**Perenjori Project**), to facilitate the more efficient loading and transport of grain. The Perenjori Project will deliver rail freight efficiencies in the transportation of grain to ports, allowing more product to get to market during the peak price season between January to June.

The Perenjori Project will be predominately constructed on land that the State of Western Australia has leased to Arc Infrastructure NarrowGauge Pty Ltd pursuant to the NarrowGauge Lease.

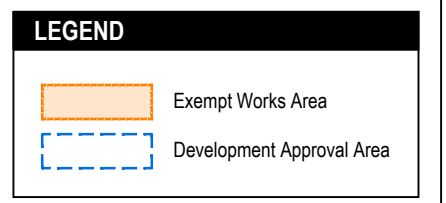
CBH will manage and carry out both the siding works and the load infrastructure works comprising the Perenjori Project.

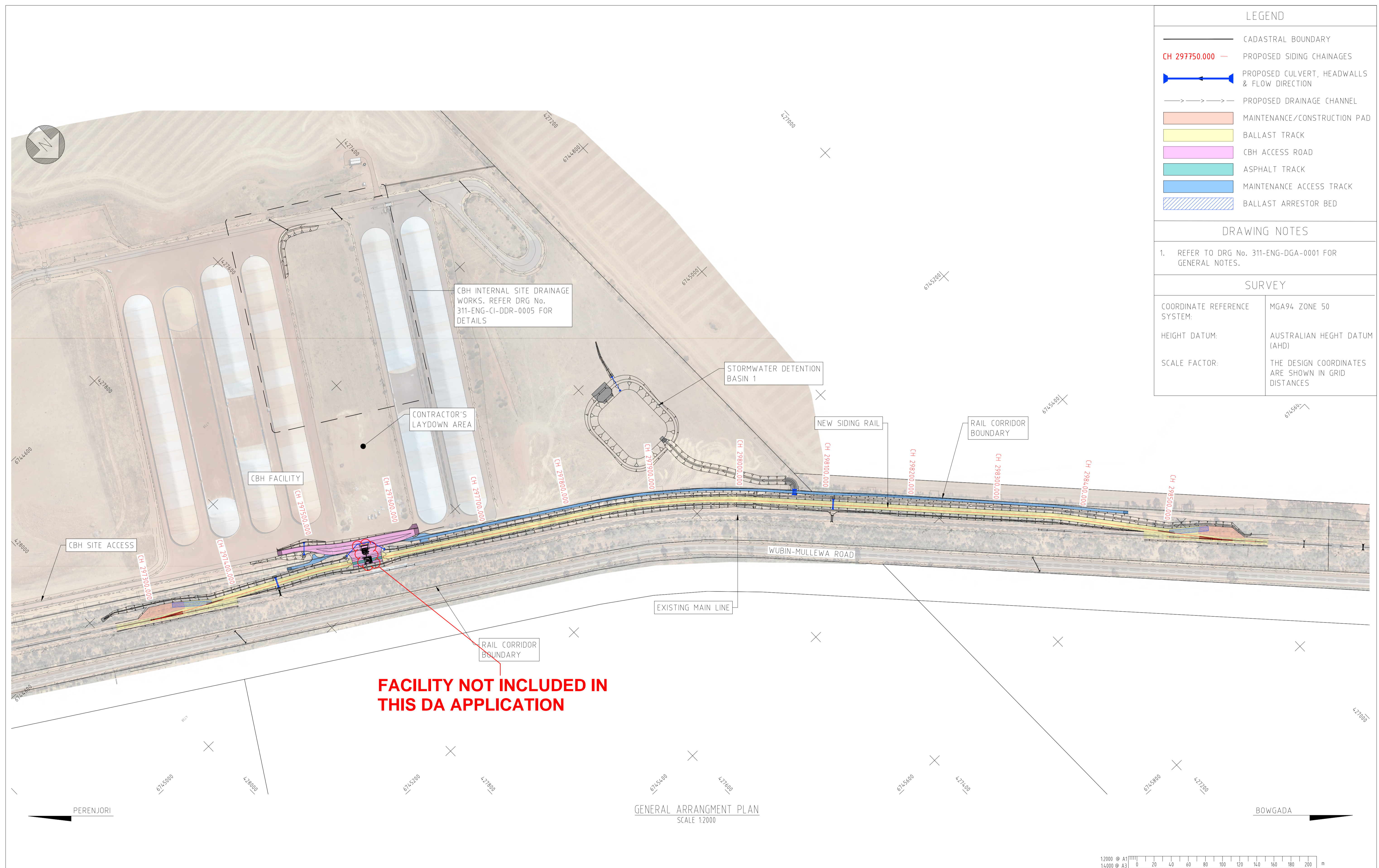
The Public Transport Authority (**PTA**), a body corporate established under the *Public Transport Authority Act 2003*, exercising its powers under sections 42 and 47 (among others) of the *Rail Freight System Act 2000 (WA)*, has the responsibility to manage State assets on Corridor Land and must undertake or approve all construction on Corridor Land subject to the NarrowGauge Lease. Pursuant to section 43 of the *Rail Freight System Act 2000*, the PTA performs the functions of the State under the NarrowGauge Lease.



Subject to CBH complying with sections 6(2) and (3) of the *Planning and Development Act 2005*, the PTA expressly authorises the construction of new siding works on Corridor Land (**CBH Works**) that CBH proposes to conduct for and on behalf of the PTA to be performed as public works, falling within the public works exemption as works authorised to be carried out under the authority of section 99 of the *Public Works Act 1902*, the *Wongan Hills – Mullewa Railway Act 1911, No. 16 of 1911* and the *Statute Law Revision Act 1965*.



This plan is indicative only and has been prepared for general information purposes only and uses potentially uncontrolled data from external sources. CLE does not guarantee the accuracy of this plan and it should not be used for any detailed site design. This plan remains the property of CLE. For further detail, please refer to detailed design plans prepared by BG&E Resources.





COPYRIGHT © THIS DRAWING AND THE CONTENTS DEPICTED OR WRITTEN THEREON, WHETHER IN WHOLE OR IN PART, IS THE EXCLUSIVE INTELLECTUAL PROPERTY OF CBH GROUP AND SHOULD NOT BE REPRODUCED OR USED FOR ANY PURPOSE WITHOUT THE PRIOR WRITTEN APPROVAL OF CBH GROUP. DO NOT SCALE FROM THIS DRAWING	 ABN 29 256 604 947	Perth Office— Level 19, Kings Square 1 556 Wellington St, Perth WA 6000 P / +61 8 6364 3300 E / info@bgeeng.com bgeeng.com—											SCALE	1:2000	DRAWN	AS	14.12.23	TITLE PERENJORI FIXED RAIL LOADING FACILITY RAIL AND CIVIL GENERAL ARRANGEMENT PLAN	
				E	24.01.25	RE-ISSUED FOR 100% CLIENT REVIEW	AS	TB	PW		SHEET	A1	CHECKED	AE	14.12.23				
				B	14.12.23	ISSUED FOR 100% CLIENT REVIEW	AS	TB	MB				DESIGNED	SOB	14.12.23				
				C	10.11.23	ISSUED FOR 85% CLIENT REVIEW	AS	TB	MB				DESIGN APPR	JH/TB/EC/AI	14.12.23				
				B	15.09.23	ISSUED FOR 60% CLIENT REVIEW	AS	EC/TB	MB										
				A	21.04.23	ISSUED FOR 30% CLIENT REVIEW	JH	MB	MB				CONTRACT No						
REF DRAWING No.		REFERENCE DRAWING TITLE	REV	DATE	REVISION DESCRIPTION	BY	CHKD	APPD									DWG No	SHEET	REV.
																	311-ENG-RL-DGA-0003	1 OF 1	E



50kg RAIL CONTINUOUSLY WELDED
SLEEPER SPACING 667mm.
SLEEPER SPACING VARIES ADJACENT TO
TURNOUTS, CATCHPOINTS AND SLAB TRACK

NEW SIDING

TOP OF RAIL
(TOR) REFERENCE POINT

LOW PROFILE CONCRETE
SLEEPER WG5490-09_4

BALLAST

RAIL FASTENING AS PER WG5490-09_4
PANDROL S.G. IRON SHOULDER

300 MIN. AND 450mm FOR CURVED TRACK WITH R<600m

WALKWAY

3%

1.5
1

1.5
1

3%

2
1

2
1

REFER 311-ENG-RL-DSE-0008
FOR SUBGRADE DETAILS

TOP OF FORMATION (TOF)
REFERENCE POINT

2050

2650

300mm DEEP SUB-BALLAST
FORMATION CAPPING

4250

3000

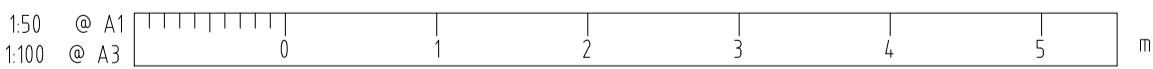
TOP OF SUBGRADE/EARTHWORKS

Diagram Description: This cross-section diagram illustrates the vertical and horizontal structure of a railway track. The track is centered on a 2650mm wide formation, with a 2050mm wide ballast bed. The ballast is composed of layers: a 150mm top layer, a 300mm sub-ballast capping layer, and a 300mm deep sub-ballast formation capping layer. The track is supported by low-profile concrete sleepers (WG5490-09_4) with 50kg continuously welded rails. The rail spacing is 667mm. The track is flanked by a 300mm wide shoulder for curved tracks with a radius less than 600m. The track is built on a subgrade with a 3% cross-slope. The diagram also shows a walkway on the left side and a new siding on the right. Key reference points include the Top of Rail (TOR), Top of Formation (TOF), and Top of Subgrade/Earthworks. Dimensions are provided for the track width, ballast bed, and formation. A reference to 311-ENG-RL-DSE-0008 is provided for subgrade details.

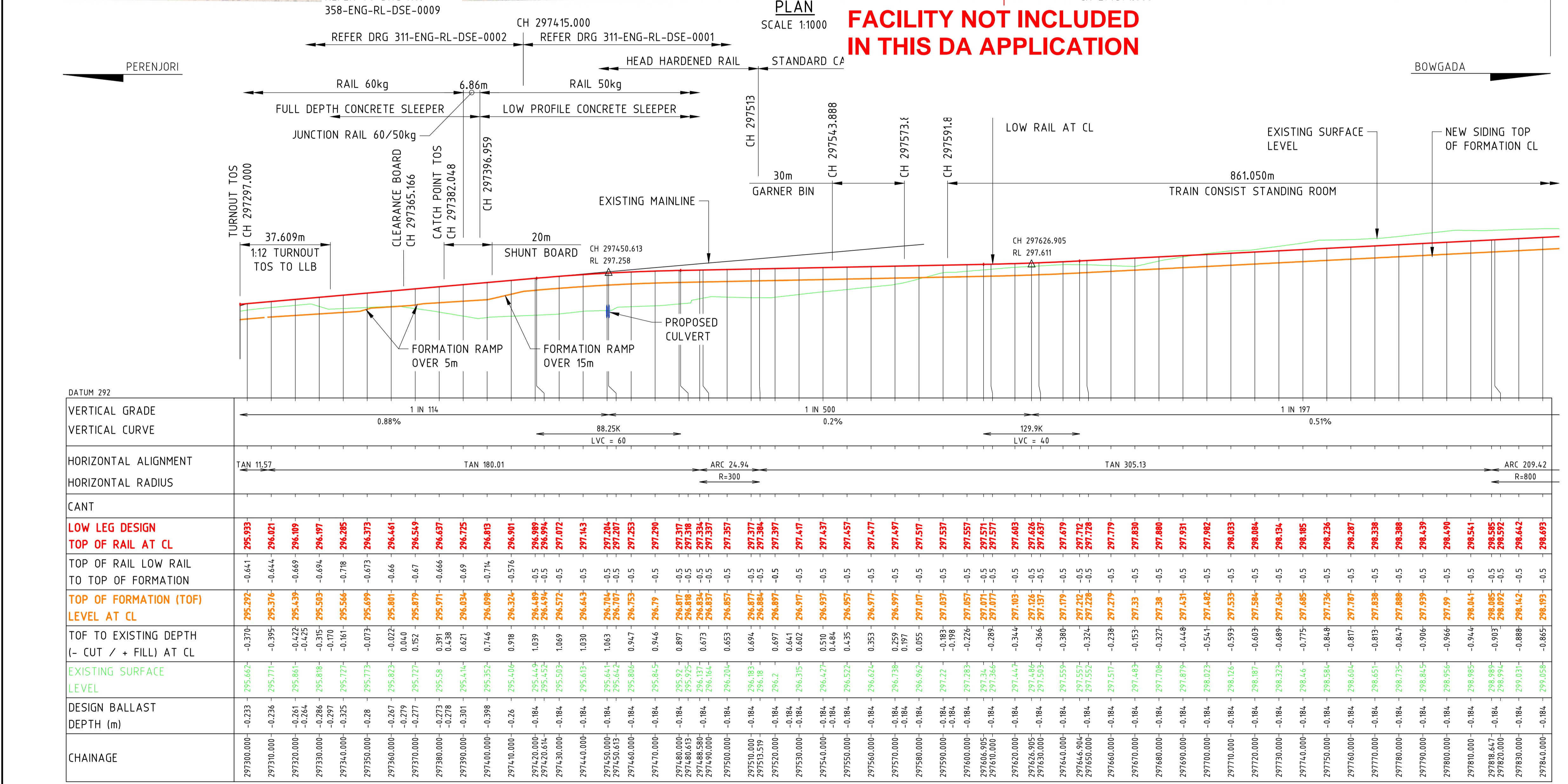
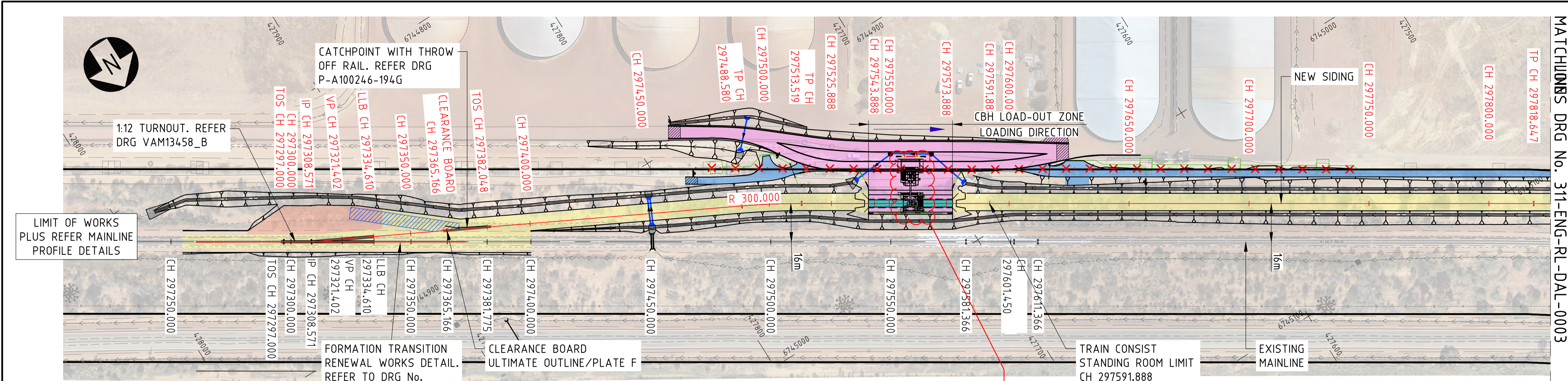
1.25 @ A1 150 @ A3 0 400 800 1200 1600 2000 2400 mm



TIE IN SIDING/MAINLINE
MAINLINE SUBSTRUCTURE AS PER EXISTING
SCALE 1:50



REV.



LEGEND

CADASTRAL BOUNDARY

CH 89640.555 —

CH 297750.000 —

EXISTING MAINLINE

PROPOSED SIDING/MAINLINE

PROPOSED CULVERT, HEADWALLS & FLOW DIRECTION

MAINTENANCE/CONSTRUCTION PAD

BALLAST TRACK

CBH ACCESS ROAD

ASPHALT TRACK

MAINTENANCE ACCESS TRACK

BALLAST ARRESTOR BED

LONGITUDINAL PROFILE LEGEND

TOP OF SIDING LOW RAIL AT CL

TOP OF FORMATION DESIGN AT CL

EXISTING MAINLINE

EXISTING SURFACE LEVEL

DRAWING NOTES

1. ALL DIMENSIONS ARE IN METRES (m) UNLESS NOTED OTHERWISE.

2. REFER TO DRG No. 311-ENG-RL-DGA-0001 FOR GENERAL NOTES.

3. FOR TRACK STRUCTURE COMPONENTS ALONG SIDING REFER TO THE FOLLOWING:

60kg RAIL - 170mm

50kg RAIL - 154mm

JUNCTION RAIL 60/50 - 170/154mm

RAIL PAD - 8mm

FULL DEPTH CONCRETE SLEEPERS - 215mm

LOW PROFILE CONCRETE SLEEPERS - 155mm

TURNOUT CONCRETE SLEEPERS - 230mm

4. FOR TRACK STRUCTURE COMPONENTS ALONG MAINLINE REFER TO THE FOLLOWING:

41kg RAIL - 136.5mm

60kg RAIL - 170mm

JUNCTION RAIL 60/41 - 170/136.5mm

JUNCTION RAIL 41/31 - 136.5/117.5mm

EXISTING 31kg RAIL

RAIL PAD - 8mm

FULL DEPTH CONCRETE SLEEPERS - 215mm

LOW PROFILE CONCRETE SLEEPERS - 155mm

TURNOUT CONCRETE SLEEPERS - 230mm

EXISTING TIMBER SLEEPERS

EXISTING STEEL SLEEPERS

5. TURNOUTS AND CATCHPOINTS TO BE MANUFACTURED FROM HEAD HARDENED RAIL, JUNCTION RAILS TO BE MANUFACTURED FROM STANDARD CARBON.

6. SIDING DESIGN BALLAST DEPTH: 200mm

7. MAINLINE DESIGN BALLAST DEPTH: 250mm

SURVEY

COORDINATE REFERENCE SYSTEM:

HEIGHT DATUM:

SCALE FACTOR:

MGA94 ZONE 50

AUSTRALIAN HEGHT DATUM (AHD)

THE DESIGN COORDINATES ARE SHOWN IN GRID DISTANCES

COPYRIGHT ©
THIS DRAWING AND THE CONTENTS
DEPICTED OR WRITTEN THEREON, WHETHER
IN WHOLE OR IN PART, IS THE EXCLUSIVE
INTELLECTUAL PROPERTY OF CBH GROUP
AND SHOULD NOT BE REPRODUCED OR
USED FOR ANY PURPOSE WITHOUT THE
PRIOR WRITTEN APPROVAL OF CBH GROUP.

DO NOT SCALE FROM THIS DRAWING



Perth Office—
Level 19, Kings Square 1
556 Wellington St, Perth WA 6000
P / +61 8 6364 3300 E / info@bgeeng.com
bgeeng.com—

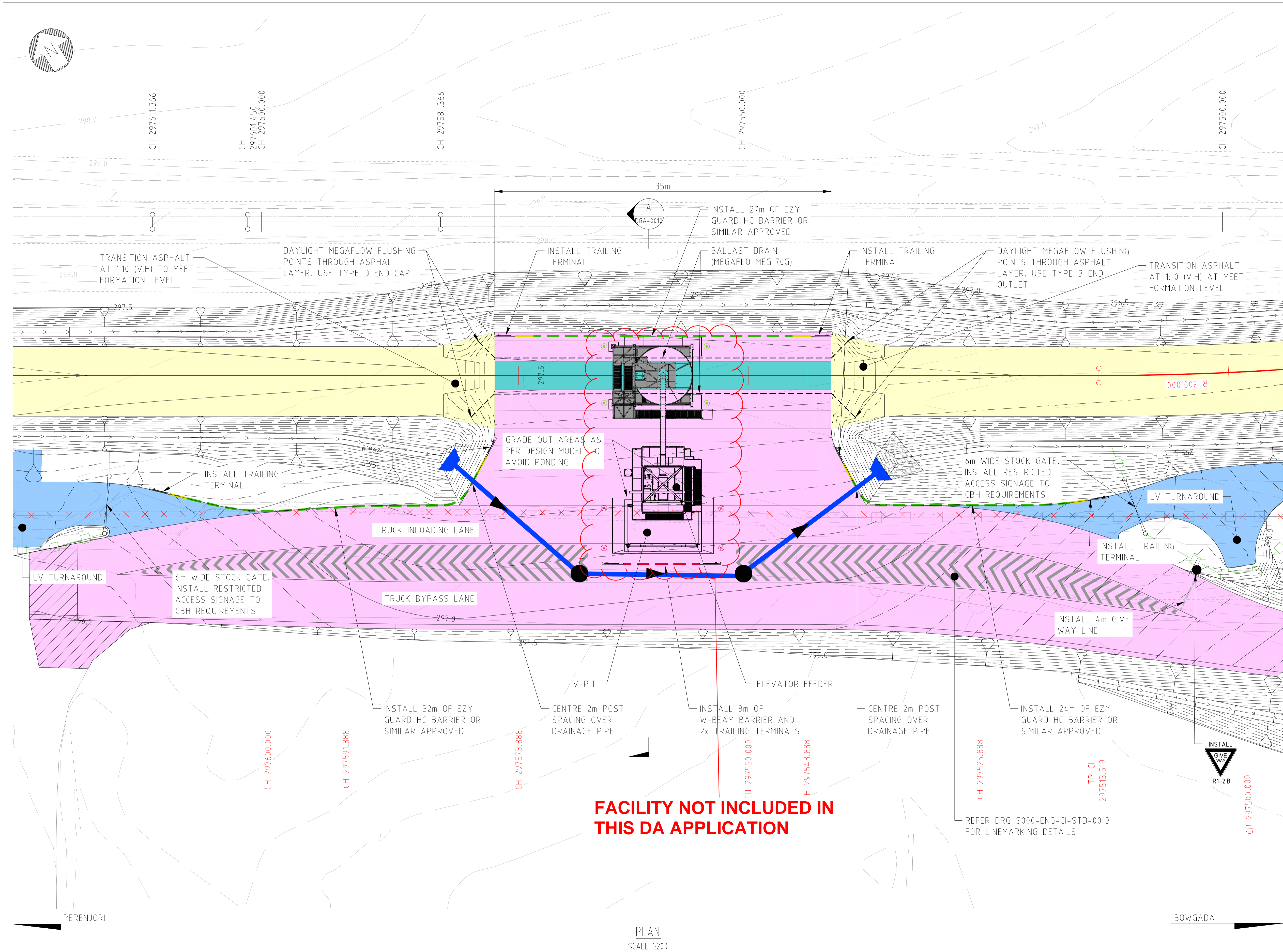


BG&E Project No. PE22014

REF DRAWING No.	REFERENCE DRAWING TITLE	REV	DATE	REVISION DESCRIPTION
		E	24.01.25	RE-ISSUED FOR 100% CLIENT REVIEW
		D	14.12.23	ISSUED FOR 100% CLIENT REVIEW
		C	10.11.23	ISSUED FOR 85% CLIENT REVIEW
		B	15.09.23	ISSUED FOR 60% CLIENT REVIEW
		A	21.04.23	ISSUED FOR 30% CLIENT REVIEW

AS	TB	PW	SCALE	DRAWN	AS	14.12.23
			1:1000H, 1:100V			
			SHEET	CHECKED	AE	14.12.23
			A1	DESIGNED	SDB	14.12.23
			PROJECT	DESIGN APPR	JH/TB/EC/AI	14.12.23
			PERENJORI RAIL			
			CONTRACT No	PROJECT APPR	MB	
			BY	CHK'D		
			APP'D			

TITLE	DRG No	SHEET	REV.
PERENJORI FIXED RAIL LOADING FACILITY RAIL AND CIVIL SIDING PLAN AND PROFILE SHEET 1 OF 3	311-ENG-RL-DAL-0002	1 OF 3	E



LEGEND

- CADASTRAL BOUNDARY
- CH 89640.555 — MAINLINE CHAINAGES
- CH 297750.000 — PROPOSED SIDING CHAINAGES
- PROPOSED CULVERT, HEADWALLS & FLOW DIRECTION
- DRAINAGE PIT
- PROPOSED DRAINAGE CHANNEL
- EXISTING DRAINAGE CHANNEL
- BALLAST DRAIN MEGAFLOW MEG170G
- DESIGN MAJOR CONTOUR
- DESIGN MINOR CONTOUR
- BALLAST TRACK
- CBH ACCESS ROAD
- ASPHALT TRACK
- EXISTING FENCE
- EXISTING FENCE TO BE REMOVED
- PROP 1200mm HIGH STOCK FENCE
- MOUNTABLE KERB A-2(x)
- EW-BEAM BARRIER
- EZY GUARD HC BARRIER
- TRANSITION BARRIER
- TRAILING TERMINAL
- FIXED BOLLARD
- REMOVABLE BOLLARD

DRAWING NOTES

- ALL DIMENSIONS ARE IN METRES (m) UNLESS NOTED OTHERWISE.
- REFER TO DRAWING 311-ENG-RL-DGA-0001 FOR GENERAL NOTES.

SURVEY

COORDINATE REFERENCE SYSTEM:	MGA94 ZONE 50
HEIGHT DATUM:	AUSTRALIAN HEIGHT DATUM (AHD)
SCALE FACTOR:	THE DESIGN COORDINATES ARE SHOWN IN GRID DISTANCES

COPYRIGHT ©
THIS DRAWING AND THE CONTENTS
DEPICTED OR WRITTEN THEREON, WHETHER
IN WHOLE OR IN PART, IS THE EXCLUSIVE
INTELLECTUAL PROPERTY OF CBH GROUP
AND SHOULD NOT BE REPRODUCED OR
USED FOR ANY PURPOSE WITHOUT THE
PRIOR WRITTEN APPROVAL OF CBH GROUP.
DO NOT SCALE FROM THIS DRAWING

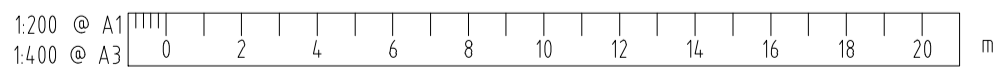


Perth Office—
Level 19, Kings Square 1
556 Wellington St, Perth WA 6000
P / +61 8 6364 3300 E / info@bgeeng.com
bgeeng.com—



BG&E Project No. PE22014

REF	DRAWING No.	REFERENCE DRAWING TITLE	REV	DATE	REVISION DESCRIPTION	BY	CHK'D	APP'D	SCALE	DRAWN	AS	14.12.23	TITLE
D	24.01.25	RE-ISSUED FOR 100% CLIENT REVIEW	AS	TB	PW				1:200				PERENJORI
C	14.12.23	ISSUED FOR 100% CLIENT REVIEW	AS	TB	MB				A1	CHECKED	AE	14.12.23	FIXED RAIL LOADING FACILITY
B	10.11.23	ISSUED FOR 85% CLIENT REVIEW	AS	TB	MB				PROJECT	DESIGNED	SOB	14.12.23	RAIL AND CIVIL
A	15.09.23	ISSUED FOR 60% CLIENT REVIEW	JH	MB	MB				PERENJORI RAIL	DESIGN APPR	JH/TB/EC/AI	14.12.23	ORB PLAN
									CONTRACT No	PROJECT APPR	MB		311-ENG-RL-DSE-0004



MATCHLINE

JOINS DRG No. 311-ENG-CI-DCL0002



PLAN
SCALE 1:500

MATCHLINE

JOINS DRG No. 311-ENG-CI-DCL-0004

LEGEND

- CADASTRAL BOUNDARY
- CH 89640.555 — MAINLINE CHAINAGES
- CH 297750.000 — PROPOSED SIDING CHAINAGES
- EXISTING MAINLINE
- PROPOSED DISTURBANCE AREA
- PROPOSED CULVERT & HEADWALLS
- DRAINAGE PIT
- PROPOSED DRAINAGE CHANNEL
- EXISTING DRAINAGE CHANNEL
- EXISTING FENCE
- EXISTING FENCE TO BE REMOVED
- PROP 1200mm HIGH STOCK FENCE
- EXISTING TREE WITH 25m DISTURBANCE RADIUS
- MAINTENANCE/CONSTRUCTION PAD
- BALLAST TRACK
- CBH ACCESS ROAD
- ASPHALT TRACK
- MAINTENANCE ACCESS TRACK
- BALLAST ARRESTOR BED
- VEGETATION DISTURBANCE

DRAWING NOTES

- ALL DIMENSIONS ARE IN METRES (m) UNLESS NOTED OTHERWISE.
- REFER TO DRAWING 311-ENG-RL-DGA-0001 FOR GENERAL NOTES.

SURVEY

COORDINATE REFERENCE SYSTEM:	MGA94 ZONE 50
HEIGHT DATUM:	AUSTRALIAN HEIGHT DATUM (AHD)
SCALE FACTOR:	THE DESIGN COORDINATES ARE SHOWN IN GRID DISTANCES

COPYRIGHT ©
THIS DRAWING AND THE CONTENTS
DEPICTED OR WRITTEN THEREON, WHETHER
IN WHOLE OR IN PART, IS THE EXCLUSIVE
INTELLECTUAL PROPERTY OF CBH GROUP
AND SHOULD NOT BE REPRODUCED OR
USED FOR ANY PURPOSE WITHOUT THE
PRIOR WRITTEN APPROVAL OF CBH GROUP.
DO NOT SCALE FROM THIS DRAWING



Perth Office—
Level 19, Kings Square 1
556 Wellington St, Perth WA 6000
P / +61 8 6364 3300 E / info@bgeeng.com
bgeeng.com—



BG&E Project No. PE22014

REF DRAWING No.	REFERENCE DRAWING TITLE	REV	DATE	REVISION DESCRIPTION	BY	CHK'D	APP'D
		D	14.12.23	ISSUED FOR 100% CLIENT REVIEW	AS	TB	MB
		C	10.11.23	ISSUED FOR 85% CLIENT REVIEW	AS	TB	MB
		B	15.09.23	ISSUED FOR 60% CLIENT REVIEW	AS	EC/TB	MB
		A	21.04.23	ISSUED FOR 30% CLIENT REVIEW	JH	MB	MB

SCALE	1:500	DRAWN	AS	14.12.23
SHEET	A1	CHECKED	AE	14.12.23
PROJECT	PERENJORI RAIL	DESIGNED	SOB	14.12.23
CONTRACT No.		DESIGN APPR	JH/TB/EC/AI	14.12.23
		PROJECT APPR	MB	

TITLE	PERENJORI FIXED RAIL LOADING FACILITY CIVIL EARTHWORKS AND DRAINAGE DISTURBANCE PLANS SHEET 2 OF 3
DRG No.	311-ENG-CI-DCL-0003
SHEET	2 OF 3
REV.	D

Memo on Clearing for Shire of Perenjori.

21 February 2025

Clearing Overview

The Project will result in clearing up to 2.503 ha of native vegetation (the Disturbance Footprint) within a Development Envelope of 21.45 ha (Figure 1-1).

Vegetation

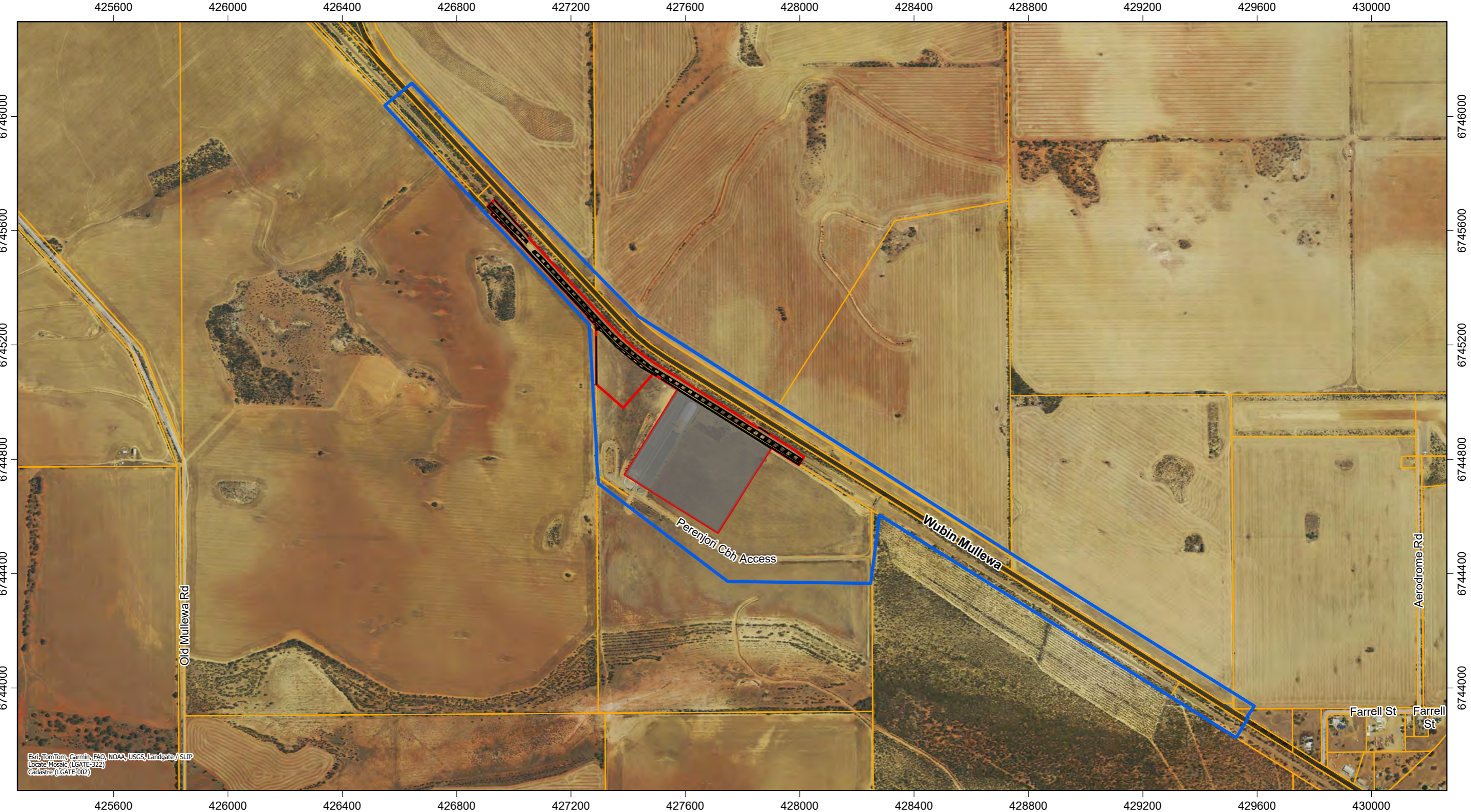
The Project will result in clearing up to 2.503 ha of native vegetation in Very Good to Degraded condition (Figure 1-6).

Four native vegetation communities which intersect the Development Envelope are mapped in (Figure 1-7).

Presence of the Eucalypt Woodland of the WA Wheatbelt TEC was assessed. No patches of vegetation were assessed as meeting the key diagnostic characteristics to be considered part of the federally protected TEC (AECOM, 2023).

Flora

47 individuals from 14 record locations of Priority 3 species *Grevillea granulosa* and 6 individuals from three locations of Priority 3 species *Apectospermum exsertum* will be removed (Figure 1-7).






Esri, TomTom, Garmin, FAO, NOAA, USGS, Landgate / SLIP
Locate Mosaic (LGATE-322)
Cadastre (LGATE-002)





Figure 1-1 - Perenjori Development Envelope & Disturbance Footprint

LEGEND

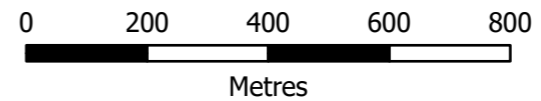
Boundaries

-  Survey Area
-  Development Envelope
-  Cadastre (LGATE-002)

Areas


-  Disturbance Footprint
-  Bulkhead

Development Envelope Area	21.5 ha
Disturbance Footprint	2.50 ha



Publication Date: 20/03/2024 1:40 PM
Map Scale: 1:12,500 (when printed at A3)
Spatial Reference System: GDA2020 MGA Zone 50
Datum: GDA2020
Projection: Transverse Mercator

CBH Group
240 St Georges Terrace
Perth WA 6000
Ph (08) 9237 9600
Fax (08) 9322 3942



Copyright ©
This figure and the contents depicted or written thereon, whether in whole or in part, is the exclusive intellectual property of CBH Group and should not be reproduced or used for any purpose without the prior written approval of CBH Group. Do not scale from this Figure.



Figure 1-6 -Clearing Impact- Vegetation Condition

LEGEND

Development Envelope

Disturbance Footprint

Cadastre (LGATE-002)

Priority Flora

P3 - Apectospermum exsertum

P3 - Grevillea granulosa

Vegetation Condition

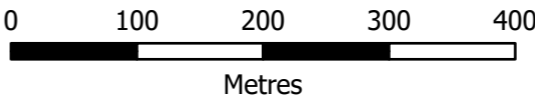
Cleared

Completely Degraded

Degraded

Good

Very Good



Publication Date: 27/03/2024 8:48 AM
Map Scale: 1:6,000 (when printed at A3)
Spatial Reference System: GDA2020 MGA Zone 50
Datum: GDA2020
Projection: Transverse Mercator

CBH Group
240 St Georges Terrace
Perth WA 6000
Ph (08) 9237 9600
Fax (08) 9322 3942



Copyright ©
This figure and the contents depicted or written thereon, whether in whole or in part, is the exclusive intellectual property of CBH Group and should not be reproduced or used for any purpose without the prior written approval of CBH Group. Do not scale from this Figure.



Esri, TomTom, Garmin, FAO, NOAA, USGS, Landgate / SLIP
Locate Mosaic (LGATE-322)
Cadastre (LGATE-002)



Figure 1-7 -Clearing Impact- Vegetation Communities

LEGEND

- Development Envelope
- Disturbance Footprint
- Cadastre (LGATE-002)

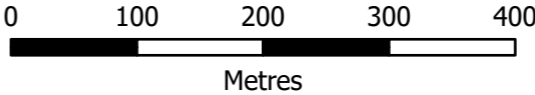
Priority Flora

- P3 - Apectospermum exsertum
- P3 - Grevillea granulosa

Vegetation

- Cleared
- EeMhAe: Melaleuca Tall Open Shrubland
- ElsEttCe: Eucalyptus Mid Open Woodland
- GofWaa: Grevillea Tall Open Shrubland
- Planted

Vegetation	Clearing Required (ha)
EeMhAe	1.22
ElsEttCe	0.03
GofWaa	1.24
Planted	0.02
Total	2.50



Publication Date: 27/03/2024 8:53 AM
Map Scale: 1:6,000 (when printed at A3)
Spatial Reference System: GDA2020 MGA Zone 50
Datum: GDA2020
Projection: Transverse Mercator

CBH Group
240 St Georges Terrace
Perth WA 6000
Ph (08) 9237 9600
Fax (08) 9322 3942



Copyright ©
This figure and the contents depicted or written thereon, whether in whole or in part, is the exclusive intellectual property of CBH Group and should not be reproduced or used for any purpose without the prior written approval of CBH Group. Do not scale from this Figure.

PERENJORI RAIL & CIVIL DRAINAGE DESIGN REPORT

Date 24.01.2025

BGE Project Number PE22021.01

CBH Project Number CW21228
CBH 311-3066-DR-RPT-0001 Rev D
100% Design Stage

Table of Contents

1. Executive Summary	1
2. Standards	2
2.1 Introduction	2
3. Design Criteria	3
3.1 Introduction	3
4. Deviations from Design Criteria	6
4.1 Introduction	6
5. Drainage Design	9
5.1 Introduction	9
5.2 Assumptions	9
5.3 Existing Condition	9
5.4 Design Scenario	13
5.4.1 Rail Corridor	14
5.4.2 Maintenance Access Track	15
5.4.3 Basin B1 at CBH Storage Expansion Area	15
5.4.4 Turnout Drainage	16
5.4.5 Asphalt Track	16
5.4.6 CBH Internal Site Drainage	17
5.5 Comparison: Pre-development and Post-development	17
5.6 Flood Analysis	21
5.7 Hydraulic Modelling Results	21
5.7.1 Design Culverts	21
5.7.2 Design Open Drains	22
5.7.3 Design Basin (Basin B1)	25
5.8 Scour Protection	26
6. Outstanding Issues, Risk & Opportunity	28
6.1 Outstanding Issues	28
6.2 Risk	28
6.3 Opportunity	28
7. Appendices	29
Appendix A – Catchment Plans	30
Appendix B – DRAINS Model Results	31
Appendix C – Pipe Class Calculation	32
Appendix D – Design Storm Intensities	33

1. Executive Summary

BG&E was awarded the Perenjori Rail and Civil Design package on the 17.01.2023. Works proceeded in accordance with

- CBH Scope of Work document 311-3066-MD-SOW-0001
- CBH TS10A Design Specification – Civil Earthworks, Roads, and Drainage
- ARC Basis of Design document INV20008 NG Rev 0.00 and
- ARC Narrow Gauge Code of Practice document W190-400-002 NG Rev 2.01

It is intended for this Drainage Report to be progressively updated at each design stage until IFC and be issued as a final

1. CBH Design Report and
2. Arc Permanent Way Report

This report is to be read in conjunction with Perenjori Rail & Civil Design Report- CBH 311-3066-PM-RPT-0001.



2. Standards

2.1 Introduction

Design standards used in the development of the drainage design are provided in **Error! Reference source not found..**

Table 2-1 Standards and Specifications

Document Number	Name	Revision
INV20008-NG	Basis of Design and Survey – Track, Civil & Structures ASCI Narrow Gauge CBH Sidings	0.00
CBH-ENG-CI-SST-0001	TS 10A Design Specification Civil Earthworks, Roads and Drainage	3
MRWA Supplement	MRWA Supplement to Austroads Guide to Road Design – Part 5, Part 5A, Part 5B	5 (2021) 5A & 5B (2020)
MRWA Culverts	MRWA Culverts General Standards	2019
AGRD5	Austroads Guide to Road Design Part 5 Drainage – General and Hydrology Considerations	2023
AGRD5A	Austroads Guide to Road Design Part 5A Drainage: Road Surface, Networks, Basins and Subsurface	2023
AGRD5B	Austroads Guide to Road Design Part 5B Drainage – Open Channels, Culverts, and Floodway Crossings	2023
Australian Rainfall and Runoff	ARR Guidelines & ARR Data Hub	2019
AS 7637	Hydrology and Hydraulic	2014
-	Stormwater Management Manual of Western Australia	2023

3. Design Criteria

3.1 Introduction

Key design criteria used to develop the overall drainage design is based on INV20008-NG 'Basis of Design and Survey – Track, Civil & Structures ASCI Narrow Gauge CBH Sidings' for assets owned and operated by ARC Rail and TS 10A 'Design Specification Civil Earthworks, Roads, and Drainage – CBH-ENG-CI-SST-0001' for assets owned and operated by CBH. The parameters used in the DRAINS software in developing the hydrological model have also been included. Review of AS 7637 found that INV20008-NG was closely aligned and no additional requirements from AS 7637 have been determined.

Table 3-1 ARC Design Criteria

Item	Description	Reference
CROSS DRAINAGE/CULVERT		
Pipe Material	Reinforced concrete (RCP/RCB)	INV20008-NG 5.2
Minimum Pipe Class	Under track siding and access road: Class 4 Under mainline track: Class 6 Under road: Class 4	INV20008-NG 5.2
Min Size	450mm DIA (RCP) 600mm high x 1200mm wide (RCB)	INV20008-NG 5.2/5.3
Min Cover	600mm	INV20008-NG 5.2-5.4
Minimum Freeboard	300mm from underside of shoulder of formation for 2% AEP 500mm below top of rail for 1% AEP	INV20008-NG 5.1
Design requirement	Inlet control – Max headwater to be 1.5 x culvert diameter/height	INV20008-NG 5.2
Culvert Alignment	Culverts shall be perpendicular to track centreline	INV20008-NG 5.2
Rock Protection Design	To be provided for flow velocities higher than 1 m/s as per AGRD Part 5B.	INV20008-NG 5.2
Rock Protection Design (Adopted)	Provide rock protection in accordance with the Floodway Design Guide	MRWA Supplement to Austroads Guide to Road Design – Part 5B MRWA Drawing No. 200131-0061-8
Self-Cleansing Velocity	0.7 m/s for loose clay soil 0.9 m/s for larger grain sizes	MRWA Culverts General Design Procedure Standards 2.10
General	Where there is existing Arc's track besides proposed track alignments then culverts between two tracks shall be aligned. Serviceability of existing culverts and track from flooding point of view is not compromised due to proposed development and construction of new track besides existing tracks	INV20008-NG 5.1
Existing culvert serviceability	Hydrology assessment should determine existing service level of above culverts in terms of ARI/AEP and confirm that proposed new track besides existing track is provided with suitable drainage structures in alignment with existing and new culverts will be suitable size so that the serviceability of existing culverts is not affected	INV20008-NG 5.1
General	ARI/AEP of culverts under new track can be higher (i.e. 2% AEP minimum) than existing, provided no negative impact on existing culverts occurs.	INV20008-NG 5.1

	Culvert sizing for both 1 in 20-year ARI (5% AEP) event and 2% AEP event with and without 300mm freeboard.	
LONGITUDINAL DRAINAGE		
Design requirement	2% AEP Where a 2% AEP event cannot be accommodated in longitudinal drainage then 5% AEP event should be considered for the design. However, for a 5% AEP event designer shall demonstrate to Arc that no negative impact on track and other assets will occur due to design of longitudinal drainage for 5% AEP event and how longitudinal drainage system will work for 2% AEP and 1% AEP events.	INV20008-NG 5.1
Longitudinal Drainage requirement	Open Drains within Arc corridor	INV20008-NG 5.1
Minimum Freeboard (open drain)	300mm below shoulder of formation (below top of sub-ballast capping layer for 2% AEP 500mm below top of rail for 1% AEP	INV20008-NG 5.1
Min Depth	300mm	INV20008-NG 5.1
Min Grade	1:300 (0.3%)	INV20008-NG 5.1
Side slope	Not exceeding 1v:2h	INV20008-NG 8.2
HGL Design (Sealed pit and pipe)	2% AEP Water level below the top of the sub-ballast capping layer	INV20008-NG 5.1
Cut-off drains	Must be provided at the top of all cuttings to prevent run-off entering the cutting	INV20008-NG 5.1
Other design requirement	Cut-off drains, cutting drains, diversion drains and stream training: 50yr ARI (2% AEP) All signalling equipment cabinets and similar: 300mm freeboard for 100yr ARI (1% AEP)	INV20008-NG 5.1
Other	Invert level for track drainage must be below the formation toe. Water shall not pond within 2m of the formation toe.	INV20008-NG 5.1
BASIN		
Location	Minimum distance of 10m from the corridor boundary No stormwater retention or drainage basin is permitted on Corridor	INV20008-NG 5.1
Design requirement	5% AEP	INV20008-NG 5.1
Max Depth	1.5m	MRWA Supplement to Austroads Guide to Road Design – Part 5A
Batter Slope	Detention Basin Above invert of basin outlet: no steeper than 6:1 Below invert of basin outlet: no steeper than 3:1 Retention Basin No steeper than 3:1	MRWA Supplement to Austroads Guide to Road Design – Part 5A
Fencing Requirement	Provide fencing around basins with batter slopes steeper than 6:1	MRWA Supplement to Austroads Guide to Road Design – Part 5A
Infiltration	Variable rates. Use as indicated in the <i>Geotechnical Investigation – Parenjori Receiving Facility</i> Report Section 9.11	PS133598-GEO-REP-00003-0-Perenjori 9.11
Groundwater	The base level of the basin should be above the Maximum Groundwater Level	MRWA Supplement to Austroads Guide to Road Design – Part 5A

MAINTENANCE ACCESS TRACK		
Minimum Grade for Swales	0.3%	INV20008-NG 10.1
Minimum Side Slope for Swales	1V:3H	INV20008-NG 10.1
Culverts and drains storm event	20% AEP (4.48 ARI)	TQ PE22021.01 / #014
Freeboard	200mm from top of pavement	TQ PE22021.01 / #014

Table 3-2 CBH Road Design Criteria

Item	Description	Reference
Storm event to capture and transfer stormwater through open drains and culverts	1:20 year ARI (5% AEP)	CBH-ENG-CI-SST-0001 Clause 6.1
Detention storage and controlled overflow pathway discharge	1:20 year ARI (5% AEP) - or local authority requirements, whichever is greater	CBH-ENG-CI-SST-0001 Clause 6.1
Freeboard to top of subgrade	300mm	CBH-ENG-CI-SST-0001 Clause 6.1
Min thickness CSP	2.0mm	CBH-ENG-CI-SST-0001 Clause 6.1
Scour protection	>2m/s in unlined drains	CBH-ENG-CI-SST-0001 Clause 6.1
Min depth for open channels	0.4m	CBH-ENG-CI-SST-0001 Clause 6.3
Min grade for open channels	0.3%	CBH-ENG-CI-SST-0001 Clause 6.3
Open Drain side slopes max	1V:3H	CBH-ENG-CI-SST-0001 Clause 6.3
Cover over culverts	RCP: 600mm RCB: 450mm	CBH-ENG-CI-SST-0001 Clause 6.4

Table 3-3 DRAINS Design Parameters

DRAINS Parameters (Software Parameters)	
Hydrology Method	Horton (ILSAX)
Rainfall data	Downloaded from the ARR Data Hub and BOM website
Paved (impervious) area depression storage	1mm
Supplementary area depression storage	1mm
Grassed (pervious) area depression storage	5mm
Soil type	3
Antecedent Moisture Condition (AMC)	3
Pre-burst rainfall depth	0
Climate Change rainfall multiplier	1.0
Design Method (long drainage hydraulics)	Manning's
Pipe roughness coefficient	0.013

4. Deviations from Design Criteria

4.1 Introduction

The following deviations from Design Criteria are noted in Table 4-1 below.

Table 4-1 Deviations from ARC Design Criteria

Source	Requirement	Deviation	Justification / Comment
INV20008-NG Section 5.2	Culverts shall be designed to operate under inlet control	The proposed 2x450DIA culvert C6 at CH 298,110 is operating at outlet control due to the tailwater condition brought by the downstream proposed culvert C7 and the large catchment from the channels contributing to the downstream discharge point.	Culvert is required to capture flows between the siding and mainline. The proposed culvert is still meeting the minimum freeboard requirements and headwater condition.
INV20008-NG Section 5.1	Water levels in open drainage systems shall have 300mm freeboard below shoulder of formation (below top of sub-ballast capping layer) for the 2% AEP event.	Proposed swale between CH 298,523 to 298,600 not meeting the 2% AEP freeboard	The freeboard only lacks at the upstream due to the level of the formation being lower at the maintenance/construction pad area. Proposed drain develops depth further downstream and provide greater freeboard. Water depth at the upstream is expected to drain quickly due to sufficient grade and drain sizes provided downstream
INV20008-NG Section 5.1	Water levels in open drainage systems shall have 300mm freeboard below shoulder of formation (below top of sub-ballast capping layer) for the 2% AEP event.	Proposed drain formed in between the turnouts are not meeting the freeboard at the upstream interface	The freeboard only lacks at the upstream interface of the mainline and siding formation. Proposed drains develop depth further downstream and provide greater freeboard. Upstream section of the drains is also expecting small catchment and is expected to drain quickly due to steep grade provided.
INV20008-NG Section 5.1	All cross drainage structure structures – Culverts to be designed for serviceability of 2% AEP with a freeboard of minimum 300mm from underside of shoulder of formation	Culvert C7 has a freeboard of 588mm against the proposed siding shoulder of formation, resulting to 288mm freeboard from the underside of shoulder of formation	Culvert C7 runs below the maintenance access with the headwater not meeting the west shoulder of formation level of the proposed siding. The increased headwater is brought by the head loss due to constricted flow at the channel upstream. Due to spatial constraint, the channel size upstream is limited and cannot be increased. However, the headwater and incoming channels to culvert C7 meet the 2% AEP freeboard requirement of 300mm below shoulder of formation and the 500mm freeboard from the top of rail for 1% AEP
INV20008-NG Section 10.1 (5d)	Swale drains side slopes of minimum 1v:3h adjacent to access track	Swale drains near the maintenance access track have 1V:2H side slopes between CH 298,540 to 298,560	Spatial constraints restrict the access track batters to 1V:2H at the given section. Beyond this, the 1V:3H is met.

		CH 298,333 to 298,400 CH 298,050 to 298,135 CH 297,700 to 297,831 CH 297,478 to 297,500	
INV20008-NG Section 5.2	Rip rap rock protections or rock mattresses per Austroads Waterway Design Guide shall be provided at culvert outlets where velocity excess 1.0 m/s	MRWA rock protection has been adopted for all culvert outlets	Headwall details have been adopted from MRWA standards and rock protection to match. MRWA guidelines for headwall detailing and rock protection provide an increase level of scour protection compared to Austroads.
INV20008-NG Section 5.1	Cut-off drains must be provided at the top of all cuttings to prevent run-off entering the cutting	No cut-off drains proposed	No external run-off is expected at the cutting section. The current proposed channels running at the bottom of formation have sufficient capacity to cater the expected flows.
MRWA Culverts General Design Procedure Standards 2.10	0.7 m/s minimum self-cleansing velocity	Proposed 2x450DIA culvert C6 at CH 298,110 has a design velocity of 0.4 m/s at 2% AEP.	Culvert is operating at outlet control with incoming flows at the downstream larger than the upstream thus generating low velocity. The culvert has been provided with 2.47% grade to facilitate higher velocity in low flow events.
INV20008-NG Section 5.1	Hydraulic design modelling software to be accepted by the ARC representative	No formal acceptance has been received	Model software and parameters are outlined in Section 3. DRAINS is considered an appropriate industry approved software – Refer AS 7637 Appendix D
INV20008-NG Section 5.1	Include plan & profile and typical section drawings showing flood levels (1EY, 1% AEP, 2% AEP, 5% AEP) with reference to top of subballast capping and base of subballast capping and culvert headwalls	Plan profiles not included. 1EY and 5% AEP storms are not included	1EY and 5% AEP storms are not requirements from the ARC BoD. Plan & profile information is tabulated. Refer TQ 017 for approval.
INV20008-NG Section 5.1	Stormwater runoff originating outside the corridor is not permitted to drain onto the rail corridor	Flows from outside the rail corridor enter the rail corridor	Existing flows enter the rail corridor. Due to existing topography and site constraints flows cannot be directed away from the corridor.

Table 4-2 Deviations from CBH Design Criteria

Source	Requirement	Deviation	Justification / Comment
CBH-ENG-CI-SST-0001 Section 6.1	Manage, control and convey the design ARI post development event by applying a freeboard of 300mm from the top of the subgrade layer (5% AEP)	<p>5% AEP freeboard of the following culverts are below the minimum requirement. Note that the freeboards provided are measured from the edge of road due to unknown subgrade level of existing road. Subgrade level assumed is at 330mm depth</p> <ul style="list-style-type: none"> EX400 Culvert F - 270mm EX300 Culvert I – 290mm 	<p>Existing condition is already overtopping at 5% AEP (-50mm freeboard at existing condition). Post-development, the design has significantly improved the hydraulic performance of the culvert.</p> <p>Existing condition is not meeting the freeboard at 5% AEP (110mm freeboard at existing condition). Post-development, the design has significantly improved the hydraulic performance of the culvert.</p>
CBH-ENG-CI-SST-0001 Section 6.1	Manage, control and convey the design ARI post development event by applying a freeboard of 300mm from the top of the subgrade layer (5% AEP)	<p>5% AEP freeboard of the following channels are below the minimum requirement. Note that the freeboards provided are measured from the edge of road due to unknown subgrade level of existing road. Subgrade level assumed is at 330mm depth</p> <ul style="list-style-type: none"> CHNL E30 – 380mm CHNL E82 – 500mm CHNL D17 - 349mm CHNL D18 – 349mm 	<p>Existing condition is not meeting the freeboard at 5% AEP (260mm freeboard at existing condition). The design scenario has improved the freeboard.</p> <p>Existing condition is not meeting the freeboard at 5% AEP (300mm freeboard at existing condition). The design scenario has improved the freeboard.</p> <p>Proposed channels are reconstruction of existing channels. The design scenario has improved the freeboard.</p>
CBH-ENG-CI-SST-0001 Section 6.3	Open channels shall be a minimum 0.4m	<p>Minimum depth of the following channels are below 0.4m</p> <ul style="list-style-type: none"> CHNL D13 – CH 297,461-297,494 CHNL D14 – CH 297,494-297,521 CHNL D28 – Basin Outlet 	<p>Channels are provided with smaller depth at the upstream to provide steeper grade and reduce cut on existing surface. Depth requirement is met further downstream of the channels.</p> <p>The channel is intended only to daylight the culvert outlet of the basin to the existing surface thus not requiring large depth.</p>

5. Drainage Design

5.1 Introduction

The proposed drainage for Perenjori is designed to minimize impact to the existing drainage systems and maintain the existing flow regime. The development of the drainage design for the 100% design phase considered number of options in accordance with the design standards listed in Table 2-1. The design was assessed with checking the following:

- Catchments check on pre-development and post-development scenarios
- Channels and pipe capacities
- Minimum pipe covers
- Minimum freeboard requirements
- Velocity and scour protection requirements
- Storage capacities
- Tie-in to existing drainage

The drainage assessment for this project has been done using DRAINS modelling software. DRAINS models were created to reflect both pre-development and post-development scenarios. Catchments were delineated using 12d with the existing survey information received. Flows for the catchments were estimated using run off routing method (ILSAX model) from ARR2019 ensemble runs. The ensembles and rainfall data were downloaded from the ARR Data Hub and BOM website, respectively. The existing condition and proposed drainage arrangements are discussed in detail in the following sections. Existing drainage conditions have been utilised where possible to ensure minimal impact.

Refer Appendix A for DRAINS model output

5.2 Assumptions

The following assumptions and design parameters have been used in undertaking the drainage design for the project:

- It has been assumed that the mainline formation, siding formation, and existing road and maintenance access are impervious (100% impervious). Areas outside of this has been assumed pervious (5% impervious – 95% pervious).
- A conservative approach has been done where the existing and proposed open drains have been assumed to be impermeable. All flows are conveyed and no losses from infiltration on the channels have been accounted for.
- Free flow conditions have been adopted at all model outlets within the DRAINS model. However, any existing condition downstream of the proposed drainage has been modelled where possible to reflect the actual downstream condition and their impact to the design.
- Existing drainage has been modelled based on the survey received from client and is assumed to be replicating the realistic drainage site conditions.
- It has been assumed that the latest survey provided by client represents the top of formation for existing rail infrastructure.

5.3 Existing Condition

The existing drainage has been modelled and analysed based on the latest survey model received. The survey information has been used to assess the pre-development scenario and check the impact of the post-development.

Rail Corridor and Wubin-Mullewa Road

The existing site drainage is generally comprised of open channels and culverts under both ARC and CBH rail or roads. The drainage features of the existing mainline are composed of slight or informal channels running along the existing mainline, however in many cases channels are not well defined and no formal drainage appears to be

present. There are existing culverts running below the existing mainline and Wubin - Mullewa Road adjacent to the proposed siding as listed in the Table 5-1 below.

Table 5-1 Existing Culverts Adjacent to Siding

Chainage approx. (along proposed siding)	Culvert configuration	Location
297,390	300DIA	Wubin - Mullewa Road
297,450	2 x 450DIA	ARC Mainline
298,110	2 x 300DIA	ARC Mainline
298,350	300DIA	Wubin - Mullewa Road

The site area is relatively flat and results in several localised trapped points within the corridor. The existing channels running along the existing mainline do not conform to the typical ARC standards and are misaligned in some areas. Apart from the channels, it appears some sections develop surface creases – some of which are running directly adjacent to or along the existing maintenance access track. Local trapped points are present at the rail corridor with some trapped points located at the toe of the existing mainline formation.

The catchments of the site are characterised by catchments within the rail corridor and incoming external catchments from adjacent rural properties. The existing topography results in runoff entering the rail corridor from the west between CH 297,980 and 298,580. These flows are then conveyed within the informal corridor drainage to the west of the mainline to existing underperforming culverts. This results in backwater conditions and the movement of flows to the south towards the trapped low area discussed below.

Post 85% design stage, the contributing catchment along the rail corridor has increased as result of the flood analysis. It has been assessed that the existing rail culverts upstream of the site area have limited capacity. This results to the rail and external catchment runoff overtopping the rail and laterally flowing along the rail embankment. These lateral flows from the upstream breaks out to the site area. The breakout flow has increased the considered flows in analysing the existing condition. The contributing breakout flow towards the trapped low area is discussed below.

Similarly, existing external catchments enter the road reserve of Wubin - Mullewa Road from the east between CH 297,400 and 297,875. Due to a lack of transverse drainage these flows are conveyed to the south and bypass the area of design.

Generally, the CBH site drains away from the rail corridor. Between CH 297,460 and 297,980 the CBH site fall away from the rail corridor. South of CH 297,460 the CBH access road located adjacent to the rail corridor boundary drains across the rail corridor boundary.

Within the rail corridor the topography is typically quite flat, with a slight grade from north to south which is more pronounced adjacent to the CBH site. Dual 300DIA culverts are present at CH 298,110 and are discussed further with the adjacent trapped area in the section below. Dual 450DIA culverts convey flows from the north between the mainline and Wubin - Mullewa Road across the mainline alignment to the west at CH 297,450. The channel downstream of this culvert is not well defined; however flows move to the south between the mainline and maintenance track.

Trapped Area at CH 297,950 – 298,270

The trapped area between CH 297,950 – CH 298,270 indicates a basin-like behaviour due to the level of the existing 2x300DIA culvert at CH 298,110.000. The US invert of the existing culvert is 208mm above the lowest point of the trapped area. This is causing 89.7 m³ retention in volume. As the volume increases, the incoming flows outlet to the existing culvert. However, at higher rainfall events, the flow to the culvert is limited and an overflow occurs at the overtopping level at the maintenance access. The overflow runs along the existing crease towards the south to the CBH Storage Expansion Area.

At the downstream of the culvert, the trapped area between the Wubin - Mullewa Road and ARC mainline generates 356.9 m³ of volume retention. Based on the survey, no existing culvert is located at Wubin - Mullewa Road to

discharge this trapped area. At higher rainfall events, the flow overtops the Wubin - Mullewa Road towards the north to the rural land.

During 5% AEP rainfall event, the overflow from the upstream trapped area going to the CBH Storage Expansion Area is at 0.440 m³/s while the overflow from the downstream trapped area towards the north rural land is at 0.320 m³/s. In the 1% AEP major event, the overflow to the CBH Storage Expansion Area is at 1.350 m³/s while the overflow to the north rural land is 1.600 m³/s. Refer Figure 5.1 for more details on the trapped areas.

The overflow towards the CBH Storage Expansion Area has increased compared to the 85% design stage due to breakout flow from the west upstream catchments. The rail and external catchments upstream have been analysed and modelled to determine the expected breakout flow. The incoming breakout flow during the 5% AEP is at 0.48 m³/s while in the 1% AEP major event, the breakout flow is at 1.49 m³/s.

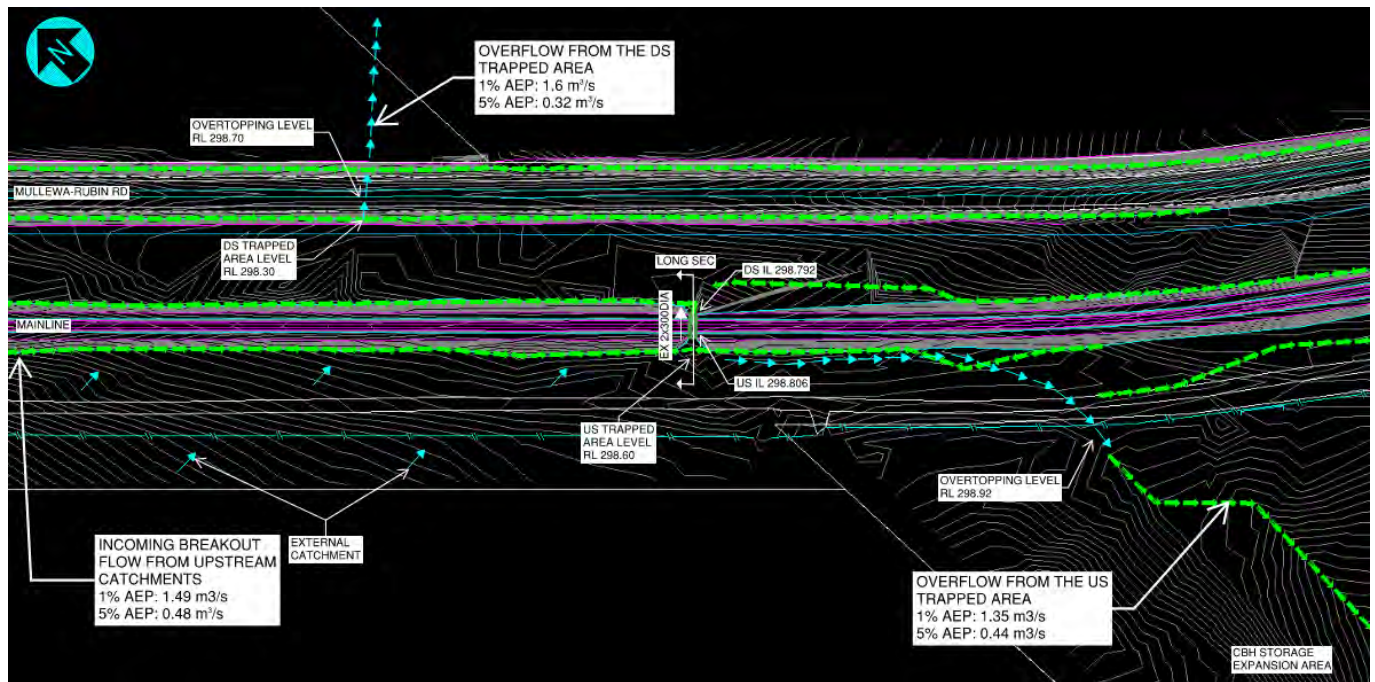


Figure 5.1 Existing Trapped Area at Rail Corridor

In the analysis to determine the overflow, the trapped areas have been assumed as impermeable due to the relatively shallow depths of rock granite at the area. Refer to PS133598-GEO-REP-00003-0-Perenjori Geotechnical Investigation Report for details of infiltration. A catchment plan outlining the existing drainage regime can be found in Appendix B.

CBH Internal Site Southern Drainage

The existing drainage south of the CBH site is comprised of channels and culverts running under the CBH access roads. The channels and culverts are mainly capturing catchments coming from the storage expansion area, open bulkheads, and access roads. Grading of the existing site directs these flows to the southern edge of the site. All these discharge to the existing basin at the south. Refer Figure 5.2 for more details on the CBH south drainage.



Figure 5.2 CBH Site South Drainage

Pre-development scenario model

The pre-development scenario has been modelled in DRAINS to assess the current condition. It has been assumed that the mainline formation, existing road and maintenance access are impervious while the external rural land catchments have been assumed pervious. To mimic the actual external flow, the kinematic wave equation has been used to calculate the overland flow time. In addition to the overland flow, the time of flow along the channel has been considered and calculated using Manning's equation with level, grades and depths based on the latest survey.

Based on the assessment of the existing drainage it appears that the existing drainage within the site of interest does not meet the ARC criteria. The 2% AEP freeboard of the existing 2x450DIA culvert at CH 297,450 and 2x300DIA at CH 298,110 along the mainline are not meeting the minimum requirement per ARC criteria. These culverts are within the site of interest affected by the proposed works. However, both existing culverts are meeting the freeboard requirement for 1% AEP below the rail.

Similarly, some of the existing drainage within CBH infrastructure is not meeting the CBH design specification. The existing 300DIA Culvert I at the CBH south internal site, as shown in Figure 5.2, is not meeting the minimum freeboard of 300mm from the top of the subgrade layer of the access road at 5% AEP. The adjacent existing 400DIA Culvert J overtops during a 5% AEP. The existing 400DIA Culvert F at the east access road, as shown in Figure 5.3 overtops during 5% AEP at existing condition. Further details on the pre-development analysis results will be discussed in Section 5.5.

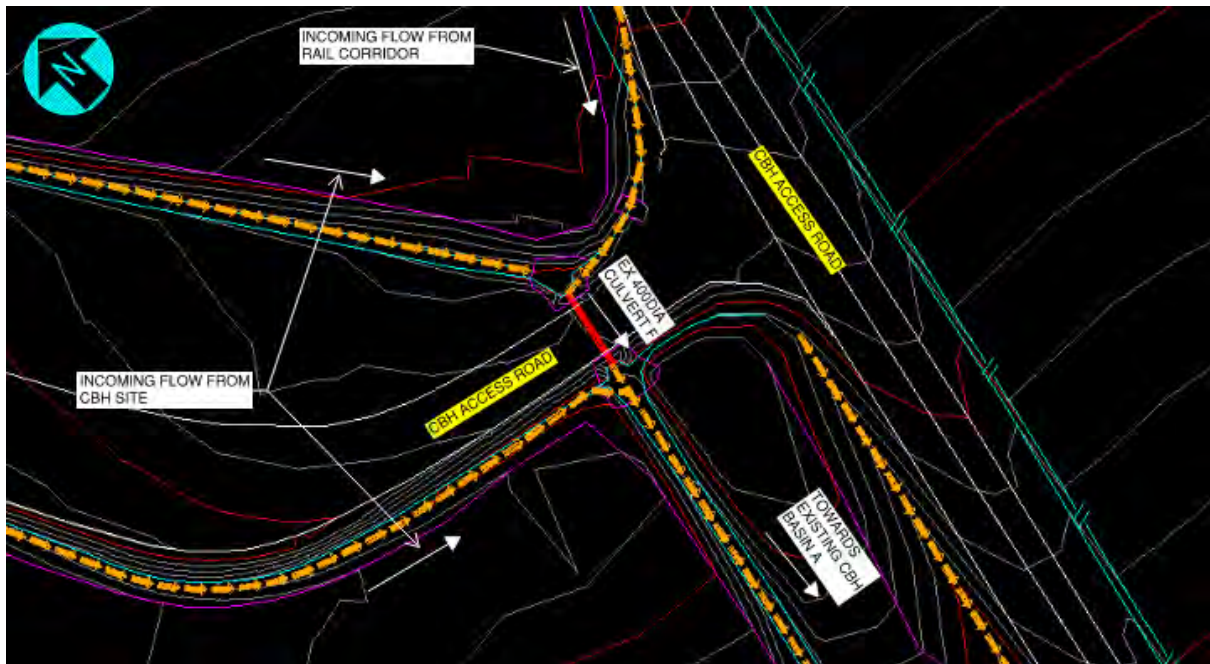


Figure 5.3 CBH East Culvert

It should be noted as well that the existing culverts at Wubin - Mullewa Road at CH 297,390 and CH 298,350 in existing condition do not have sufficient freeboard and are expected to overtop the existing road at 2% AEP.

5.4 Design Scenario

The proposed drainage in Perenjori aims to maintain the existing flow regimes and minimize any major impact in the existing flow patterns brought by the proposed works. The proposed siding impacts several existing open drains and catch drains running along the western side of the existing mainline. The siding design provides open channels on both sides of the siding where possible. The proposed siding requires new culverts aligned with the existing culverts to maintain flow patterns from existing culverts and avoid concentrated flows running along between the existing mainline and proposed siding.

Longitudinal drainage has been proposed as open drains (channels) within the rail corridor. The channels have been sized as v drain and trapezoidal depending on the expected flows, freeboard requirements, and considering space constraints. The depths are designed to meet capacities and the specified freeboard requirements depending on the location per the design criteria in Tables 3-1 and 3-2. However, some sections along the drain may provide more depth and may vary depending on site constraints. The grade of the channels has been proposed to be at the same grade of the rail alignment where possible. However, on cases where the rail alignment grade is below the minimum requirement, the channels were proposed to be independently graded with minimum 0.3% grade. Other channels have been independently graded steeper than the rail alignment grade to provide depths to culverts and grade suitably with the existing surface.

New culverts have been proposed at the proposed siding to align with existing mainline culverts. This is to minimise hydraulic impact brought by the proposed siding and to maintain existing flow pattern. A minimum diameter of 450mm has been adopted for all proposed culverts under track as per the basis of design. Appropriate pipe class of the new culverts have been provided as well based on the expected loading and considering the minimum per basis of design. Pipe class calculations have been provided in Appendix C.

This section summarises the proposed drainage design across the site. An assessment and comparison of the existing and design flows are discussed in Section 5.5. The receiving basins at the CBH site has been analysed for pre-development and post-development. A summary is discussed in Section 5.5.

5.4.1 Rail Corridor

The proposed siding will be removing the existing channels between the existing mainline and maintenance access track. New channels have been proposed to run on both sides of the proposed siding in lieu of the existing channels. The channels will provide smooth flow and will remove any potential ponding between the existing surface and proposed siding. Channels flow is typically from north to south, drains are only reverse-graded to convey flows towards culverts.

A new 2x450DIA RCP culvert has been proposed at the new siding downstream of the existing 2x450DIA culvert at CH 297,450. This proposed culvert will convey the incoming flow from the existing culvert and flows from the proposed channels between the proposed siding and existing mainline to the north. This culvert will discharge to a channel between the new siding and maintenance access and will tie-in to the existing channel downstream to the south. This will match the existing condition where the existing culvert is discharging to the existing channel between the maintenance access and existing mainline.

Channels have been proposed between the proposed siding and maintenance access. These new channels will be carrying over the external catchment from the south-west rural land.

Trapped Area at CH 297,950 – 298,270

The proposed siding alignment reduces the existing storage available at the trapped low point adjacent to the dual existing 300DIA culverts. The design has considered how to maintain existing flow regimes and meet the design criteria for the siding while not negatively impacting the mainline drainage.

The design approach has undergone an optioneering process with CBH input to consider potential solutions. A drainage workshop with CBH was held on 9 June 2023 for the assessment of this area. Two options were considered:

1. No proposed culvert at the proposed siding. The existing 2x300DIA culvert function will be maintained.
2. A 2x450DIA culvert is proposed to convey flows from between the mainline and siding to the west.

Option 1 will direct the flows between the proposed siding and existing mainline to the existing 2x300DIA culvert. No culvert is proposed below the siding. Option 1 has 3 sub options:

- A. Do nothing. The 208mm trapped depth between the siding and mainline will be maintained. However, this now has a reduced storage due to the footprint taken by the proposed siding. The ponding depth will not be able to meet the freeboard requirement and will not meet the ARC specification of no ponding at the rail corridor.
- B. Provide infill between the siding and mainline to match the US invert of the culvert. Infill to be provided with minimum 0.3% grade from the upstream to the culvert. This will remove the remaining trapped area between the siding and mainline. However, the infill reduced the depth between the mainline and siding thus not achieving minimum freeboard requirement.
- C. Provide local infill nearby the culvert to match the US invert. Infill to be flat at the downstream to provide depth at critical sections. The freeboard requirement will not be met due reduced storage and depth.

Option 2 is to propose a new culvert under the proposed siding to convey flows trapped between the siding and mainline to the west. Flows to the west of the siding will then be conveyed via an open channel to the CBH site for attenuation due to the increase of flows. This option makes the existing 300DIA culverts effectively redundant.

Option 2 was adopted as it eliminates any chance of negative impacts to the mainline drainage and effectively improves the mainline drainage to some degree. This will also remove the potential water ponding from the residual trapped points between the proposed siding and existing mainline at CH 297,950 – CH 298,270.

It was observed that the two existing culverts below the mainline are not meeting the minimum pipe cover of 600mm as stated by Basis of Design. No pipe protection structure has been captured in the recent survey. However, since no works are expected at these existing culverts and no reduction on the mainline formation, the existing condition will be retained. An on-site assessment of the existing pipe class and condition is highly recommended.

Diversion and attenuation with the CBH site will be further discussed in Sections 5.4.2 and 5.4.3.



5.4.2 Maintenance Access Track

The maintenance access track is typically graded towards the rail, with runoff being collected and conveyed by the open drain located between the MAT and siding. Channel slopes adjacent to the MAT have typically been kept at 1:3 or shallower, however due to special constraints in some locations, steeper slopes are required.

As discussed in section 5.4.1, the existing trapped area at CH 297,950 – CH 298,270 will be removed due to the new siding. Proposed channels are graded towards a sag at CH 298,064 where a 5x525DIA RCP culvert under the MAT has been proposed. This proposed culvert is downstream of the proposed 2x450DIA RCP culvert under the siding at CH 298,110.

The 5x525DIA culvert is vital to divert the external catchment flows and the upstream breakout flows outside the site away from the rail corridor. This culvert will outlet to a proposed channel towards the CBH Storage Expansion Area. Due to this diversion, an increase of flow towards the CBH Storage Expansion Area is expected compared to the pre-development condition.

5.4.3 Basin B1 at CBH Storage Expansion Area

A detention basin has been proposed at the CBH Storage Expansion Area with the aim of attenuating peak discharge flow towards the CBH site and match the pre-development flow condition. The basin will be attenuating the flow coming from the rail corridor as highlighted in section 5.4.2. The proposed channel from the maintenance access will be discharging to the basin and tie-in to the basin bed level. Properties of the proposed basin are highlighted in the Table 5-2 below.

In the Perenjori Geotechnical Investigation report, it was observed that rock was encountered at shallow depths relatively near the rail corridor. Due to shallow rock level, infiltration is not an ideal discharge method. Thus, a detention basin has been proposed rather than a retention basin. The basin bed level has been set to require minimal cut on the rock layer. The incoming channel and basin have been positioned to align with the existing surface crease line to reduce the volume of cut required.

Table 5-2 Basin Parameters

Detention Basin Parameter	Value
Base of Basin RL	296.70 mAHD
Top of Basin RL	298.20 mAHD
Base Area	4130 m ²
Side Slope	1V:6H
Weir Shape	Trapezoidal
Overflow Weir RL	297.60 mAHD
Weir Length	14m
Weir Depth	0.6m
Weir Side Slope	1V:6H

The basin has been sized to attenuate flows and mitigate any impacts to the downstream CBH site. To match the 5% AEP pre-development flow of 0.44 m³/s, an outlet control pipe has been proposed. The outlet control pipe has been initially designed to 525DIA RCP since it will be controlling the pipe discharge just within the pre-development flow. However, even though the pipe will convey a flow below the pre-development flow, the 525DIA will be conveying a significant concentrated flow and has a potential risk of increasing scouring at the downstream. Also, during rainfall events smaller than 5% AEP, the flows will not be attenuated due to the large size of the pipe. This will make the basin less effective during smaller rainfall events.

A drainage discussion with CBH was held on 7 December 2023 to agree on decreasing the outlet control pipe to 300DIA to effectively attenuate the 5% AEP flows. The reduction of the size of the outlet pipe will reduce any risk

downstream due to a more controlled flow. The agreement to decrease the outlet pipe and the consideration of the additional lateral breakout flows at the rail corridor as discussed in section 5.3 have resulted to increase of the size of the basin and weir.

For events larger than a 5% AEP, the proposed weir will be activated. The basin has been designed to have the combined flow at the weir and pipe match the pre-development major flows in the 1% AEP of 1.35 m³/s. The water level at 1% AEP at the basin will provide a 303mm of freeboard from the top of the basin.

It should be noted that the analysis done with the basin is not isolated. The effect of the basin water levels to the upstream drainage at the rail corridor has been considered. At 1% AEP, the water depth at the incoming channel will not overtop and will still be contained within the channel. Proposed basin hydraulic results and comparison with pre-development flow will be discussed on section 5.7.3.

Scour protection has been proposed at the weir lining and adjacent batter to prevent scouring during the high flow events. Scour protection has also been proposed at the incoming channel to prevent scouring at the basin bed. The 300DIA RCP low flow pipe has been designed with a channel outlet to daylight to the existing surface. The outlet flow runs off towards the existing channels at the truck loop at the CBH Site and at the southern block.

The top of the basin has been provided with 14m width of maintenance access and driveable batter slopes at the weir.

5.4.4 Turnout Drainage

The drainage provision for the turnout is limited due to the space constraints. Pit and pipe arrangement will not be able to meet the criteria. Ultimately, at both ends of the turnouts, a channel has been proposed at the interface between the formation of the siding and existing mainline. This channel starts as v-drain with the crest tied into the top of formation level. Both then discharge to the downstream drains and to the proposed culverts under the siding. Since the crest upstream of the channel ties into the top of formation level, certain length of the channel upstream does not provide the 2% AEP freeboard requirement. However, the drain develops depth as it ties to the downstream drain. Also, the upstream section of the drain is expecting small catchment only and has been provided with sufficient grade to immediately discharge flows.

Where the siding and mainline are a singular formation (with no drain) the siding is an extension of the mainline's crossfall of 3%. For this short section of the turnout flows from the crowned point of the mainline formation will pass through the siding ballast to discharge to siding drainage.

5.4.5 Asphalt Track

A proposed asphalt track is running through the Garner Bin area. As the asphalt surface will be running at the same level of the rail, surface flows may enter the ballast through the rail head / asphalt interface. To capture these flows, MEGAFLO170 drainage has been proposed at the bottom of the ballast. MEGAFLO170 is a panel drain system which is made of perforated HDPE. Although it may enter the ballast, the expected flows are very minimal as the area is sealed.

Two MEGAFLO170 drainage have been proposed at the asphalt track, one on each side. The pipes will daylight at both ends beyond the asphalt area. The upstream has been provided with an end cap for maintenance access and to avoid any debris entering the pipe. The downstream has been provided with an end outlet and will discharge down the batter of the downstream channel. The grade of the MEGAFLO170 drainage will follow the grade of the asphalt track.



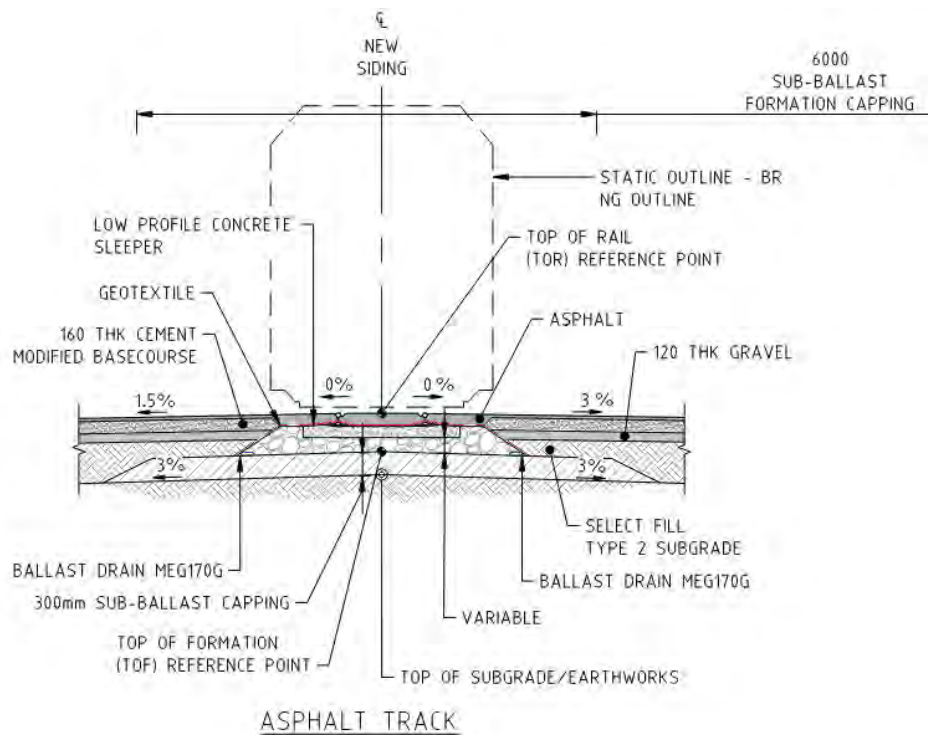


Figure 5.4 Asphalt Track

5.4.6 CBH Internal Site Drainage

As discussed in Section 5.3, the existing culverts at the CBH site are already under capacity and are not meeting the CBH requirements in existing condition.

The existing 300DIA Culvert I receives the additional flows coming from the Garner Bin area. This additional flow, despite being minimal, increases the headwater of the existing Culvert I by 40mm and makes the existing condition worse. To match the existing freeboard levels of Culvert I, a proposed adjustment on the existing drainage has been considered. The three options considered are outlined below:

1. Increase capacity of existing culvert by increasing pipe size from 300DIA to 375DIA or by placing an additional 300DIA pipe (2-barrel culvert);
2. Widening and regrading of the adjacent upstream channels; or
3. Remove in-situ material at the upstream end of the culvert which will act as a basin-like structure.

Option 1 requires reconstruction of the existing culvert and is not a preferred solution. Both options 2 and 3 provide additional storage upstream of existing Culvert I and are more cost-effective and provide less impact to the CBH site. Option 2 has been adopted as it requires less volume cut compared to Option 3. Refer TQ PE22021.01 / #016 for agreement on proposed works.

Impacts to Culvert F from the proposed design were considered negligible and the post- development freeboard at this culvert is expected to match the pre-development freeboard. Comparison of the existing water levels with the proposed work water levels will be discussed in Section 5.5.

5.5 Comparison: Pre-development and Post-development

Existing drainage infrastructure was assessed as part of the drainage design. The pre-development and post-development condition has been modelled and checked separately to study the impacts of the proposed design on

the existing drainage networks. The aim is to meet the same level of serviceability similar to pre-development condition and make sure that the post-development will not negatively impact the existing drainage. In modelling the post-development condition in DRAINS, these existing drainage elements were considered.

Existing Culverts

Given ARC standards specify a minimum of 2% AEP serviceability for their culverts, the 2% AEP and 1% AEP rainfall event results of both the existing and proposed conditions are provided in Table 5-3. The numbers in the table are in the order of existing / design results.

Table 5-3 Existing ARC Culverts Hydraulic Results

ID	Mainline Chainage	1% AEP			2% AEP		
		HWL (mAHD)	Freeboard (m) ¹	Velocity (m/s)	HWL (mAHD)	Freeboard (m) ¹	Velocity (m/s)
EX2x300 CULVERT B	298,110	299.14 / 298.90	0.55 / 0.79	1.49 / 0.02	299.09 / 298.86	0.00 / 0.23	1.24 / 0.02
EX2x450 CULVERT C	297,450	296.32 / 296.18	0.79 / 0.93	1.82 / 1.46	296.27 / 296.16	0.38 / 0.49	1.60 / 1.97
EX2x450 CULVERT E	297,125	293.32 / 293.19	0.73 / 0.86	2.14 / 2.09	293.29 / 293.18	0.35 / 0.46	2.03 / 2.02

1. Freeboard for 2% AEP is measured from the top of shoulder of formation. Freeboard for 1% AEP is measured from the TOR.

As mentioned in section 5.3, the existing culverts lack the required freeboard of 300mm from the underside of the shoulder of formation (600mm from the top of formation level) in the 2% AEP rainfall event based on the ARC standard. The proposed design has negligible impacts on the existing culverts except the existing 2x300DIA Culvert B. As discussed in section 5.4.1, the existing culvert will be redundant due to the diversion of the flow to the proposed 2x450DIA culvert under the siding. The headwater of this existing culvert in the design condition is driven only by the tailwater brought by the trapped area downstream.

Similarly, the CBH culverts were assessed both in pre-development and post-development conditions. The CBH drainage assets require a design event of 1:20 year ARI (5% AEP). The 1% AEP has been included as well to show comparison for a major rainfall event. The results are provided in Table 5.4. The numbers in the table are in the order of existing / design results.

Table 5-4 Existing CBH Culverts Hydraulic Results

ID	Mainline Chainage	5% AEP			1% AEP		
		HWL (mAHD)	Freeboard (m)	Velocity (m/s)	HWL (mAHD)	Freeboard (m)	Velocity (m/s)
EX400 CULVERT F	East Access	290.73 / 290.41	-0.05 / 0.27	2.30 / 2.31	290.81 / 290.49	-0.13 / 0.19	2.39 / 2.40
EX300 CULVERT I	South Access	291.48 / 291.30	0.11 / 0.29	1.92 / 1.73	291.92 / 291.55	-0.33 / 0.04	2.23 / 2.06

The EX400 Culvert F receives flow from the rail corridor and CBH site as shown in Figure 5.3. This culvert, as discussed in section 5.3, is overtopping at 5% AEP. Based on the post-development results, the proposed condition has had significant change on the freeboard.

EX300 Culvert I receives the flow from the land between the storage facilities OBH03 and TBH04 and the sheet flow from the Garner Bin area. As discussed in sections 5.3 and 5.4.7, this culvert is under capacity and is not meeting the CBH requirements in existing condition. Based on the design results, the proposed widening of the adjacent upstream channels has provided more freeboard relative to the existing condition. However, it should be noted the

headwater of the existing culvert at post-development condition is still not meeting the minimum freeboard requirement of CBH specification. The proposed works has been aimed to preserve the freeboard of the existing condition. Refer TQ PE22021.01 / #016 for agreement on proposed works.

Existing Open Drains

The pre-development and post-development condition of existing open drains have been checked as well. These are the existing open drains at the downstream that will be receiving flows from the proposed drains and additional overland flows. These channels have been modelled in DRAINS as close to the profile in the survey as possible. Table 5.5 shows the hydraulic results of the downstream channel between the MAT and mainline at the southern end of the design. This channel is within the rail corridor and has been checked for 2% AEP and 1% AEP rainfall event based on the ARC standards. The numbers in the table are in the order of existing / design results.

Table 5-5 Existing Rail Corridor Channel Hydraulic Results

ID	Location	1% AEP				2% AEP			
		Flow Depth (m)	Freeboard (m)	Velocity (m/s)	Flow (m³/s)	Flow Depth (m)	Freeboard (m)	Velocity (m/s)	Flow (m³/s)
CHNL E17	297,125 – 297,240	0.25 / 0.23	1.37 / 1.39	0.83 / 1.01	0.53 / 0.53	0.21 / 0.21	1.40 / 1.40	0.78 / 0.96	0.41 / 0.41

Based on the results, there are minimal changes between the existing and proposed condition. Both cases still achieve the minimum freeboard based on ARC standards

Table 5.6 shows the hydraulic result for the channels within the CBH access roads. Similar to the CBH culverts, the channels have been assessed for 5% AEP and 1% AEP rainfall event. The numbers in the table are in the order of existing / design results. Note that the freeboard provided is from the edge of the road and not at the subgrade. The subgrade level of the existing CBH road is unknown and has been assumed to be at similar depth to the proposed pavement.

Table 5-6 Existing CBH Access Roads Channel Hydraulic Results

ID	Location	5% AEP			1% AEP		
		Flow Depth (m)	Freeboard (m)	Flow (m³/s)	Flow Depth (m)	Freeboard (m)	Flow (m³/s)
CHNL E30C	East Access	0.33 / 0.21	0.26 / 0.38	0.33 / 0.15	0.41 / 0.31	0.11 / 0.21	0.63 / 0.29
CHNL E82	OBH03-TBH04	0.36 / 0.16	0.30 / 0.50	0.18 / 0.15	0.56 / 0.24	0.1 / 0.42	0.45 / 0.36
CHNL E85	South Access	0.25 / 0.07	0.44 / 0.66	0.05 / 0.02	0.56 / 0.20	0.17 / 0.53	0.10 / 0.04

The freeboards of the channels at existing condition are not meeting the minimum freeboard per CBH criteria. In the design scenario, CHNL 30C, CHNL 82, and CHNL 85 all show a noticeable decrease in flow depth. The reduction in flow depth for CHNL 82 and CHNL 85 is attributed to the widening and regrading of the downstream channels towards EX300DIA Culvert I. This additional storage from reconstruction of channels has improved the freeboard of the existing condition.

Existing Basins

There are two existing basins on the southern side of the CBH site. These basins will be capturing the flows coming from the rail corridor. Figure 5.5 shows the location of the existing basins.



Figure 5.5 Existing CBH Basins

CBH Basin A and CBH Basin B have been modelled and assessed at both pre-development and post-development condition. The aim of this assessment is to check the impact of the proposed condition. Similar to the culverts and channels, the pre-development and post-development conditions of these basins have been modelled separately in DRAINS. The basins profile and storage including the outlets and weir levels have been modelled in DRAINS as provided in the survey model. The storage volume was extracted from a storage calcs in 12d to mirror the best possible information.

This analysis has adopted a simple approach. Both basins have been checked for 5% AEP storage based on the CBH criteria. The catchments of the basins were modelled based on the known catchment boundary. In analysing these basins, no infiltration has been assumed. The basins were assumed to have no retention as evident from the invert levels of the low flow outlet pipes. CBH Basin B was separately modelled from the proposed design model as most of the involved source of catchments will be similar and untouched in the proposed condition.

At the existing condition, CBH Basin A captures flows from the rail corridor coming from the east of the high point, CBH access roads, and the open lot area between the CBH access roads. CBH Basin B consists of catchments from the bin storage areas, internal CBH access roads, and the storage expansion area. The overflow from the trapped area at the existing condition and outlet flow from the basin outlet pipe for the design condition have been included on CBH Basin B.

In the design scenario, Basin A has additional catchments coming from the upstream of the rail corridor while Basin B has the additional flows from the Garner Bin area.

The hydraulic results for 5% AEP rainfall event are shown in Table 5-7. The numbers in the table are in the order of existing / design results.

Table 5-7 Existing CBH Basins Hydraulic Results

Basin ID	Basin Bed Level (m AHD)	5% AEP WL (m AHD)	Overtopping Level (m AHD)	Freeboard (m)
CBH Basin A	286.597	287.386 / 287.390	287.747	0.361 / 0.357
CBH Basin B	287.970	288.800 / 288.800	288.591 (To Weir)	- 0.209 / -0.209 (To Weir)

CBH Basin A shows no significant change in water level at the 5% AEP post-development scenario. The post-development condition continues to meet the 300mm minimum freeboard requirement specified by CBH. Note that the survey information suggests that CBH Basin A doesn't have an overflow weir. The assumed overtopping level is at the lowest point of the top bank of the basin.

CBH Basin B shows no increase in the water level at the post-development condition. The additional flow from the Garner Bin area did a minimal effect on the existing basin. Note that at existing scenario, the weir is already activated at 5% AEP.

5.6 Flood Analysis

A flood assessment has been conducted for the existing Perenjori Rail site. Refer to technical note PE22021.01-MEM-W-001 for more details.

Results of the flood assessment in the baseline (existing) scenario has been considered in the 100% stage drainage design as discussed in section 5.3. A model of rail and external catchments upstream of the site area has been included in DRAINS to capture the resulting and contributing breakout lateral flow to the site.

5.7 Hydraulic Modelling Results

This section contains information on the hydraulic modelling, structures information and analysis results of the proposed drainage structures. The hydraulic analysis has been carried out using DRAINS software. Given results have been exported from DRAINS to satisfy the information required as noted in the basis of the design.

5.7.1 Design Culverts

The hydraulic analysis for culverts has been carried out for 1% and 2% AEP rainfall event and the result are listed in Table 5-8 below. The freeboards for culverts against the rail in a 2% AEP event have also been included.

Table 5-8 Design Culverts Hydraulic Results

Culvert ID	Chainage approx. (along proposed siding)	Culvert configuration	Pipe class	Maximum Flow, 1% AEP (m3/s)	Maximum Flow, 2% AEP (m3/s)	Freeboard to siding, 2% AEP (m) ¹
C1	297,450	2 x 450mm RCP	4	0.340	0.272	0.680
C2	297,573	1 x 450mm RCP	4	0.128	0.106	0.992
C3	297,540	1 x 450mm RCP	4	0.130	0.107	1.114
C5	297,494	1 x 450mm RCP	4	0.095	0.077	N/A
C6	298,110	2 x 450mm RCP	4	0.154	0.110	0.761
C7	298,064	5 x 525mm RCP	4	1.609	1.295	0.588
C8	297,900 (BASIN)	1 x 300mm RCP	2	0.122	0.122	N/A

1. Provided freeboard for 2% AEP was measured from the culvert headwater (upstream HGL) to shoulder of formation. Note that the minimum freeboard for cross drainage structures is 300mm from underside of shoulder of formation.

All proposed culverts are meeting the minimum 300mm freeboard to the underside of the formation in the 2% AEP rainfall event except culvert C7. Culvert C7 runs below the maintenance access. The increased headwater is brought by the head loss due to constricted conveyed flow at the channel upstream. Due to spatial constraint, the channel size upstream is limited and cannot be increased. However, the headwater of the culvert and the incoming channels meet the formation freeboard requirement of 300mm below shoulder of formation at 2% AEP and the 500mm freeboard from the top of rail for 1% AEP.



The class of pipes were determined using PipeClass software. However, due to the limitation of the software to set a fill height less than 1m for rail loading according to standard AS/NZ 3725-2007, the PipeClass software was limited to culverts below road while culverts below rail have been checked with manual structural calculations. For consistency, all rail culverts have been checked using the manual calculation.

Culverts C2 and C3, located under the Garner Bin area, were initially identified as Class 2 in the software results. However, as this area primarily consists of paved roads, walkways, and an asphalt track with significant surface loading, and the entire unit (including two pits and a connecting pipe between C2 and C3) is located beneath the CBH Access Road, the pipe classes for culverts C2 and C3 were increased to Class 4.

It should be noted that for other pipes identified to have lower pipe classes in both the software and manual calculations, the pipe classes were similarly increased to satisfy the minimum requirement specified in the basis of design, as shown in Table 3-1.

The pipe class checks are provided in Appendix C.

5.7.2 Design Open Drains

Properties of proposed open drains are given in Table 5-9. The depth refers to the open drain depth up to the rail siding formation level in this table. However, for open drains between existing mainline and siding, the lower formation level between the two has been set as reference of the depth.

Table 5-9 Design Channels Properties

Open Drain ID	Location (to proposed siding)	Chainage From	Chainage To	Length (m)	Base Width (m)	Minimum Depth (m)	Maximum Depth (m)	Grade (%)
D1	West	297,243	297,296	53.0	1.0	1.5	1.7	0.3
D2	West	297,296	297,337	40.3	1.0	1.5	1.5	0.9
D3	West	297,337	297,450	113.6	1.0	1.4	1.5	1.3
D4	East	297,400	297,440	40.0	0.5	0.0	0.7	0.7
D5	East	297,440	297,450	10.5	1.0	0.7	1.1	2.9
D6	West	297,450	297,535	85.6	1.0	0.8	1.2	0.4
D7	East	297,450	297,541	91.0	1.0	0.8	1.1	0.7
D8	East	297,541	297,576	35.0	1.0	1.0	1.2	0.4
D10	West	297,582	297,938	351.9	1.0	1.0	1.0	0.7
D11	West	297,938	298,019	81.4	1.0	0.7	1.0	0.5
D12	East	297,576	298,019	440.5	1.0	0.5	1.0	0.5
D13	West	297,461	297,494	32.6	1.0	0.0 ¹	1.1 ¹	1.9
D14	West	297,494	297,513	23.4	1.0	0.0 ¹	1.1 ¹	4.8
D16	West	297,428	297,489	62.4	0.0	0.4 ¹	1.3 ¹	0.3
D17	West	-	-	54.5	1.0	0.5 ¹	1.0 ¹	2.1
D18	West	-	-	35.0	1.0	0.5 ¹	1.0 ¹	1.9
D20	East	298,020	298,102	82.7	2.0	0.4	0.8	0.3
D21	West	298,020	298,064	44.0	2.0	0.4	1.2	1.3
D22	West	298,235	298,406	172.3	2.0	1.3	1.5	0.6
D23	West	298,406	298,578	174.4	2.0	0.4	1.4	0.3

D24	East	298,118	298,525	135.8	1.0	0.7	0.8	0.3
D25	East	298,525	298,388	135.6	1.0	0.7	0.9	0.6
D26	East	298,388	298,475	86.6	1.0	0.0	0.9	1.8
D27	West (Basin In)	-	-	70.0	2.0	1.3 ¹	1.4 ¹	0.3
D28	West (Basin Out)	-	-	45.9	1.0	0.1 ¹	0.6 ¹	0.3
D29	West	298,110	298,235	124.0	2.0	1.1	1.3	0.4
D30	West	298,064	298,110	45.8	2.0	1.1	1.2	0.4
D31	West (Basin In)	-	-	102.4	2.0	1.3 ¹	1.4 ¹	1.0
D32	East	298,102	298,110	8.0	2.0	0.8	1.0	1.0
D33	East	298,110	298,118	8.0	2.0	0.8	1.0	1.0

1. Open drain is not within the rail corridor. Depth based on road shoulder level/existing surface.

The open drains hydraulic analysis results from DRAINS for 2% AEP and 1% AEP rainfall events are provided in Table 5-10. The results are provided in the order of 2% AEP / 1% AEP. The channels have been designed to ensure to provide the 300mm freeboard to top of rail formation in 2% AEP rainfall event and 500mm freeboard to the top of rail in a 1% AEP rainfall event.

The minimum freeboard was measured from the shoulder of formation of the proposed siding. However, in areas where the shoulder of formation of existing mainline is lower than the proposed siding, the freeboard has been measured from the existing mainline. Also, the freeboard along the channels may vary due to changing depths and expected incoming flows. As such, the minimum freeboard provided has been measured on sections providing the observed least freeboard along the drain.

Channel D23 do not meet the 300mm freeboard at the upstream section due to the level of the formation being lower at the maintenance/construction pad area. The water depth is mainly brought by the addition of the breakout flow upstream of the site. Beyond the maintenance/construction pad, the proposed channel develops depth and grade further downstream and provides greater freeboard more than the minimum required. The water depth upstream is expected to drain quickly due to sufficient grade and channel size provided downstream.

Channels D4 and D26 do not meet the 300mm freeboard to the formation of the mainline – mainly only at the upstream section. Both proposed D4 and D26 are the channels formed in between the turnouts. Channel D4 is reverse graded at 0.72% and grade towards culvert C1. The channel only lacks freeboard at the upstream interface but then gradually develops depth and provides freeboard more than the minimum. Same with channel D26, it is graded at 1.77% and develops greater depth and freeboard as it discharges towards culvert C6. The upstream sections of both channels D4 and D26 are just expecting small catchment and are expected to drain quickly due to the steep grade.

All channels have velocities less than 2 m/s thus there are no requirement to provide lined drains. Channels D8 adjacent to loading facility has been concrete lined to assist maintenance in this area where debris from loading may impact drain performance. Several channels as well have velocities less than 1 m/s in both 2% and 1% AEP. Note that non-silting velocities have not been considered since all channels are meeting the minimum 0.3% grade.

Table 5-10 Design Channels Hydraulic Results

Open Drain ID	Max channel flow (m ³ /s)	Max channel velocity (m/s)	Max flow depth (m)	Minimum freeboard to proposed siding (m) ¹
D1	0.416 / 0.529	1.220 / 1.290	0.302 / 0.336	1.436 / 2.022
D2	0.418 / 0.532	1.250 / 1.330	0.216 / 0.246	1.295 / 1.855
D3	0.382 / 0.488	1.230 / 1.300	0.222 / 0.252	1.322 / 1.410
D4	0.007 / 0.009	0.385 / 0.667	0.026 / 0.070	0.220 / 0.766

D5	0.019 / 0.022	0.660 / 0.690	0.0480 / 0.052	0.924 / 1.510
D6	0.105 / 0.128	0.870 / 0.920	0.094 / 0.106	1.243 / 1.821
D7	0.120 / 0.148	0.940 / 0.990	0.316 / 0.376	0.802 / 1.338
D8	0.117 / 0.154	0.900 / 1.010	0.108 / 0.182	1.057 / 1.373
D10	0.119 / 0.150	0.850 / 0.900	0.170 / 0.204	0.802 / 1.358
D11	0.099 / 0.126	0.870 / 0.930	0.092 / 0.158	0.551 / 1.075
D12	0.112 / 0.144	0.890 / 0.990	0.101 / 0.180	0.906 / 1.417
D13	0.026 / 0.031	0.640 / 0.690	0.096 / 0.132	N/A
D14	0.061 / 0.076	1.190 / 1.280	0.045 / 0.052	N/A
D16	0.075 / 0.093	0.820 / 0.860	0.179 / 0.195	0.471 / 1.044
D17	0.328 / 0.420	1.080 / 1.150	0.587 / 0.615	N/A
D18	0.083 / 0.106	0.760 / 0.800	0.076 / 0.088	N/A
D20	0.032 / 0.061	0.100 / 0.040	0.177 / 0.489	0.383 / 0.731
D21	0.022 / 0.042	0.150 / 0.020	0.337 / 0.801	0.451 / 0.764
D22	1.248 / 1.767	1.080 / 1.190	0.447 / 0.544	1.029 / 1.563
D23	1.056 / 1.525	0.900 / 1.030	0.403 / 0.491	0.031 / 1.021
D24	0.131 / 0.163	0.480 / 0.510	0.188 / 0.227	0.594 / 1.011 ²
D25	0.156 / 0.187	0.620 / 0.660	0.188 / 0.209	0.474 / 0.856 ²
D26	0.032 / 0.039	0.360 / 0.390	0.174 / 0.191	0.294 / 0.865 ²
D27	1.249 / 1.627	1.270 / 1.350	0.428 / 0.485	N/A
D28	0.119 / 0.119	0.490 / 0.490	0.169 / 0.169	N/A
D29	1.247 / 1.748	0.900 / 0.960	0.523 / 0.700	0.766 / 1.204
D30	1.246 / 1.631	0.720 / 0.630	0.622 / 0.824	0.588 / 1.015
D31	1.267 / 1.672	1.290 / 1.400	1.071 / 1.197	N/A
D32	0.032 / 0.130	0.030 / 0.070	0.422 / 0.603	0.612 / 0.846
D33	0.116 / 0.139	0.160 / 0.090	0.422 / 0.603	0.612 / 0.846

- Freeboard for 2% AEP is measured from the top of shoulder of formation. Freeboard for 1% AEP is measured from the TOR.
- Freeboard has been measured against existing mainline. Shoulder of formation of existing mainline is lower than the proposed siding.

The freeboard for channels running parallel beside the maintenance track has been provided with minimum 200mm from top of edge of pavement at 20% AEP as agreed with the client. The freeboard agreement is highlighted in TQ #014. Table 5-11 shows the minimum freeboard against maintenance access track and corresponding 20% AEP hydraulic results from DRAINS. All proposed channels are meeting the minimum freeboard against the maintenance access track.

Table 5-11 Maintenance Access Track Design Channels Hydraulic Results

Open Drain ID	Max channel flow (m ³ /s)	Max channel velocity (m/s)	Max flow depth (m)	Minimum freeboard to access track (m)
D1	0.113	0.890	0.163	0.600
D2	0.116	0.910	0.102	0.997
D3	0.082	0.830	0.096	0.948



D6	0.032	0.620	0.076	0.663
D10	0.033	0.620	0.065	0.832
D11	0.028	0.610	0.067	1.106
D21	0.006	0.130	0.095	0.347
D22	0.113	0.500	0.111	0.499
D23	0.031	0.180	0.102	0.834
D29	0.104	0.410	0.130	0.486
D30	0.137	0.450	0.196	0.412

The proposed channels running parallel with the CBH access road requires freeboard of 300mm from the top of subgrade at 1:20 year ARI (5% AEP) based on the CBH Design Specification. The proposed pavement at the Garner Bin area access road has the top of subgrade at 320mm depth thus requiring a freeboard of 620mm from the edge of the road. A similar top of subgrade depth of 320mm has been assumed for the existing CBH access roads that are running parallel with the proposed channels (Channels D17 and D18). Table 5-12 shows the minimum freeboard against access road and corresponding 5% AEP hydraulic results from DRAINS.

The results show that channels D17 and D18 meet the minimum freeboard requirement. These channels are reconstruction of the existing channels at the south of CBH Area.

Channel D13 has a low velocity due to the low incoming flow. The channel flow depth is mainly driven by the headwater of the culvert C5. However, non-silting velocities have not been considered as these open drains are compliant with the minimum grade for channels per design specification.

Table 5-12 CBH Access Road Design Channels Hydraulic Results

Open Drain ID	Max channel flow (m ³ /s)	Max channel velocity (m/s)	Max flow depth (m)	Minimum freeboard to edge of access road (m)
D13	0.018	0.320	0.240	0.683
D14	0.040	0.920	0.240	0.861
D17	0.181	0.920	0.601	0.349
D18	0.053	0.680	0.601	0.349

5.7.3 Design Basin (Basin B1)

The hydraulic results of the proposed basin are provided in Table 5-13. Properties of the proposed basin is provided in Section 5.4.3

Table 5-13 Proposed Basin Hydraulic Results

PROP BASIN	5% AEP	1% AEP
Pre-Development Flow (m ³ /s)	0.44	1.35
Storage Provided (m ³)	4334	7968
Water Level (m AHD)	297.36	297.90
Freeboard (m)	0.240 (to weir)	0.303 (to top)

Pipe Outlet Flow (m ³ /s)	0.120	0.122
Overflow Weir Flow (m ³ /s)	0.00	1.18

The basin has been designed based on the 5% AEP storage requirements detailed in CBH specification. The 1% AEP major flow has also been considered to ensure that there is no expected increase in overland flow during the major flow event.

The basin proposed is a detention basin thus a low flow 300DIA RCP pipe has been proposed. As discussed in section 5.4.3, the outlet pipe has been designed to be lower to control the conveyed flow at 5% AEP. Based on the results, the outflow from the basin is at 0.12 m³/s at 5% AEP. This is lower than the pre-development flow of 0.44 m³/s.

For events larger than the 5% AEP, the weir is activated and is expected to discharge 1.18 m³/s at 1% AEP. The combined flow from the pipe and the weir is lower than the pre-development flow thus meeting the criteria. The water level during the 1% AEP is contained within the basin and will have a 303mm freeboard from the top of basin.

Based on the results, the proposed basin has maintained the pre-development flows. This entails that the diversion of the flows, originally stored at the existing trapped area at CH 297,950 – CH 298,270, will not affect the CBH site downstream.

5.8 Scour Protection

A scour assessment has been undertaken for each proposed culvert. Velocity results for culverts after running 1% AEP rainfall event in DRAINS indicated the requirement for scour protection as shown in Table 5-14.

Table 5-14 Scour Protection

Culvert ID	Chainage approx. (along proposed siding)	Culvert configuration	Maximum Velocity, 1% AEP (m/s)	Maximum Velocity, 2% AEP (m/s)	Minimum Length of Scour Protection (m)	Minimum D50 rock size (mm)
EXISTING 2X450	297,450	EX 2 x 450mm RCP	-	-	3.0	150
C1	297,450	2 x 450mm RCP	1.870	1.730	3.0	150
C2	297,573	1 x 450mm RCP	1.080	1.040	3.0	150
C3	297,540	1 x 450mm RCP	1.600	1.510	3.0	150
C5	297,494	1 x 450mm RCP	1.380	1.290	3.0	150
C6	298,110	2 x 450mm RCP	0.560	0.360	3.0	150
C7	298,064	5 x 525mm RCP	1.920	1.730	3.0	150
C8	297,900 (BASIN)	1 x 300mm RCP	1.870	1.870	3.0	150

As highlighted in the design criteria in Table 3-1, the scour protection design adopted is based on MRWA design guide. The Facing class, which is the minimum rock class, has a minimum design velocity of 2 m/s to require a rock protection. However, to satisfy the basis of design requirement of culvert outlet velocities exceeding 1 m/s, the Facing class has been adopted even for velocities less than 2m/s.

The result shows that culvert C6 has a low velocity due to the outlet control condition of the culvert. Rock protection has been proposed on it regardless of the low velocity of this culvert.

Due to the new open drain between the existing mainline and siding, the scour protection of the existing mainline 2x450DIA culvert upstream of culvert C1 needs to be reconstructed. The scour protection will extend from the existing culvert outlet headwall apron to the proposed culvert C1 inlet headwall apron. Velocity results of the existing culvert analysis from DRAINS at 1% AEP rainfall event indicated the minimum scour protection requirement.



6. Outstanding Issues, Risk & Opportunity

6.1 Outstanding Issues

The following outstanding are identified and will be further investigated in the next design phase

Item	Outstanding Issue	Impact
Flood assessment result (design scenario)	Differences with flood results on design scenario assessment (stage 2)	Potential adjustment on proposed drainage structures based on aligning flood and drainage results.

6.2 Risk

No risks have been identified at this stage of design.

6.3 Opportunity

No opportunities have been identified at this stage of design.



CBH GROUP

PERENJORI
RAIL OUT-LOADING FACILITY

ENVIRONMENTAL NOISE ASSESSMENT

OCTOBER 2022

OUR REFERENCE: 30142-3-22201-02



EXECUTIVE SUMMARY

CBH Group commissioned Herring Storer Acoustics to carry out an acoustic study of noise emissions for a proposed new rail out loading facility in Perenjori, Western Australia.

CBH is currently investigating the potential of constructing new rail out loading siding infrastructure at Perenjori (see Figure 1.1 and 1.2). This includes new train loading infrastructure and necessary development to internal site roads. The installation of this new infrastructure will increase the amount of grain currently transported by rail to port. It is expected that current daily truck traffic on the local road networks will be reduced significantly.

This acoustic assessment is required for the approval process and is being undertaken prior to the final design of the plant being known. Whilst the plant design is undergoing final consideration, for the purpose of the predictive noise modelling, the current design has been used as a basis for the assessment.

The nearest noise sensitive premises are situated approximately 2km to the southeast of the proposed facility in the town of Perenjori.

FREIGHT RAIL

Under the State Planning Policy 5.4, the requirements for the development of existing infrastructure, such as the siding rail line, are outlined in Table 2: Noise Targets. Under the policy, we note that practicable noise management and mitigation measures should be considered, having regard to –

- The existing transport noise levels.
- The likely changes in noise emissions resulting from the proposal.
- The nature and scale of the works and the potential for noise amelioration; and
- Also, the proponent should prepare a noise management plan for the redevelopment works in accordance with the guidelines, and in consultation with the state environmental agency and local government.

Additional to the above, "Reasonable and practicable measures" it is recognised that in some instances it may not be reasonable and practicable to meet the Noise Limits, especially in cases of major redevelopment of existing transport infrastructure. In these situations, the focus of the policy is to achieve:

"the lowest level of noise, with other reasonable and practicable consideration being secondary to this objective."

As the construction of the rail component for the out-loading facility would constitute a major change, the infrastructure provider is required to undertake noise mitigation to reduce noise emissions from the development to meet the "Noise Target" and implement a noise management plan.

The policies "Noise Limits" for this upgrade are:

EXTERNAL

$L_{Aeq(Day)}$ of 60 dB(A);
 $L_{Aeq(Night)}$ of 55 dB(A).

Assessment has been undertaken of the current noise levels and compared to the noise levels for the design changes. The noise level for the current freight rail movements, which is based on one per day is 46 dB $L_{Aeq}(\text{Day})$ at the nearest noise receiver to the project.

The future noise level as per the requirements of the State Planning Policy 5.4 requires consideration of 1 train per hour if the train volumes are unknown. However, for this operation, CBH will only be responsible for one train per day for future operations, hence the noise levels will be as per the current levels of 46 dB(A). This can be compared to the criteria noted above.

The acoustic assessment shows that in the worst case, that noise received at a premise is below the criterion noise level. Thus, noise emissions from the proposed out-loading facility – rail component would be deemed to comply with the requirements of the state Planning Policy 5.4.

OUT-LOADING FACILITY

Noise levels for the proposed Out-Loading Facility have been assessed as 23 dB(A) during the day period at the nearest noise sensitive receiver. This can be compared to the criteria of 45 dB(A) during the day and 35 dB(A) during the night period. The noise levels have been assumed to contain tonal characteristics, hence include a +5 dB penalty.

Additional; to the above out-loading noise sources, the train moving through the siding has been assessed. Whilst the train is technically not assessable under the environmental noise regulations as it is part of the rail infrastructure, for completeness, the noise of the train has been assessed.

For the period of train movement in the siding, the noise level at the nearest residential premise is 43 dB(A). Given the speed at which the train moves, i.e. in the siding for around 3 hours, the dominant noise is the Locomotive, hence the noise level is when the loco is nearest to the residential premise, with noise levels being less as the train moves.

The acoustic assessment shows that in the worst case, that noise received at a premise is below the assigned noise level. Thus, noise emissions from the proposed out-loading facility would be deemed to comply with the requirements of the Regulations.

CONTENTS

1.	INTRODUCTION	1
2.	ACOUSTIC CRITERIA	2
2.1	<i>Environmental Protection (Noise) Regulations 1997</i> (WA)	3
2.2	Western Australian Planning Commission State Planning Policy 5.4 Road and Rail Noise	4
3.	MONITORED NOISE	7
4.	ANALYSIS	9
5.	METHODOLOGY	12
5.1	FREIGHT RAIL	12
5.2	OUT LOAD FACILITY	12
6.	RESULTS	14
7.	ASSESSMENT	15
7.1	FREIGHT RAIL	15
7.2	OUT LOAD FACILITY	16

APPENDICES

A	Location Plan
B	Noise Contour Plots
C	Monitoring Data

1. INTRODUCTION

CBH Group commissioned Herring Storer Acoustics to carry out an acoustic study of noise emissions for a proposed new rail out loading facility in Perenjori, Western Australia.

CBH is currently investigating the potential of constructing new rail out loading siding infrastructure at Perenjori (see Figure 1.1 and 1.2). This includes new train loading infrastructure and necessary development to internal site roads. The installation of this new infrastructure will increase the amount of grain currently transported by rail to port. It is expected that current daily truck traffic on the local road networks will be reduced significantly.

The new facility will be located approximately 3.5km northwest of the existing CBH rail loading facility in Perenjori.

As a part of the acoustic assessment, the following has been considered:

- Rail out load facility with noise emissions assessable under *The Western Australian Planning Commission State Planning Policy 5.4 Road and Rail Noise*.
- Rail out load facility with noise emissions assessable under *Environmental Protection (Noise) Regulations 1997*.

The assessment of noise for the proposed operation of the site is required to support Development Application for the proposed works.

The nearest noise sensitive premises are situated approximately 2km to the southeast of the proposed facility in the town of Perenjori.



FIGURE 1.1 – LOCATION PLAN

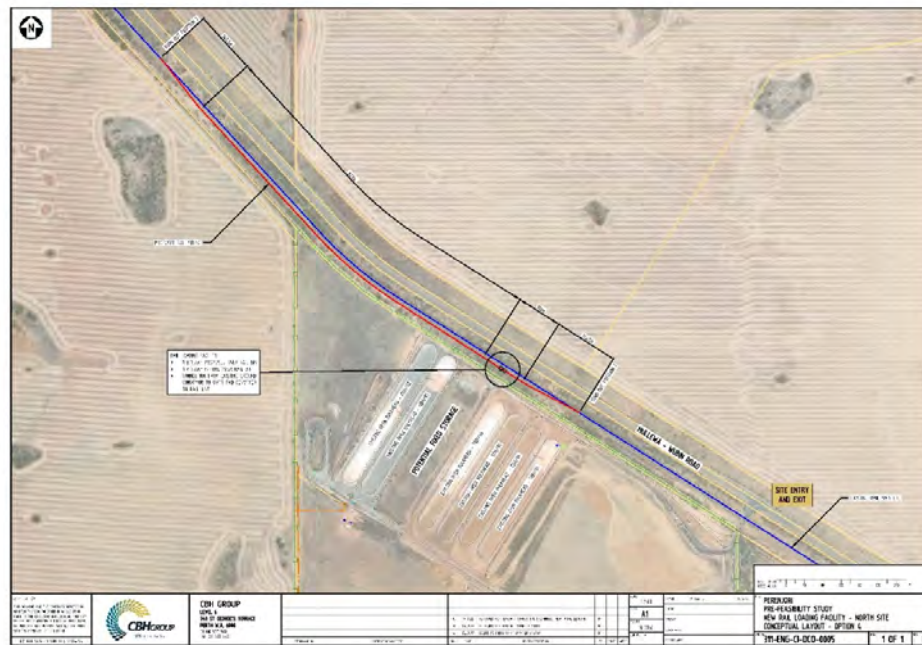


FIGURE 1.2 – LAYOUT PLAN

2. ACOUSTIC CRITERIA

The nearest noise sensitive premises (R1 and R4) are situated approximately 2km to the southeast of the proposed facility.

The proposed site boundary and receiver locations are shown in Figure 2.1, and Appendix A.

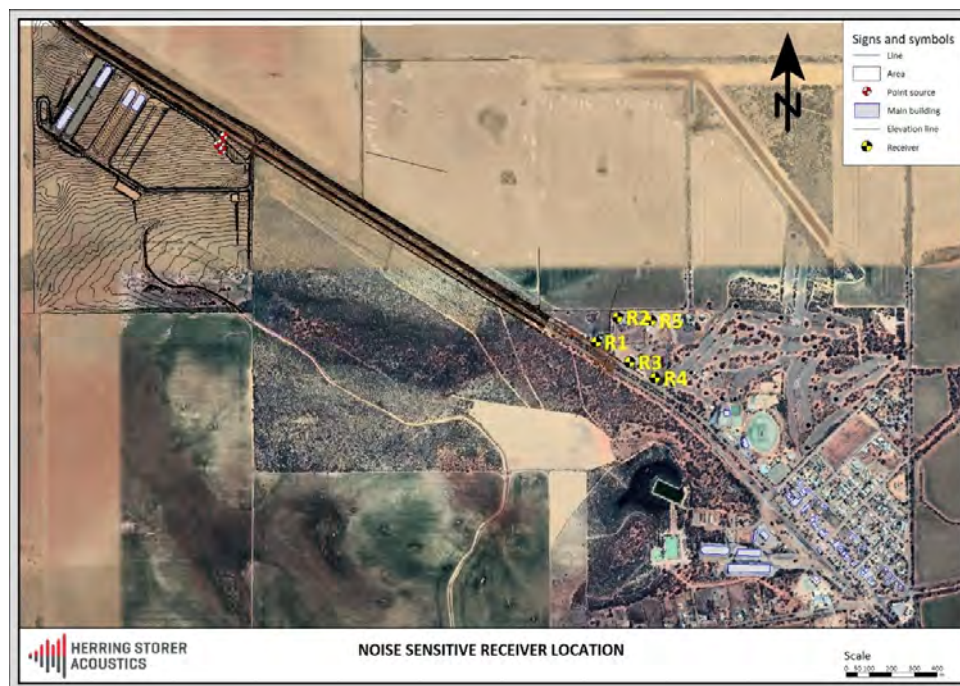


FIGURE 2.1 – RESIDENTIAL RECEIVER LOCATION MAP

2.1 ENVIRONMENTAL PROTECTION (NOISE) REGULATIONS 1997 (WA)

These regulations stipulate maximum allowable external noise levels that can be received at premises. For noise sensitive premise, the assigned noise levels are determined by the calculation of an influencing factor, which is then added to base noise levels. The influencing factor is calculated for the usage of land within the two circles, having radii of 100m and 450m from the premises of concern. For commercial and industrial premises, the assigned noise levels are fixed. The base assigned noise levels for noise sensitive premises and the fixed assigned noise levels for commercial and industrial premises are listed in Table 2.1.

TABLE 2.1 –ASSIGNED OUTDOOR NOISE LEVELS

Type of premises receiving noise	Time of day	Assigned level (dB)		
		L _{A10}	L _{A1}	L _{Amax}
Noise sensitive premises: highly sensitive area (i.e within 15m of a dwelling)	0700 to 1900 hours Monday to Saturday	45 + IF	55 + IF	65 + IF
	0900 to 1900 hours Sunday and public holidays	40 + IF	50 + IF	65 + IF
	1900 to 2200 hours all days	40 + IF	50 + IF	55 + IF
	2200 hours on any day to 0700 hours Monday to Saturday and 0900 hours Sunday and public holidays	35 + IF	45 + IF	55 + IF
Noise sensitive premises: any area other than highly sensitive area	All hours	60	75	80
Commercial premises	All hours	60	75	80
Industrial and utility premises	All hours	65	80	90

Note: The L_{A10} noise level is the noise that is exceeded for 10% of the time.
The L_{A1} noise level is the noise that is exceeded for 1% of the time.
The L_{Amax} noise level is the maximum noise level recorded.
IF = Influencing Factor

At the neighbouring residences, the influencing factor would be 0dB(A) as there are no significant noise sources within the area of influence i.e. 100 and 450m. Hence, the assigned noise levels are as listed in Table 2.1.

Where the above characteristics are present and cannot be practicably removed, the following adjustments are made to the measured or predicted level at other premises.

TABLE 2.2 – ADJUSTMENTS FOR ANNOYING CHARACTERISTICS WHEN MUSIC IS NOT PRESENT

Where tonality is present	Where modulation is present	Where impulsiveness is present
+ 5 dB	+ 5 dB	+ 10 dB

Note: Where annoying characteristics are present, the adjustment is +10 dB, and is +15dB if impulsiveness is present. The adjustments are cumulative to a maximum of 15 dB.

Where there is more than one industry that emits noise to a residence and the combined noise levels of all industries results in an exceedance to the assigned noise levels, each industry is required to be at least 5 dB less than these levels as documented below (Regulation 7(2)).

“Noise emitted from any premises or public place when received at other premises –

(a) must not cause, or significantly contribute to, a level of noise which exceeds the assigned level in respect of noise received at premises of that kind...

*“...a noise emission is taken to **significantly contribute** to a level of noise if the noise emission ... exceeds a value which is 5 dB below the assigned level at the point of reception.”*

Under the Regulations, noise received at a residence is deemed to be NOT “significantly contributing” to the noise received at a premises if it is at least 5 dB(A) below the assigned noise level. Table 3.3 presents the required outdoor noise levels at each residence to comply with the Regulations.

**TABLE 3.3 – NOT “SIGNIFICANTLY CONTRIBUTING”
OUTDOOR NOISE LEVELS AT RESIDENCES**

Type of premises receiving noise	Time of day	Assigned level (dB)		
		L _A 10	L _A 1	L _A max
Noise sensitive premises: highly sensitive area (i.e. within 15m of a dwelling)	0700 to 1900 hours Monday to Saturday	40	50	60
	0900 to 1900 hours Sunday and public holidays	35	45	60
	1900 to 2200 hours all days	35	45	50
	2200 hours on any day to 0700 hours Monday to Saturday and 0900 hours Sunday and public holidays	30	40	50

2.2 WESTERN AUSTRALIAN PLANNING COMMISSION STATE PLANNING POLICY 5.4 ROAD AND RAIL NOISE

The Western Australian Planning Commission (WAPC) released on 6th September 2019 State Planning Policy 5.4 “Road and Rail Noise”. The requirements of State Planning Policy 5.4 are outlined below.

POLICY APPLICATION (Section 4)

When and where it applies (Section 4.1)

SPP 5.4 applies to the preparation and assessment of planning instruments, including region and local planning schemes; planning strategies, structure plans; subdivision and development proposals in Western Australia, where there is proposed:

- a) noise-sensitive land-use within the policy’s trigger distance of a transport corridor as specified in **Table 1**;*
- b) New or major upgrades of roads as specified in **Table 1** and maps (**Schedule 1,2 and 3**); or*
- c) New railways or major upgrades of railways as specified in maps (**Schedule 1, 2 and 3**); or any other works that increase capacity for rail vehicle storage or movement and will result in an increased level of noise.*

Policy trigger distances (Section 4.1.2)

Table 1 identifies the State’s transport corridors and the trigger distances to which the policy applies.

*The designation of land within the trigger distances outlined in **Table 1** should not be interpreted to imply that land is affected by noise and/or that areas outside the trigger distances are un-affected by noise.*

*Where any part of the lot is within the specified trigger distance, an assessment against the policy is required to determine the likely level of transport noise and management/ mitigation required. An initial screening assessment (**guidelines: Table 2: noise exposure forecast**) will determine if the lot is affected and to what extent."*

Table 1: Transport corridor classification and trigger distances

Transport corridor classification	Trigger distance	Distance measured from
Roads		
Strategic freight and major traffic routes Roads as defined by Perth and Peel Planning Frameworks and/or roads with either 500 or more Class 7 to 12 Austroads vehicles per day, and/or 50,000 per day traffic volume	300 metres	Road carriageway edge
Other significant freight/traffic routes These are generally any State administered road and/or local government road identified as being a future State administered road (red road) and other roads that meet the criteria of either >=23,000 daily traffic count (averaged equivalent to 25,000 vehicles passenger car units under region schemes)	200 metres	Road carriageway edge
Passenger railways		
	100 metres	Centreline of the closest track
Freight railways		
	200 metres	Centreline of the closest track

Proponents are advised to consult with the decision making authority as site specific conditions (significant differences in ground levels, extreme noise levels) may influence the noise mitigation measures required, that may extend beyond the trigger distance.

POLICY MEASURES (Section 6)

The policy applies a performance-based approach to the management and mitigation of transport noise. The policy measures and resultant noise mitigation will be influenced by the function of the transport corridor and the type and intensity of the land-use proposed. Where there is risk of future land-use conflict in close proximity to strategic freight routes, a precautionary approach should be applied. Planning should also consider other broader planning policies. This is to ensure a balanced approach takes into consideration reasonable and practical considerations.

Noise Targets (Section 6.1)

Table 2 sets out noise targets that are to be achieved by proposals under which the policy applies. Where exceeded, an assessment is required to determine the likely level of transport noise and management/mitigation required.

In the application of the noise targets the objective is to achieve:

- *indoor noise levels as specified in **Table 2** in noise sensitive areas (for example, bedrooms and living rooms of houses, and school classrooms); and*
- *a reasonable degree of acoustic amenity for outdoor living areas on each residential lot. For non-residential noise-sensitive developments,*

for example schools and child care centres the design of outdoor areas should take into consideration the noise target.

It is recognised that in some instances, it may not be reasonable and/or practicable to meet the outdoor noise targets. Where transport noise is above the noise targets, measures are expected to be implemented that balance reasonable and practicable considerations with the need to achieve acceptable noise protection outcomes.

Table 2: Noise Targets

Proposals	New/Upgrade	Noise Targets		
		Outdoor		Indoor
		Day ($L_{Aeq}(\text{Day})$ dB) (6 am-10 pm)	Night ($L_{Aeq}(\text{Night})$ dB) (10 pm-6 am)	(L_{Aeq} dB)
Noise-sensitive land-use and/or development	New noise sensitive land use and/or development within the trigger distance of an existing/proposed transport corridor	55	50	$L_{Aeq}(\text{Day})$ 40(Living and work areas) $L_{Aeq}(\text{Night})$ 35 (bedrooms)
Roads	New	55	50	N/A
	Upgrade	60	55	N/A
Railways	New	55	50	N/A
	Upgrade	60	55	N/A

Notes:

- The noise target is to be measured at one metre from the most exposed, habitable façade of the proposed building, which has the greatest exposure to the noise-source. A habitable room has the same meaning as defined in State Planning Policy 3.1 Residential Design Codes.
- For all noise-sensitive land-use and/or development, indoor noise targets for other room usages may be reasonably drawn from Table 1 of Australian Standard/New Zealand Standard AS/NZS 2107:2016 Acoustics – Recommended design sound levels and reverberation times for building interiors (as amended) for each relevant time period.
- The 5dB difference in the criteria between new and upgrade infrastructure proposals acknowledges the challenges in achieving noise level reduction where existing infrastructure is surrounded by existing noise-sensitive development.
- Outdoor targets are to be met at all outdoor areas as far as is reasonable and practical to do so using the various noise mitigation measures outlined in the guidelines. For example, it is likely unreasonable for a transport infrastructure provider to achieve the outdoor targets at more than 1 or 2 floors of an adjacent development with direct line of sight to the traffic.

3. MONITORED NOISE

As per the “Draft Guidelines on Environmental Noise for Prescribed Premises” (released in May 2016), continuous noise monitoring has been conducted to establish the ambient noise levels.

Continuous noise monitoring was conducted for a seven-day period at two reference locations within the study area. The locations were chosen to represent the current noise environment associated with the freight rail, and out loading facility.

The locations are as follows:

- A. Loading Street, near to the existing out load facility.
- B. Wubin- Mullewa Road – Proposed facility

Figure 3.1 and 3.2 details the location maps, with Figure 3.3 showing the monitoring units in-situ.



FIGURE 3.1 - LOCATION A – MONITOR MAP



FIGURE 3.2 - LOCATION B – MONITOR MAP

At each of the monitoring locations, Class 2, noise monitors were used to capture continuous noise data from 31st August to 5th September 2022. The monitoring units were calibrated prior to and post deployment.

Automatic noise data loggers record sound pressure levels in accordance with Australian Standard 2702-1984: *Acoustics - Method for Measurement of Road Traffic Noise*. The logger records statistical noise level data of which, the L_{A1} , L_{A10} , L_{Aeq} and L_{A90} levels are reported. These are defined below:

- L_{A1} The noise level exceeded for 1% of the time (in this instance, the noise level exceeded for 36 seconds in each 1-hour period).
- L_{A10} The noise level exceeded for 10% of the time (in this instance, the noise level exceeded for 6 minutes in each 1-hour period).
- L_{Aeq} The energy equivalent noise level for the 1-hour period. A single number value that expresses the time-varying sound level for the 1-hour period as though it were a constant sound level with the same total sound energy as the time-varying level.
- L_{A90} The noise level exceeded for 90% of the time (in this instance, the noise level exceeded for 54 minutes in each 1-hour period).

Weather data from the Bureau of Meteorology site was reviewed to determine the influence, if any, of the wind and rain conditions during the monitoring. These conditions were compared to the specification and where required; the results were excluded from the data.



FIGURE 3.3 - MONITORING UNITS IN SITU

4. ANALYSIS

From the logger data, the $L_{10(18\text{hour})}$, $L_{eq(24\text{hour})}$, $L_{eq(16\text{hour})}$ and $L_{eq(8\text{hour})}$ were calculated, which are defined below:

$L_{eq(16\text{hour})}$ is the logarithmic average of the hourly L_{eq} values between 0600 hours and 2200 hours.

$L_{eq(8\text{hour})}$ is the logarithmic average of the hourly L_{eq} values between 2200 hours and 0600 hours on the same day.

Data has been included in Appendix B for references purposes, with the information as follows:

- Recorded noise levels in graphical form.

To differentiate the noise associated with only freight rail from the overall noise levels influenced by vehicles on the road network and other localised noise sources, analysis of the time history for the monitoring locations to the rail line was undertaken.

As the time history contained noise levels at one second intervals, correlation could be made using both these descriptors, as well as comparison against the freight train timetable for this section of the rail line.

An example detailing the time history of the rail out load facility is shown for Location A –in Figure 4.1. Figure 4.2 details an individual train pass for Location B - Perenjori North proposed facility.

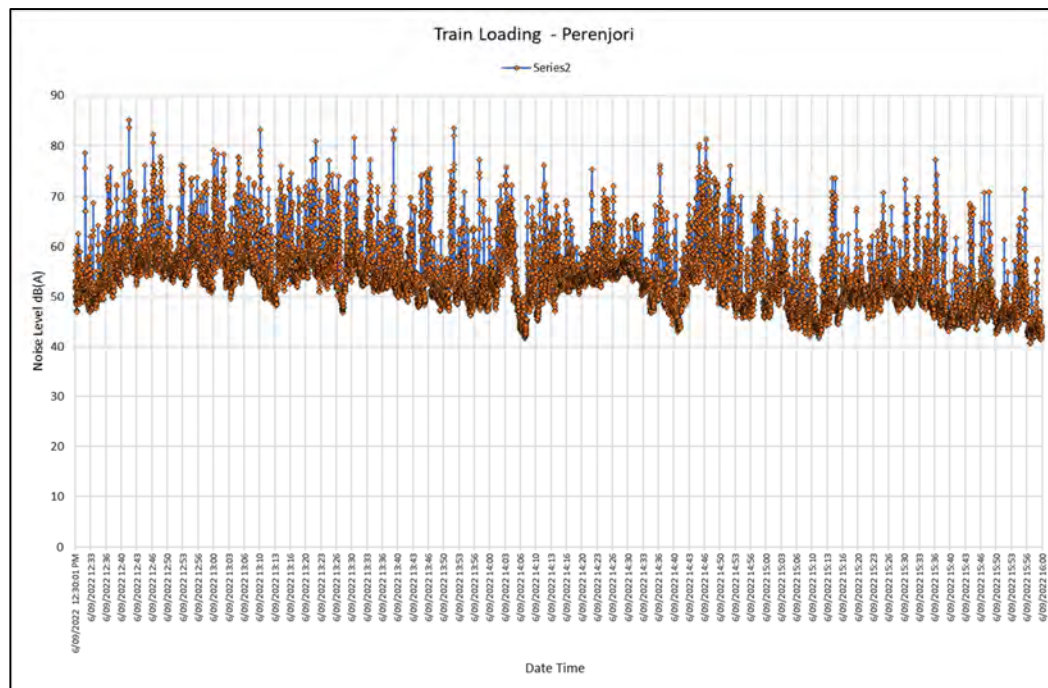


FIGURE 4.1 - NOISE TIME HISTORY RAIL OUT LOADING

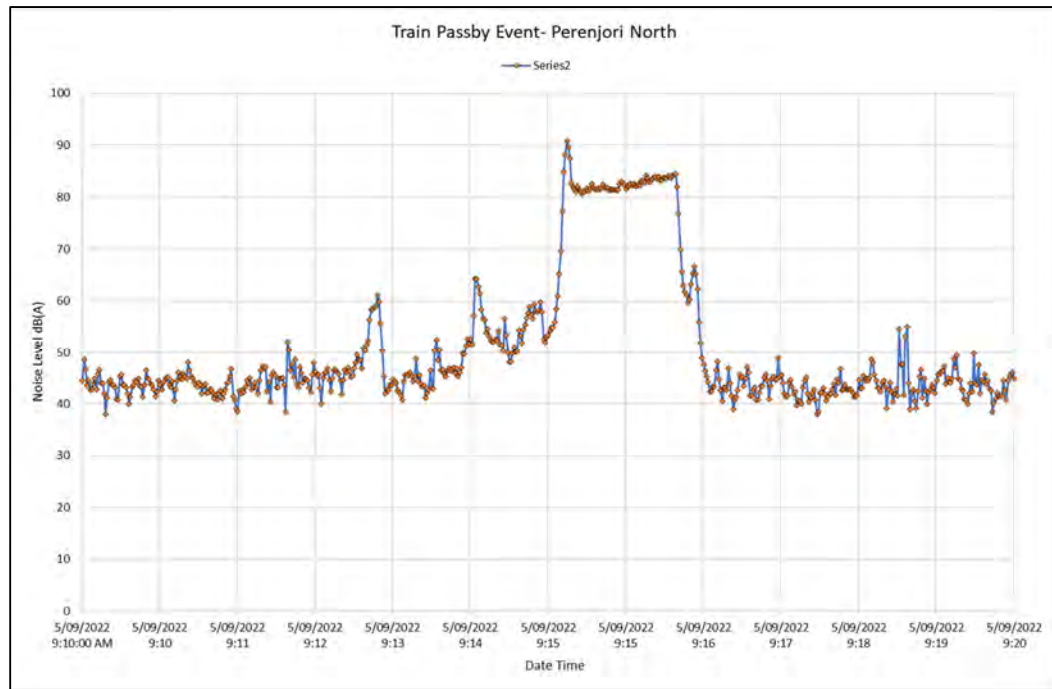


FIGURE 4.2 - NOISE TIME HISTORY RAIL OUT LOADING

For completeness, the overall noise levels at each monitoring location have been included. These noise levels are of all noise, including road, freight rail and localised noise sources. This data is prior to analysis where individual trains are identified. Figures 4.3 and 4.4 shows the overall graphical results.

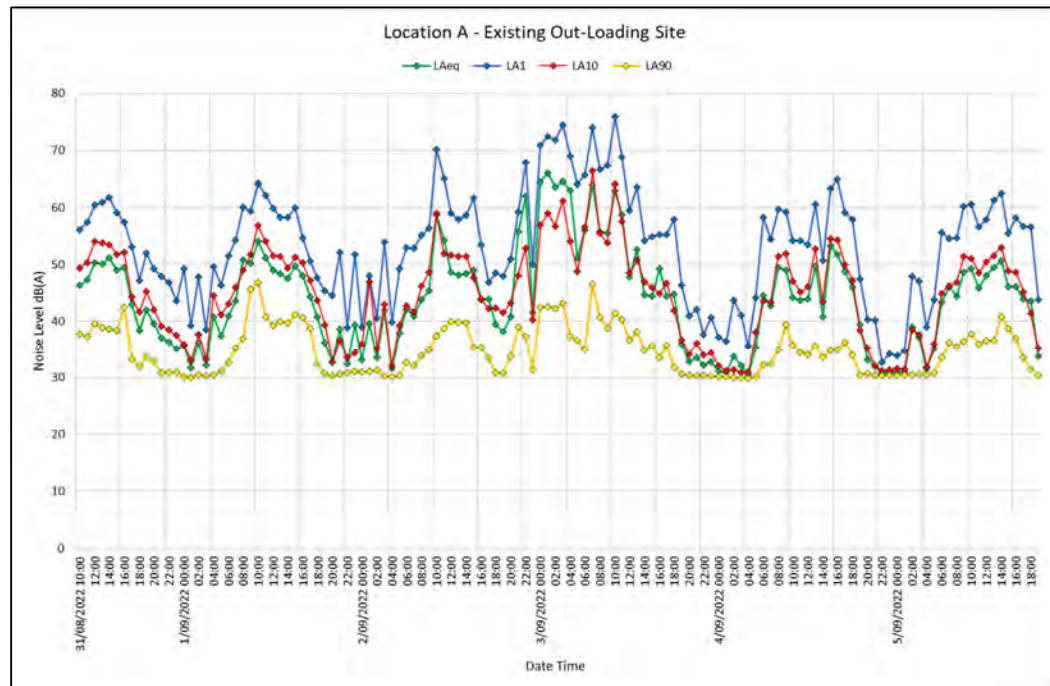


FIGURE 4.3 – LOCATION A – OVERALL NOISE LEVELS

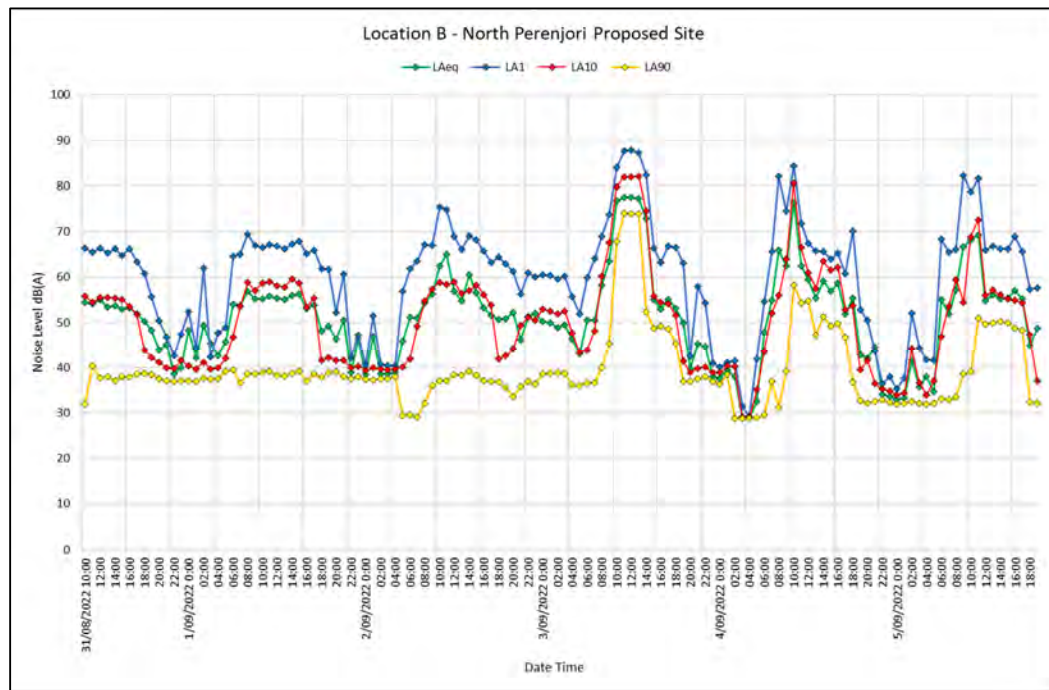


FIGURE 4.4 – LOCATION B– OVERALL NOISE LEVELS

Utilising detailed data analysis as outlined above, noise levels for each train pass-by event, at each monitoring location were analysed with the resultant levels summarised in Table 4.1.

TABLE 4.1 - SUMMARY OF MEASURED TRAIN NOISE LEVELS L_{Aeq}

Parameter	Noise Level $L_{Aeq(event)}$	Time Train Noise Present	Comment
Loc A	82	90 seconds	Freight Train Movement – at speed
Loc B	62	64 seconds	Freight Train Movement – at speed
Loc A	61	3 hours	Train loading, including train movement in siding

Based on analysis of the measured noise levels, at the monitoring points, train noise is present for an average of 90 seconds for passby events on the main line.

For train loading, including the train movement withing the loading siding, the noise is present over a 3 to-3.5-hour period.

The above individual train pass noise level can be used to calculate the $L_{Aeq(16hour)}$ Day, and the $L_{Aeq(8hour)}$ Night noise levels.

CBH have provided information relating to the future train movements. Whilst there is an upgrade to the grain loading facility, the train movements remain the same, being one per day. However, as this could occur at any time over a 24 hour period the more stringent criteria noise level of night time would be the appropriate assessment.

Hence, Table 6.2 contains the details used for the calculations.

TABLE 4.2 – RELATIONSHIP BETWEEN MEASURED NOISE LEVELS AND TRAIN VOLUMES

Description	Current Freight Rail	Future Freight Rail
Train Qty per 24 hours	1	1
Train Qty per 16 hours Day	1	1
Train Qty per 8 hours Night	1	1
$L_{Aeq(90second)}$ at receiver (Train pass by event)	82	82
Distance from Train line to Monitoring point (meters)	18	18
Time train noise is present (Seconds)	90	90
Total time noise present (Minutes) Day	1.5	1.5
Total time noise present (Minutes) Night	2	2
Total time noise present (Minutes) 24 hours	3.5	3.5
$L_{Aeq(16hour)}$ Day period	53.9*	53.9*
$L_{Aeq(8hour)}$ Night period	56.9*	56.9*

*At monitoring location.

5. METHODOLOGY

5.1 FREIGHT RAIL

To determine the requirements of any noise amelioration, acoustic modelling was carried out using the computer program 'SoundPlan'.

Acoustic modelling was carried out for the following scenarios:

- Current Freight rail movements
- Freight trains at a quantity of 1 train per 24 hours with the proposed design alignment.

5.2 OUT LOAD FACILITY

Noise immissions¹ at the nearest neighbouring residential premises, due to noise associated with the proposed operations, were modelled using the computer programme SoundPlan. Sound power levels used for the noise modelling were based on both manufacturer data and measured sound pressure levels of similar equipment proposed for use on site.

This acoustic assessment is required for the approval process and is being undertaken prior to the final design of the plant being known. Whilst the plant design is undergoing final consideration, for the purpose of the predictive noise modelling, the current design has been used as a basis for the assessment.

The modelling of noise levels has been based on noise sources and sound power levels shown in Figure 5.1 and Table 5.1.

¹ Immissions – noise received at a source

² Emissions – noise emanating from a source and / or location

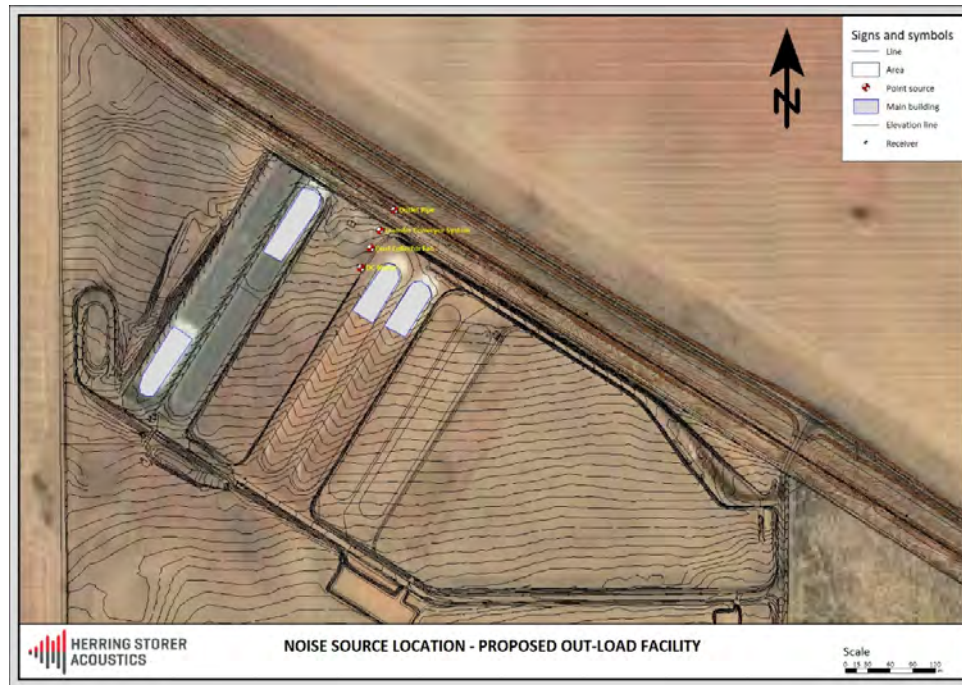


FIGURE 5.1 – NOISE SOURCE LOCATION

TABLE 5.1 – SOUND POWER LEVEL - NOISE SOURCES dB(A)

Noise Sources	Sound Power Level dB(A)
DC Motor	105
Load Outlet Pipe	93
PM Dust Collector Fan	97
Vent	102

Based on noise emissions from the above equipment, the following operating scenario was developed:

SCENARIO 1

Out-Loading plant operations

SCENARIO 2

Train in siding – moving through loading facility

It is noted, that for the scenarios considered, all equipment has been assumed to be operating at the same time.

The following input data was used in the calculations:

- Provided site layouts.
- Sound Power Levels as listed.
- Ground contours and receiver points provided by client.

Weather conditions for modelling were as stipulated in the Environmental Protection Authority's "Draft Guidance for Assessment of Environmental Factors No. 8 - Environmental Noise" as listed in Table 5.2.

TABLE 5.2 – WEATHER CONDITIONS

Condition	Night	Day
Temperature	15°C	20°C
Relative humidity	50%	50%
Pasquill Stability Class	F	E
Wind speed	3 m/s*	4 m/s*

* From sources, towards receivers.

It is noted that ‘worst case’ wind conditions refer to conditions where there is a temperature inversion in conjunction with light winds in the direction from noise source to receiver, resulting in the highest sound propagation towards receiver locations.

6. RESULTS

A summary of the calculated noise levels for scenarios are shown in Table 6.1 and 6.2.

TABLE 6.1 – FREIGHT RAIL TRAFFIC CALCULATED NOISE LEVELS, $L_{Aeq(Day)}$ dB(A)

Receiver Name	Current	1 Train per hour
R1	45	45
R2	39	39
R3	46	46
R4	45	45
R5	37	37

TABLE 6.1 – OUT-LOADING FACILITY CALCULATED NOISE LEVELS, L_{Aeq} dB(A)

Receiver Name	Out-Loading Facility	Train Movement in Siding
R1	18	42
R2	17	43
R3	16	38
R4	16	36
R5	16	37

Noise contour plots for the above scenarios are included in Appendix B.

7. ASSESSMENT

7.1 FREIGHT RAIL

Under the State Planning Policy 5.4, the requirements for the development of existing infrastructure, such as the siding rail line, are outlined in Table 2: Noise Targets. Under the policy, we note that practicable noise management and mitigation measures should be considered, having regard to –

- The existing transport noise levels.
- The likely changes in noise emissions resulting from the proposal.
- The nature and scale of the works and the potential for noise amelioration; and
- Also, the proponent should prepare a noise management plan for the redevelopment works in accordance with the guidelines, and in consultation with the state environmental agency and local government.

Additional to the above, "Reasonable and practicable measures" it is recognised that in some instances it may not be reasonable and practicable to meet the Noise Limits, especially in cases of major redevelopment of existing transport infrastructure. In these situations, the focus of the policy is to achieve:

"the lowest level of noise, with other reasonable and practicable consideration being secondary to this objective."

In such cases, longer term strategies for land use planning, transport policy and vehicle emissions should be considered to minimise transport noise impact over time.

As the construction of the rail component for the out-loading facility would constitute a major change, the infrastructure provider is required to undertake noise mitigation to reduce noise emissions from the development to meet the "Noise Target" and implement a noise management plan.

The policies "Noise Limits" for this upgrade are:

EXTERNAL

$L_{Aeq(Day)}$ of 60 dB(A);

$L_{Aeq(Night)}$ of 55 dB(A).

Assessment has been undertaken of the current noise levels and compared to the noise levels for the design changes. Table 7.1 shows the comparison values along with the level of compliance with the criterion noise levels.

TABLE 7.1 –NOISE LEVEL COMPARISON ASSESSMENT

Receiver	Current $L_{Aeq(16\text{hour})}$ Day Noise Level dB(A)	Future $L_{Aeq(16\text{hour})}$ Day Noise Level dB(A)	Criteria (Night)	Receiver
R1	45	45	55	Complies
R2	39	39	55	Complies
R3	46	46	55	Complies
R4	45	45	55	Complies
R5	37	37	55	Complies

7.2 OUT LOAD FACILITY

It is assumed that during the day period, the operational noise will likely contain ‘tonality’ characteristic due to the presence of fans etc. Therefore, a +5 dB(A) penalty has been included in the day period assessable noise levels.

Based on this, Table 7.2 contain the applicable adjustments, with Table 6.3 showing the assessable noise levels.

TABLE 7.2 – SCENARIO 1 - APPLICABLE ADJUSTMENTS AND ASSESSABLE LEVEL OF NOISE EMISSIONS, L_{A10} dB(A) DAY OPERATIONS

Noise Measurement Location	Calculated Noise Level Scenario 1 Day Time L _{A10}	Applicable Adjustments to Measured Noise Levels, Characteristics			Assessable Noise Level
		Where Noise Emission is NOT music			
		Tonality	Modulation	Impulsiveness	
R1	18	+5	-	-	23
R2	17	+5	-	-	22
R3	16	+5	-	-	21
R4	16	+5	-	-	21
R5	16	+5			21

TABLE 7.3 – ASSESSMENT OF NOISE LEVELS

Scenario	Receiver	Assessable Noise Level, dB(A)	Applicable Times of Day	Applicable L_{A01} Assigned Noise Level (dB)	Exceedance to Assigned Noise Level L_{A01} (dB)
Day Time Scenario 1	R1	23	Day (Weekdays and Saturday 07:00 to 19:00)	45	Complies
	R2	22			Complies
	R3	21			Complies
	R4	21			Complies
	R5	21			Complies

The operating scenarios consider all noise sources from the proposed facilities operating at the same time. The calculated noise levels have been assessed under the highest propagation weather conditions.

Additional; to the above out-loading noise sources, the train moving through the siding has been assessed. Whilst the train is technically not assessable under the environmental noise regulations as it is part of the rail infrastructure, for completeness, the noise of the train has been assessed.

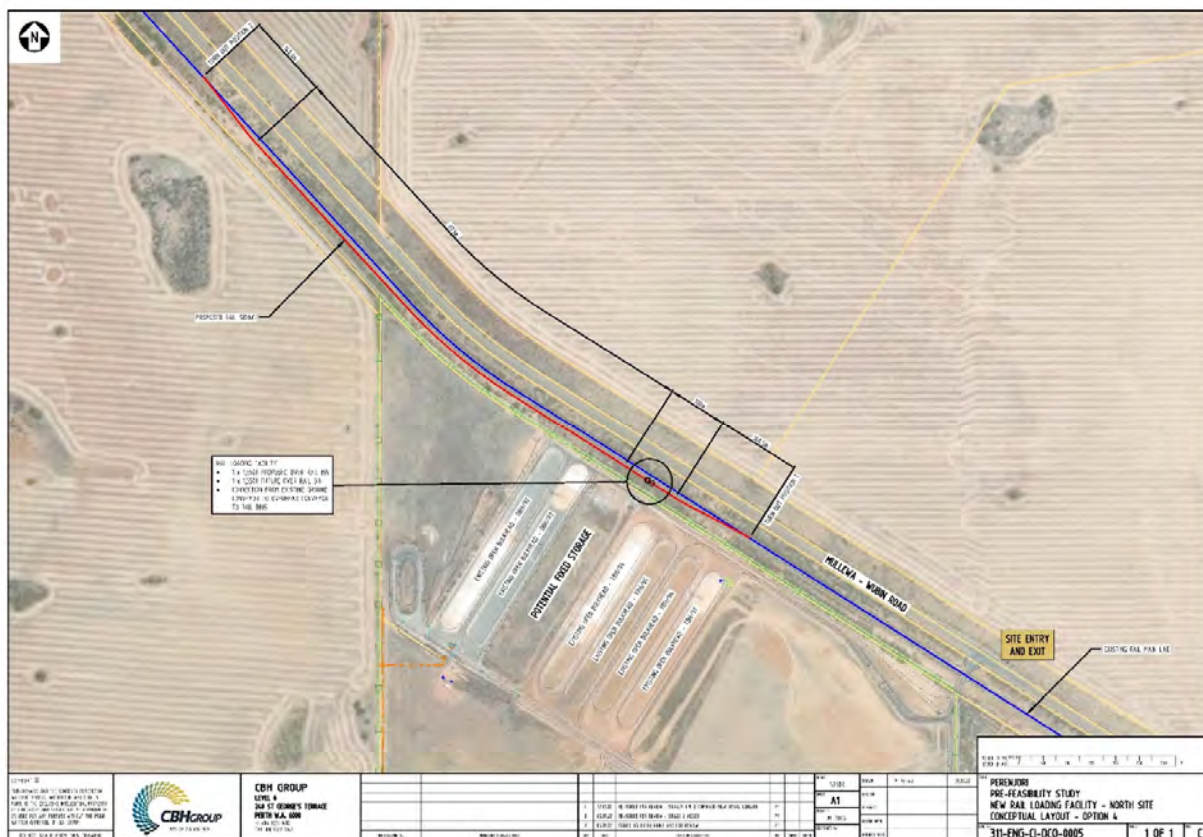
Therefore, if considered under the regulations, Table 7.4 shows the assessable noise levels.

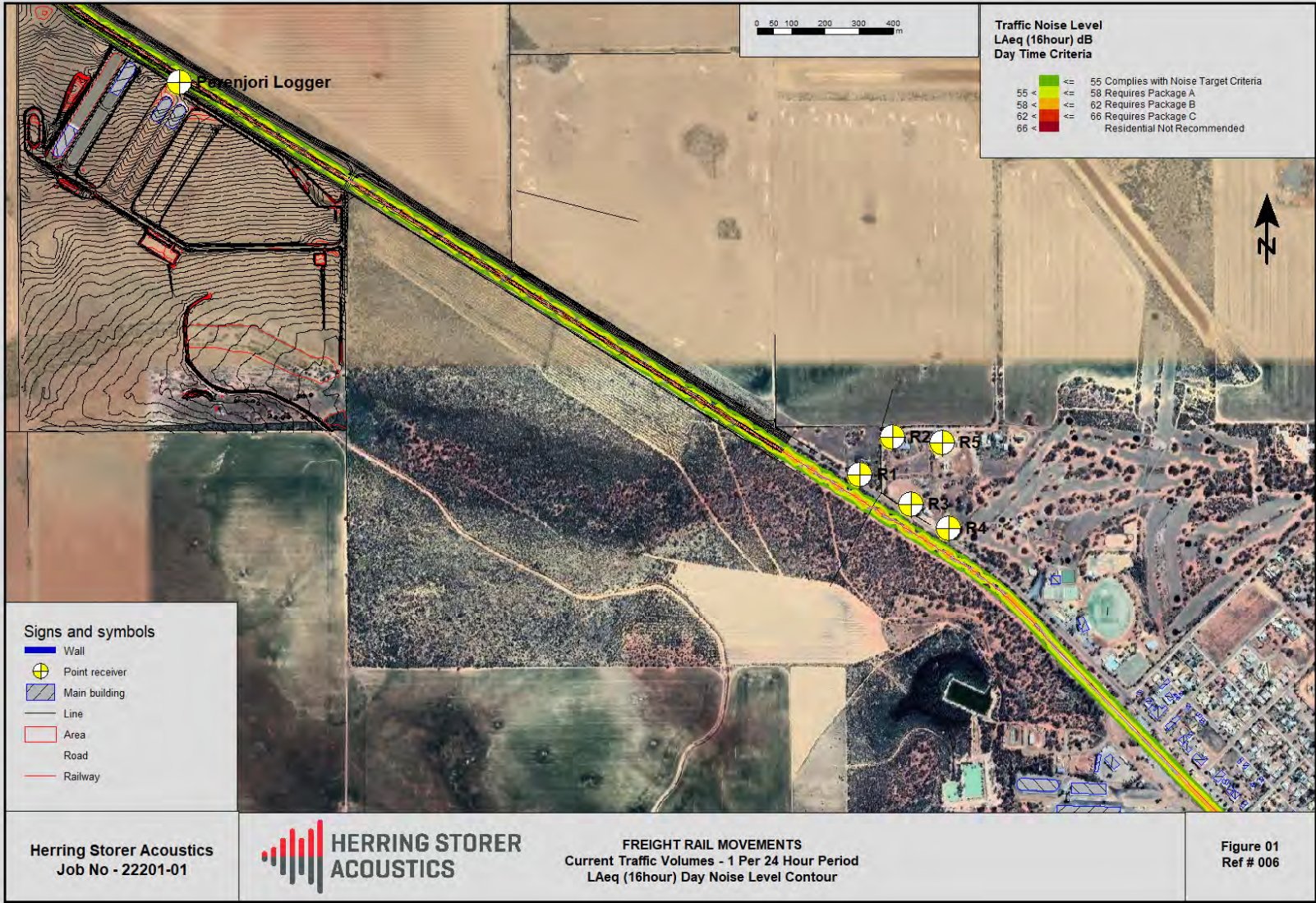
TABLE 7.4 – ASSESSMENT OF NOISE LEVELS – TRAIN IN LOADING SIDING

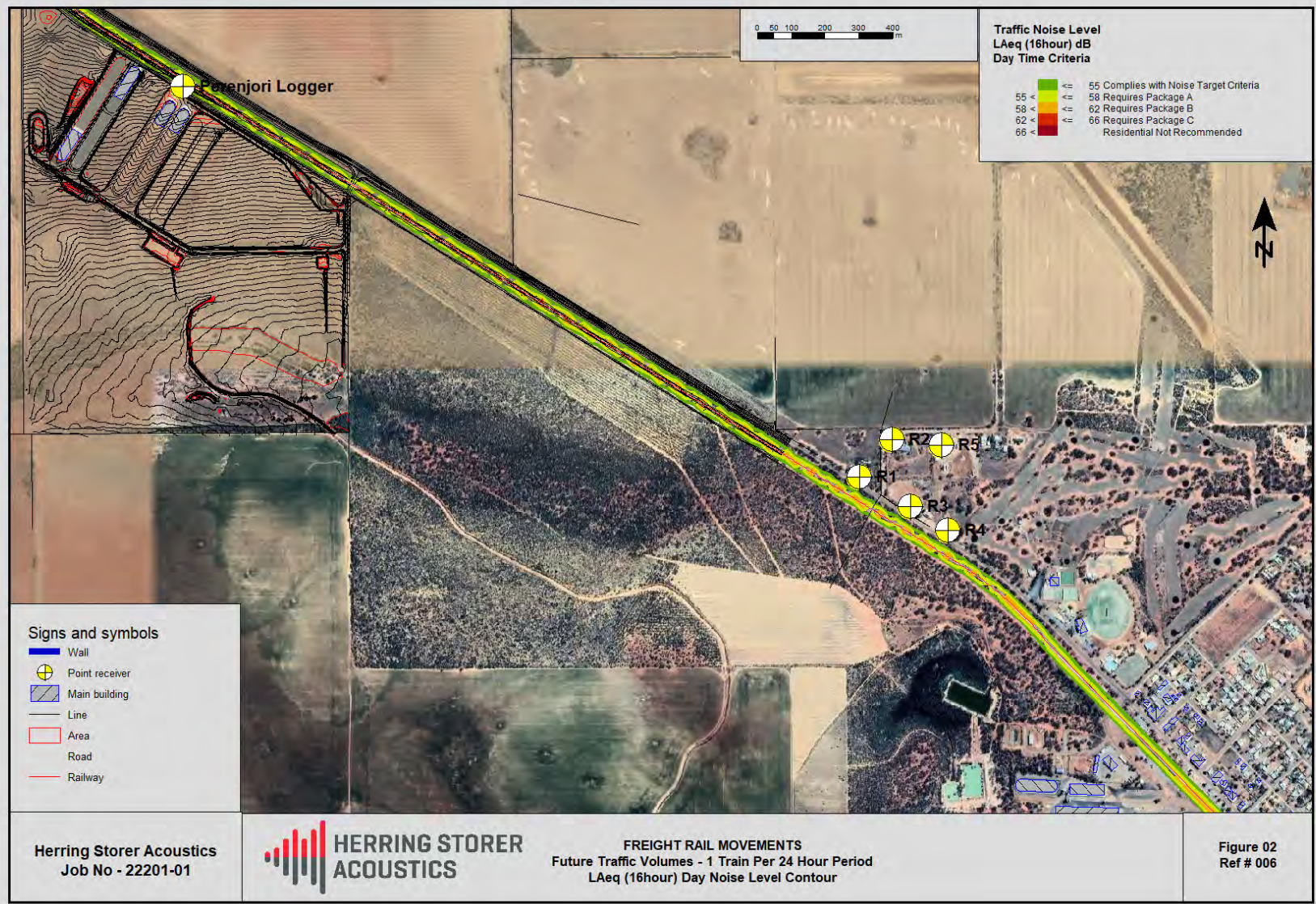
Scenario	Receiver	Assessable Noise Level, dB(A)	Applicable Times of Day	Applicable L _{A01} Assigned Noise Level (dB)	Exceedance to Assigned Noise Level L _{A01} (dB)
Day Time Scenario 2	R1	42	Day (Weekdays and Saturday 07:00 to 19:00)	45	Complies
	R2	43			Complies
	R3	38			Complies
	R4	36			Complies
	R5	37			Complies

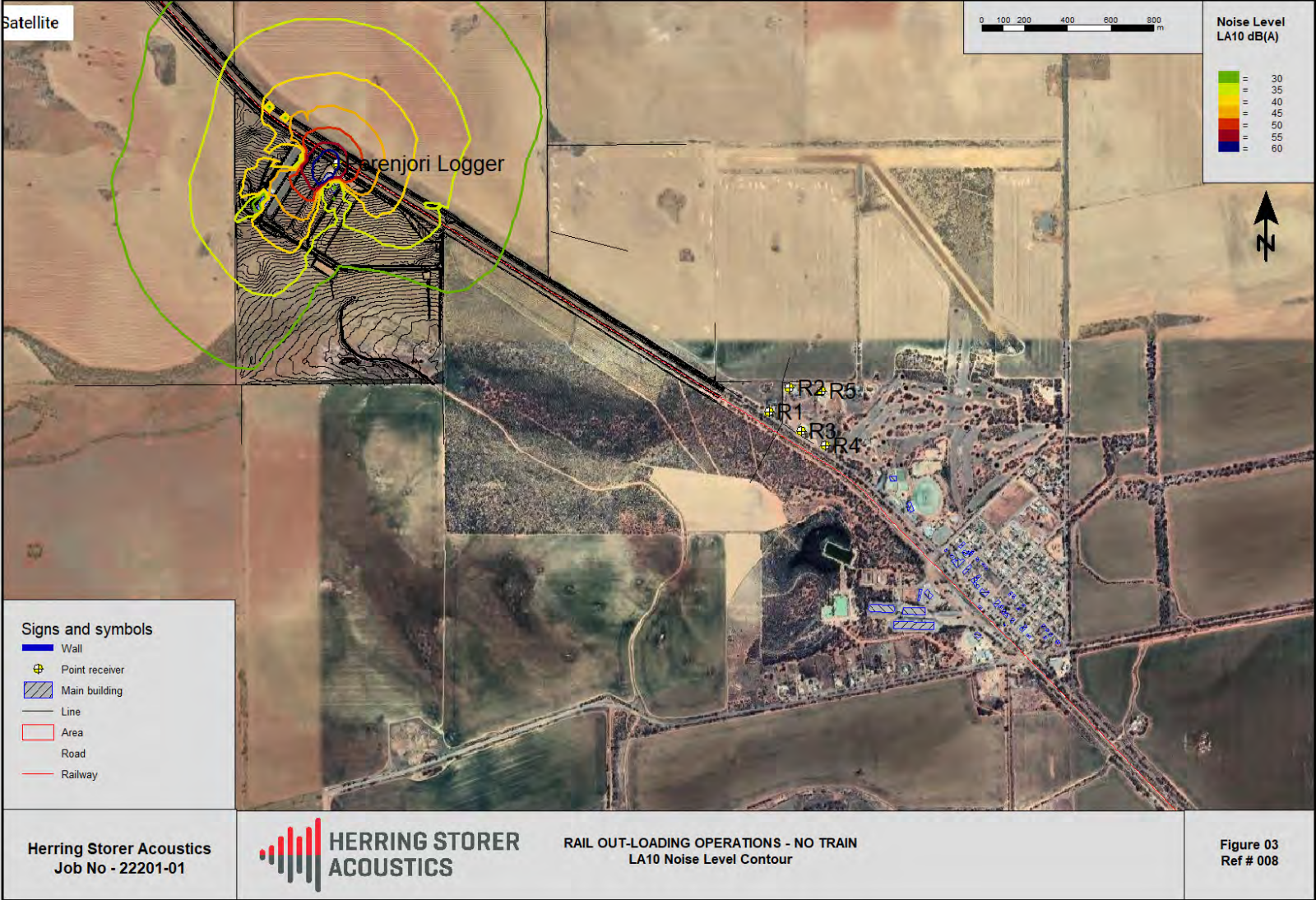
The acoustic assessment shows that in the worst case, that noise received at a premise is below the assigned noise level. Although noise assessments have been based on the day operations for the out-loading facility, it is noted that if loading occurred during the night period, the noise levels would still comply.

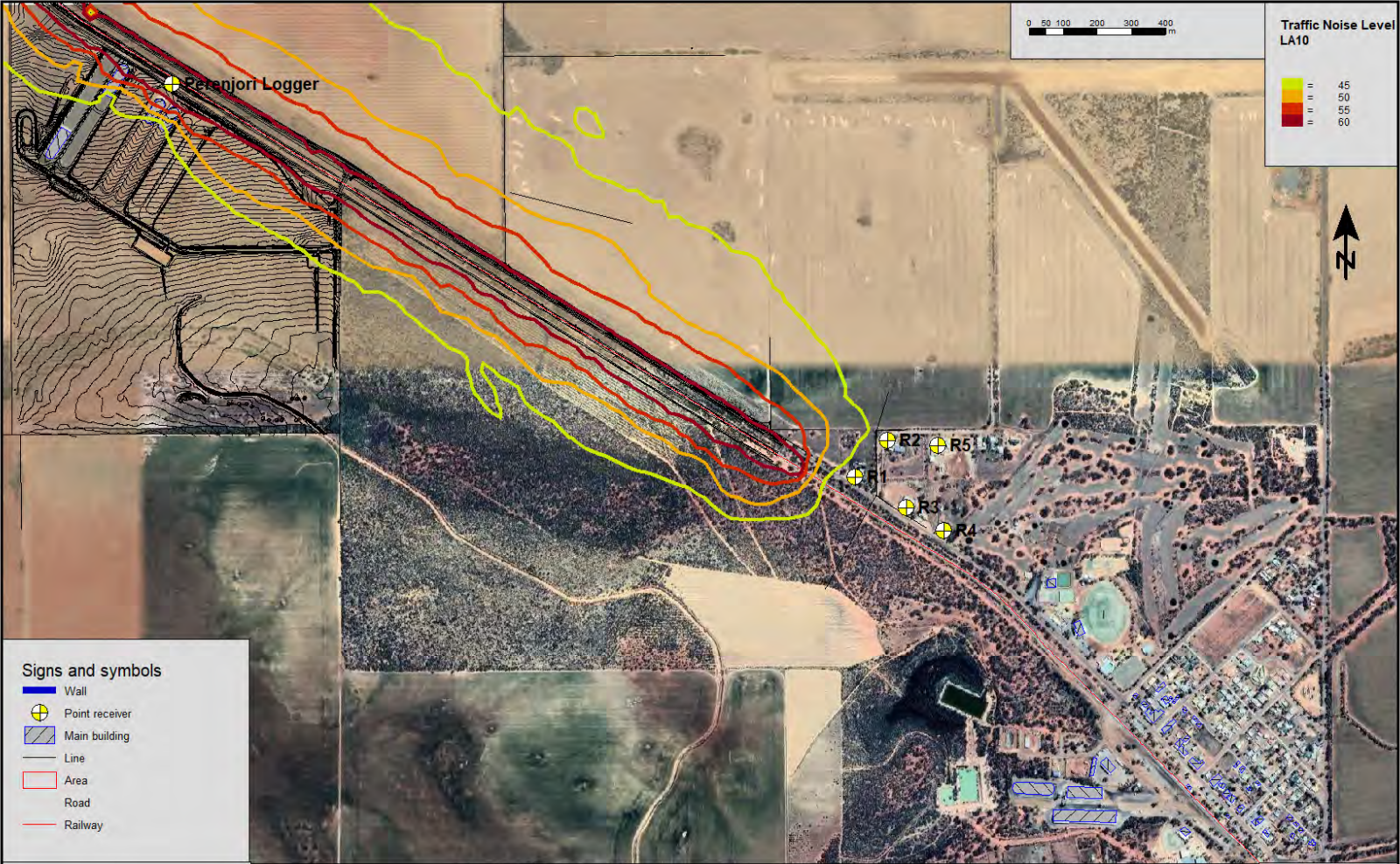
Thus, noise emissions from the proposed out-loading facility would be deemed to comply with the requirements of the Regulations.













Co-operative Bulk Handling Ltd
ABN 29 256 604 947
Level 6, 240 St Georges Terrace
Perth WA 6000 Australia
GPO Box L886
Perth WA 6842 Australia
Telephone
+61 8 9237 9600
Grower Service Centre
1800 199 083
cbh.com.au

11 March 2025

Paul Anderson
Shire of Perenjori
Via email: ceo@perenjori.wa.gov.au

Dear Paul,

ADDITIONS TO GRAIN HANDLING & STORAGE FACILITY (INDUSTRY-RURAL) AT LOT 101 ON DEPOSITED PLAN 65758, PERENJORI (CBH PERENJORI NORTH).

CBH is seeking development approval from the Shire of Perenjori for proposed additions to its grain handling and storage facility located at Lot 101 on Deposited Plan 6575, Perenjori. The subject application is prepared in accordance with Schedule 2 (*Deemed Provisions*) of the *Planning and Development (Local Planning Schemes) Regulations 2015* and the *Shire of Perenjori Local Planning Scheme No. 3 (LPS3)*.

Proposal

The proposed development incorporates the following elements:

- New truck marshalling area and bypass lane.
- Installation of a type 9 sample platform with 3 x pull forward lanes.
- Installation of a new 'in' weighbridge at 36m in length.
- New car parking area to accommodate 5 light vehicles.
- Drainage improvement works to direct stormwater runoff to existing drainage basins.

Shire of Perenjori Local Planning Scheme 3

Lot 101 is zoned rural under the Shire's operative LPS3. The best fit land use for CBH's grain handling and storage operations is 'Industry-Rural' which is defined in LPS3 as '*premises used for industry that supports and/or is associated with primary production*'.

This land use is already approved for the subject site and the proposal is therefore subject to the general development standards prescribed by Table 4 of LPS3 which sets out the development requirements for various land uses to ensure the scale, nature, design, general appearance, and impact of such uses is compatible with the objectives of the zone in which the development is located as well as the general purposes and aims of the Scheme.

Whilst 'industry-rural' is not specifically listed within Table 4, it is suggested that using the 'industry' site and development requirements would be appropriate. Table 4 stipulates that a front and rear boundary setback of 7.5m is the minimum site boundary setback. One car parking space per every two employees is the minimum number of car parking bays and a minimum landscaped area of 15% of the development site is to be provided

The subject site is a large landholding and the proposed setbacks for the weighbridge and sample hut are more than 300m from Wubin-Mullewa Road. These setbacks are compliant with the development standards of LPS3. The proposed additions are an extension to existing CBH operations without increasing the number of operational staff.

It is therefore considered that the number of car parking spaces proposed are appropriate to the rural zone in which the development is located. Further landscaping that addresses the development standards is not deemed appropriate as it would detract from the existing rural amenity that the site already provides that is consistent with existing landscaping on adjoining rural zoned properties.

It is noted that LPS3 stipulates that side boundary setback and plot ratio development requirements are to be determined by the local government. Notwithstanding noting the large landholding of the subject site, the generous lot boundary setbacks proposed, and that the proposal is for development contained within an existing development footprint it is considered that the setbacks, height and plot ratio proposed by this application is appropriate to the rural zone in which the development is located.

Whilst it is acknowledged that landscaping technically does not meet the development standards prescribed by Table 4, Clause 33 of LPS3 does allow for the local government to approve an application for development approval that does not comply with the general development standards and requirements. It is not considered that the proposed works will likely adversely affect any owners or occupiers in the locality. The local government may only approve an application for development approval under this clause if the local government is satisfied that approval of the proposed development would be appropriate having regard to the matters that the local government is to have regard to in considering an application for development approval as set out in clause 67(2) of the deemed provisions. Compliance with these matters is further discussed below.

Traffic Management

It is noted that this development application forms part of a wider strategy for grain receivals within the Perenjori locality. Specifically, there is a rail siding development application currently before the Shire for consideration as well as a future rail loading facility development application that intends to remove rail outloading and hubbing away from the townsite facility.

Under the current situation and using five-year average outloading data across both CBH facilities at Perenjori, 59.5% (70.9kt) of grain is currently outloaded via rail and 40.5% (48.3kt) of grain is outloaded via road. This means that currently 59kt of grain is transported from the Perenjori North receival facility to the Perenjori townsite receival facility for outloading via rail. This figure equates to a total of 980 trucks delivering to the Townsite facility from the Perenjori North Facility each year.

Construction of a new rail siding and rail loading facility in addition to the relocation of an entry weighbridge and sample pit at the Perenjori North receival facility will remove the requirement for these 980 truck movements from Perenjori North to Perenjori Townsite annually. It is accepted that with rail outloading taken away from the Townsite receival facility, some grain will need to be transported from the Townsite receival facility to the Perenjori North facility. The Townsite facility has a five year receival average of 23kt, 12kt of which are outloaded via rail. This equates to 200 trucks transporting grain between the Townsite facility to the North facility for outloading via rail.

The net effect of this development application in conjunction with the rail siding and rail loading facility projects is that 780 truck movements will be removed from the road network each year.

Stormwater Management

All stormwater drainage associated with the proposed development is to be directed to and contained within the two existing stormwater drainage basins. These drainage basins cater for a 1:20 year ARI event with all stormwater runoff from the proposed hardstand and associated accessways directed to this drainage basin system where all surface water will be detained on-site up to the 1:20 ARI year event.

Noting the small amount of non-permeable surface area proposed as part of this development application the existing drainage basin is able to accommodate additional stormwater runoff from the proposed additions. Should the Shire be of a different opinion on stormwater management and the ability for the two existing drainage basins to accommodate the additional non permeable surface area, CBH would be supportive of a condition being imposed on any development approval requiring stormwater to be retained and disposed of within the existing drainage basin. Any non-compliance and post development drainage basin modifications could be enforced through such a condition.

Noise & Dust Management

CBH shall ensure that noise from the specification and installation of any mechanical equipment as well as traffic and construction noise does not exceed assigned levels prescribed in the *Environmental Protection (Noise) Regulations 1997*, when it is received at a neighbouring property.

Dust Management will comply with *Environmental Protection Act 1986* and the relevant *National Environmental Protection Measures*. Due to the nature of grain handling and storage, some dust can be produced from a range of activities.


CBH is committed to improving the overall environmental impacts of its business and in achieving the environmental objectives outlined in the CBH Group Health, Safety and Environmental Policy. CBH undertakes frequent noise and dust monitoring across its sites to ensure that dust and noise levels are measured and are mitigated whenever there is an exceedance.

Conclusion

The proposed development is consistent with Clause 68 of the Schedule 2 (*Deemed Provisions*) of the *Planning and Development (Local Planning Schemes) Regulations 2015* and the provisions of LPS3. Further, the proposed additions are not considered to result in any new amenity impacts to the surrounding area. CBH respectfully requests the Application for Development Approval is considered expeditiously by the Shire of Perenjori given the straightforward nature of the application and its general compliance with the Shire's planning framework. It is therefore requested that Council exercise its discretion by granting conditional approval.

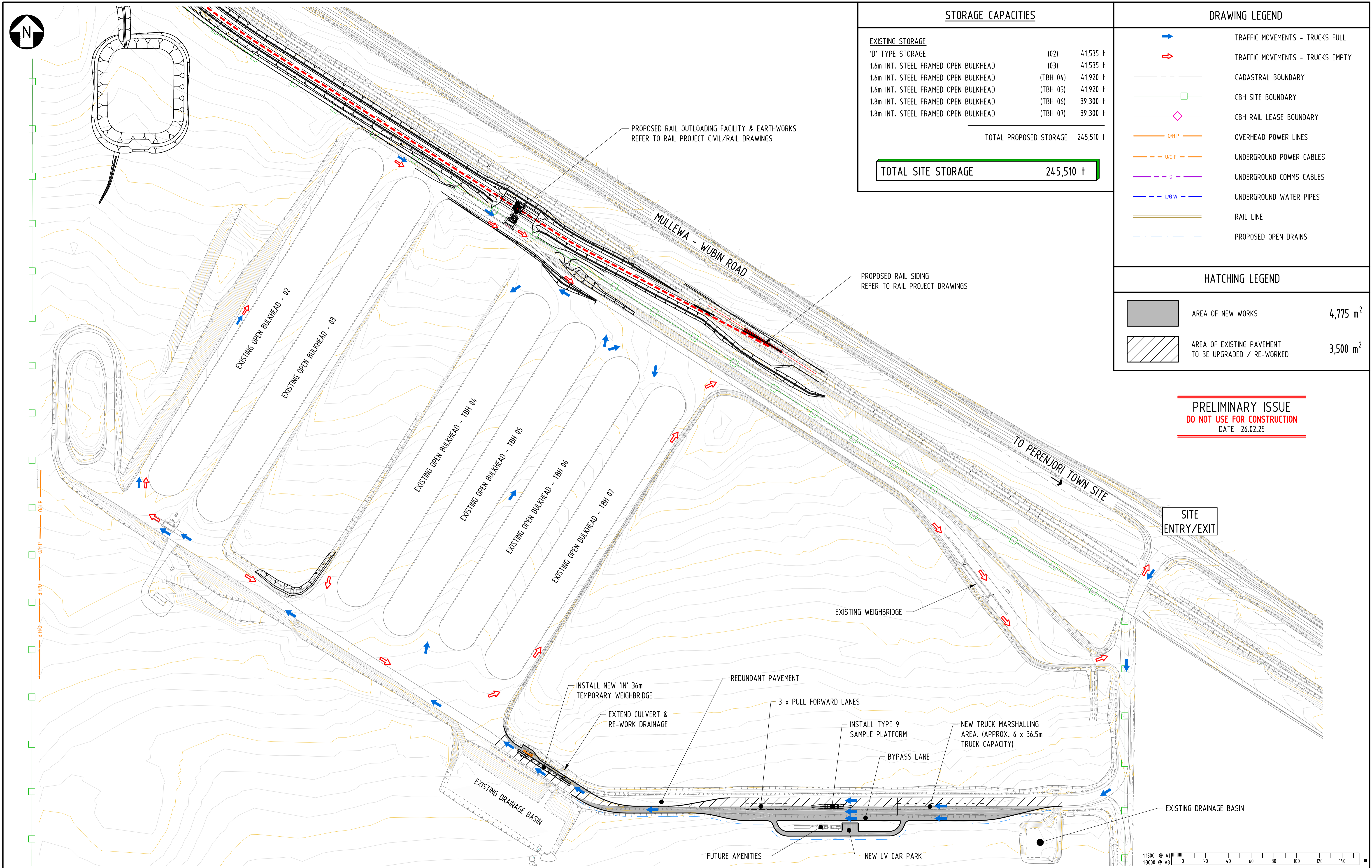
Should you have any question in relation to the details provided in this submission, please contact Timothy Roberts on 9216 6061 or timothy.roberts@cbh.com.au.

Yours Sincerely,



Timothy Roberts

Lead – Planning & Approvals



STORAGE CAPACITIES			
EXISTING STORAGE			
'D' TYPE STORAGE	(02)	41,535	†
1.6m INT. STEEL FRAMED OPEN BULKHEAD	(03)	41,535	†
1.6m INT. STEEL FRAMED OPEN BULKHEAD	(TBH 04)	41,920	†
1.6m INT. STEEL FRAMED OPEN BULKHEAD	(TBH 05)	41,920	†
1.8m INT. STEEL FRAMED OPEN BULKHEAD	(TBH 06)	39,300	†
1.8m INT. STEEL FRAMED OPEN BULKHEAD	(TBH 07)	39,300	†
TOTAL PROPOSED STORAGE		245,510	†
TOTAL SITE STORAGE		245,510	†

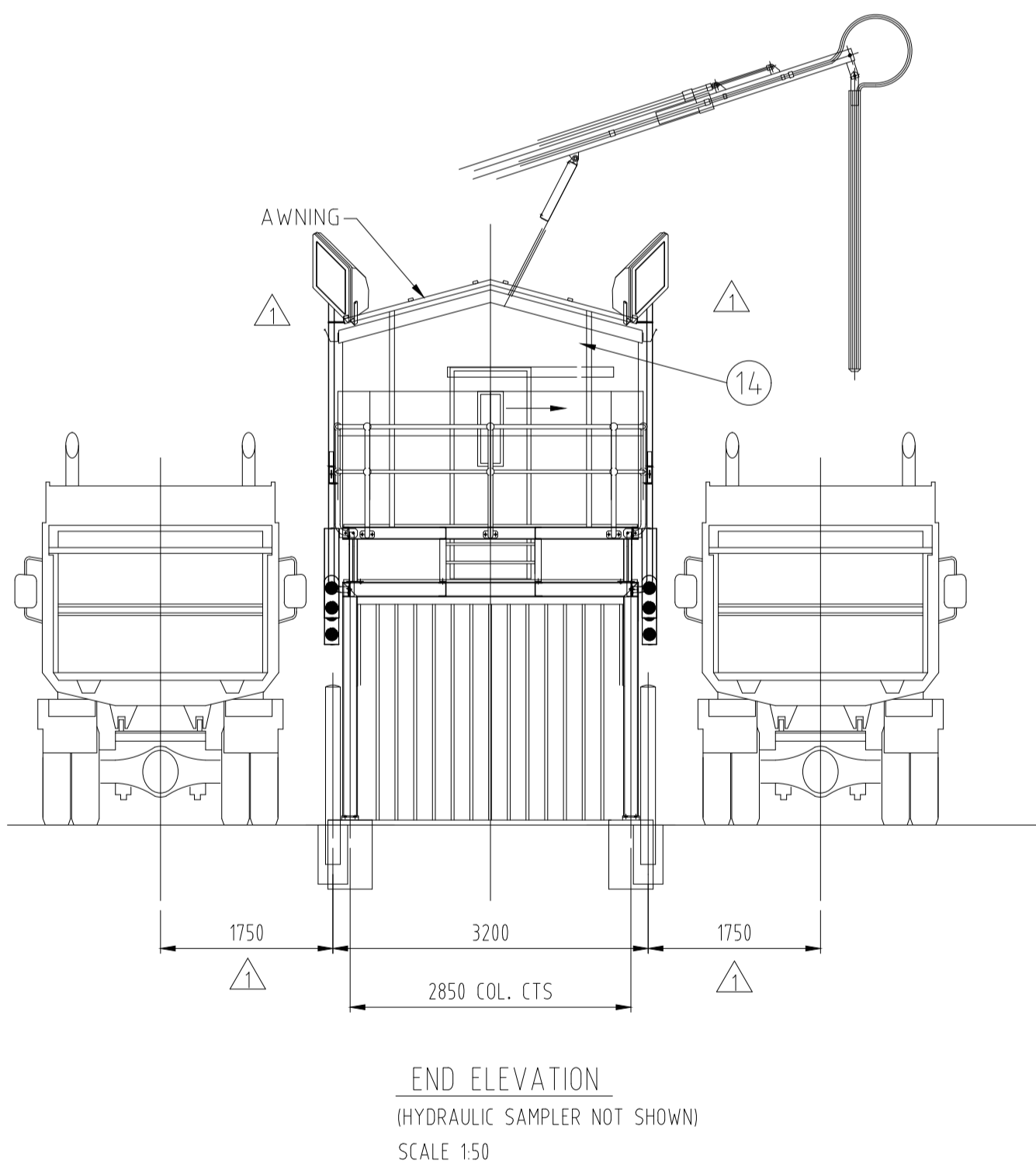
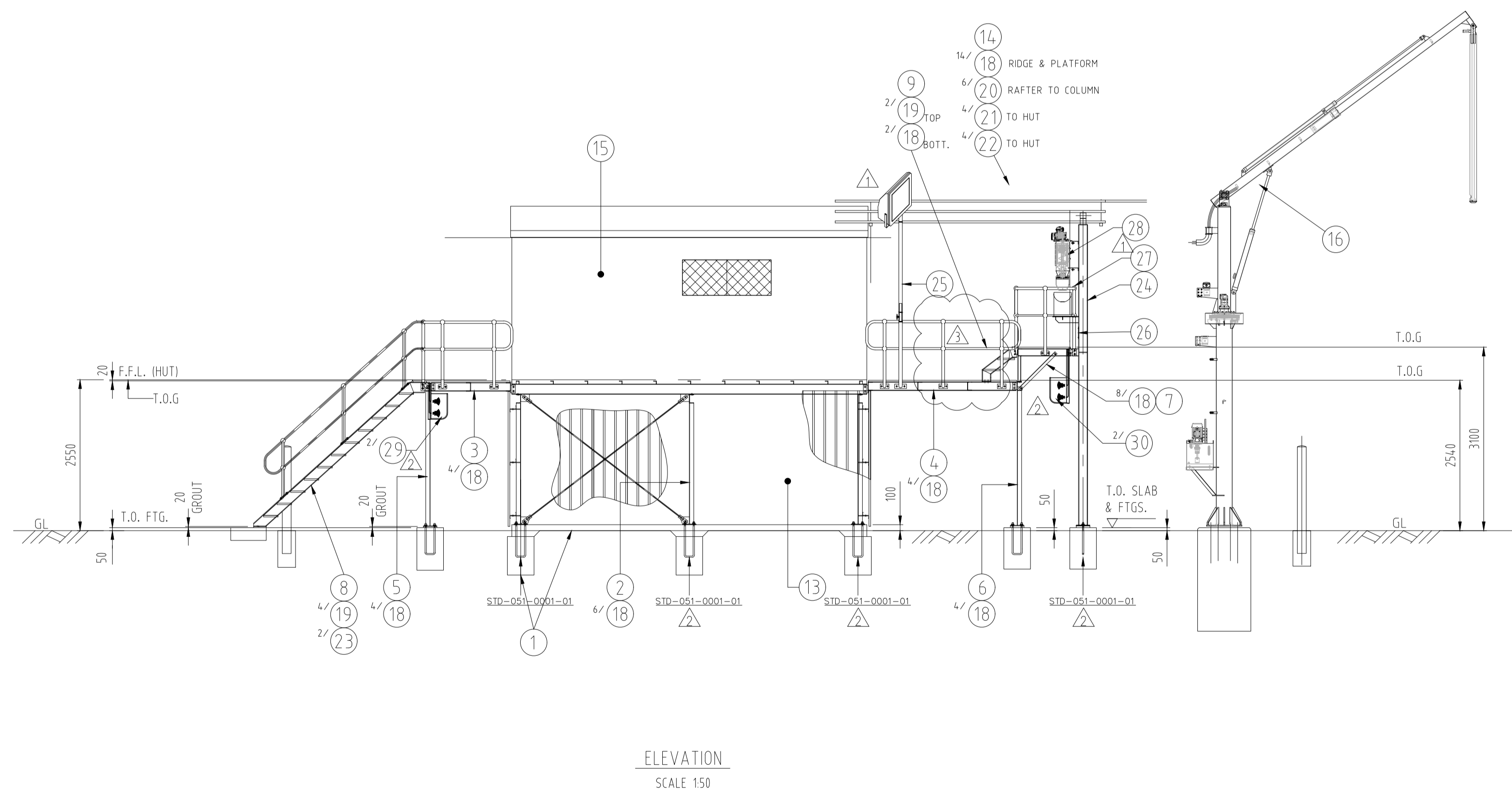
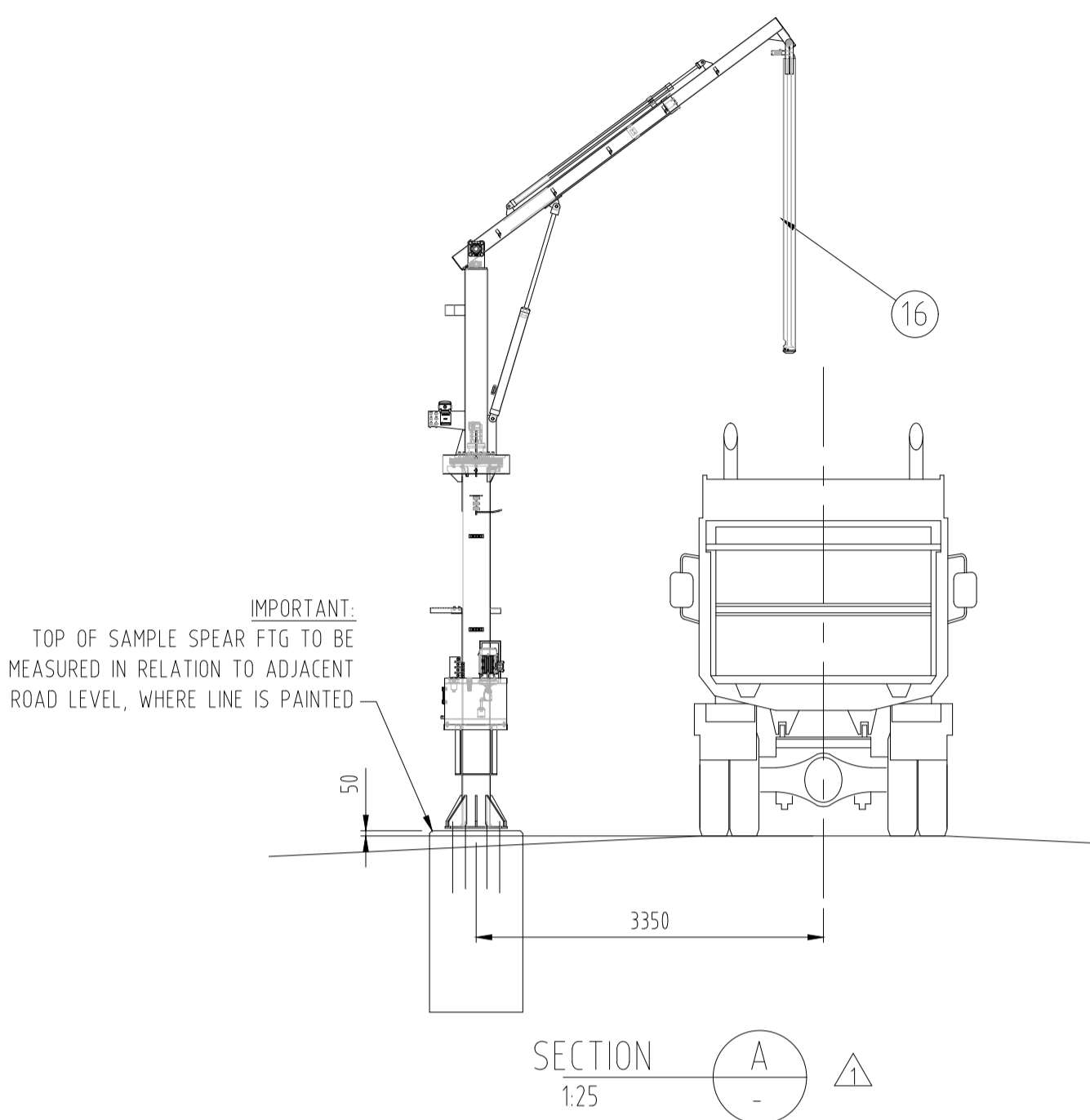
DRAWING LEGEND	
	TRAFFIC MOVEMENTS - TRUCKS FULL
	TRAFFIC MOVEMENTS - TRUCKS EMPTY
	CADASTRAL BOUNDARY
	CBH SITE BOUNDARY
	CBH RAIL LEASE BOUNDARY
	OHP
	UGP
	C
	UGW
	RAIL LINE
	PROPOSED OPEN DRAINS

HATCHING LEGEND	
	AREA OF NEW WORKS 4,775 m ²
	AREA OF EXISTING PAVEMENT TO BE UPGRADED / RE-WORKED 3,500 m ²

PRELIMINARY ISSUE
DO NOT USE FOR CONSTRUCTION
DATE 26.02.25

[illegible]

1. ALL STEELWORK, WELDING & FABRICATION TO COMPLY WITH CURRENT CODE AS1554 CAT SP.
2. ALL WELDS TO BE 6 FILLET UNLESS NOTED AND 3 FILLET ON 3 PL.
3. REMOVE ALL BURRS & SHARP EDGES.
4. SURFACE TREATMENT: AS PER CBH SPEC, NO TREATMENT TO INSIDE REQUIRED.
5. ALL HOLES Ø14 TO SUIT M12 HEX. HEAD BOLT GR. 4.6 U.N.O.



15.3 AUSTRALIAN LOCAL GOVERNMENT ASSOCIATION: NATIONAL GENERAL ASSEMBLY REGISTRATION

Applicant:	Shire of Perenjori
File:	ADM 0653
Date:	20 March 2025
Disclosure of Interest:	
Voting Requirements:	Simple Majority
Author:	Bianca Plug – Governance Officer
Responsible Officer:	Paul Anderson – Chief Executive Officer
Attachments:	Nil

Summary

For Council to consider the attendance of Elected Members and the Chief Executive Officer at the 2025 National General Assembly of Local Government (NGA) – incorporating the Regional Cooperation and Development Forum to be held from 24-27 June at the National Convention Centre, Canberra.

Background

The 31st Australian Local Government Association's annual NGA is a professional development and networking opportunity involving the largest gathering of local government elected members throughout Australia. The NGA provides an opportunity to learn about the work of local governments across Australia, to develop community leadership skills and to engage in advocacy on behalf of the Shire of Perenjori.

The National General Assembly starts on 24 June with a Welcome Reception. Sessions run from 25- 27 June 2025 at the National Convention Centre Canberra.

The Regional Forum runs in conjunction with the NGA on Tuesday 24 June 2025, 9.30am - 3.00pm at the National Convention Centre Canberra. It is an additional fee to attend.

The theme of this year's event will be "National Priorities Need Local Solutions", focusing on the key role that we all play delivering local-place based initiatives that help address our nation's big challenges.

Registrations are open with Early Bird rates available until Friday, 23 May 2025.

Statutory Environment

Nil

Policy Implications

Policy No. 7019 – Council Member Continuing Professional Development & Travel

Consultation

Elected Members – Shire of Perenjori
Paul Anderson – Chief Executive Officer

Financial Implications

There is approximately \$12,000 remaining in the 2024/25 Annual Budget for Elected Member training, travel and conference attendance. Should more than three Councillors wish to attend the 2025 National General Assembly, a budget amendment will need to be undertaken.

National General Assembly Registration

- Full Registration (Early Bird) \$979.00*
Includes all National General Assembly sessions, access to the Exhibition Hall, catering, Welcome Reception & Exhibition Opening, General Assembly satchel and materials.

Regional Forum Registration

- Regional Forum for NGA Delegate Discount \$279.00

Partner Registration (Optional)

- Partner Registration \$335.00
Includes a ticket to the Welcome Reception & Exhibition Opening and attendance to 2x day partner tours on Wednesday and Thursday during the NGA. Tours are yet to be announced.

Social Function (Optional)

- NGA Gala Dinner Ticket \$179.00

Canberra Accommodation Options

- Crowne Plaza Canberra \$367.00 per night
- Avenue Hotel \$315.00 per night
- Nesuto Canberra Apartment Hotel \$305.00 per night
- QT Hotel \$239.00 per night
- Mantra on Northbourne \$219.00 per night

Perth Accommodation Saturday, 28 June 2025

Due to the late return flight from CBR-PER, a hotel is recommended

- Perth Ascot Central Apartment Hotel \$179.00 per night
- Airport Apartments by Vetroblu \$217.00 per night

Flights: \$1,100.00 return (estimated)

TOTAL: \$3,911 per person**

* Early bird registration fee must be secured by 23 May 2025. Registrations beyond this date will be \$1099.

** The total amount per person excludes the Partner Registration and is based on 5 nights at the QT Hotel Canberra which is where Councillors stayed in 2023. The QT is approximately 800 meters (10-minute walk) from the National Convention Centre.

The total amount does not include other incidentals such as meals, uber/taxi fees or airport parking.

Strategic Community Plan

Goal 4: A strong and diverse Council working closely with the proactive and involved community.

4.3. The Shire advocates and partners effectively on behalf of the community.

Officer Comment

The last time the Council attended the NGA in Canberra was in 2023, with two Council members, Cr Sutherland and Cr Fraser, along with the Chief Executive Officer. The 2024 NGA was not attended.

As per Council Policy No. 7019 – Council Member Continuing Professional Development & Travel, authorisation for Shire representatives and Council members to attend a conference held outside of WA, is dependent upon approval granted by Council resolution.

As recommended by previous attendees, flights would be reserved for a day either side of the Assembly to allow a more leisurely arrival and departure.

Qantas is the only airline operating direct flights from Perth to Canberra. These flights are scheduled daily, with an average flight duration of approximately 3 hours 50 minutes (PER-CBR) and 4 hours 40 minutes (CBR-PER). Due to Qantas being the sole airline, flight times are limited with only the below currently available:

- Monday, 23 June 2025 (PER-CBR)
 - Depart Perth 1.05 pm
 - Arrive Canberra 6.55 pm
- Saturday, 28 June 2025 (CBR-PER)
 - Depart Canberra 7.45 pm
 - Arrive Perth 10.25 pm

OFFICER RECOMMENDATION

Council Resolution Number:

Moved:

Seconded:

That Council:

1. **Decline the invitation for the Chief Executive Officer and Elected Members to attend the 2025 National General Assembly of Local Government to be held from 24-27 June at the National Convention Centre, Canberra**

OR

2. **Authorise the attendance of the Chief Executive Officer and the following Elected Members to attend the 2025 National General Assembly of Local Government, incorporating the Regional Cooperation and Development Forum to be held from 24-27 June at the National Convention Centre, Canberra**

1. _____
2. _____
3. _____
4. _____
5. _____
6. _____
7. _____

Motion put and carried / lost

For:

Against:

16. Confidential Reports:

17. Ordering the Common Seal:

18. Reports of Committees and Members:

19. Motions of Which Previous Notice Has Been Given:

20. Notice of Motions:

21. New Business of an Urgent Nature Admitted by Council:

22. Closure of Meeting:

The Shire President to declare the meeting closed.

23. Next Meeting:

The Shire President to advise that the next Ordinary Meeting of Council has been rescheduled and will now be held on Thursday, 17 April 2025 in the Shire of Perenjori Council Chambers, Perenjori WA 6620, commencing at 3.00 pm.